

Borough Council of
**King's Lynn &
West Norfolk**



Planning Committee

Agenda

Monday, 5th June, 2023
at 9.30 am

in the

Assembly Room
Town Hall
King's Lynn

Also available to view at

<https://youtube.com/user/WestNorfolkBC>



King's Court, Chapel Street, King's Lynn, Norfolk, PE30 1EX
Telephone: 01553 616200
Fax: 01553 691663

PLANNING COMMITTEE AGENDA

Please note that due to the number of applications to be considered it is proposed that the Committee will adjourn for lunch at approximately 12.30 pm and reconvene at 1.10 pm.

Please ensure that all mobile phones are switched to silent

DATE: Monday, 5th June, 2023

VENUE: Assembly Room, Town Hall, Saturday Market Place, King's Lynn PE30 5DQ

TIME: 9.30 am

1. APPOINTMENT OF VICE-CHAIR FOR THE MEETING

To appoint a Vice-Chair for the meeting.

2. APOLOGIES

To receive any apologies for absence and to note any substitutions.

3. MINUTES

To confirm as a correct record the Minutes of the Meeting held on 24 April 2023.

4. DECLARATIONS OF INTEREST (Page 7)

Please indicate if there are any interests which should be declared. A declaration of an interest should indicate the nature of the interest (if not already declared on the Register of Interests) and the agenda item to which it relates. If a disclosable pecuniary interest is declared, the Member should withdraw from the room whilst the matter is discussed.

These declarations apply to all Members present, whether the Member is part of the meeting, attending to speak as a local Member on an item or simply observing the meeting from the public seating area.

Councillor appointed representatives on the Internal Drainage Boards are noted.

5. URGENT BUSINESS UNDER STANDING ORDER 7

To consider any business, which by reason of special circumstances, the Chair proposes to accept, under Section 100(b)(4)(b) of the Local Government Act, 1972.

6. MEMBERS ATTENDING UNDER STANDING ORDER 34

Members wishing to speak pursuant to Standing Order 34 should inform the Chairman of their intention to do so and on what items they wish to be heard before a decision on that item is taken.

7. CHAIR'S CORRESPONDENCE

To receive any Chair's correspondence.

8. RECEIPT OF LATE CORRESPONDENCE ON APPLICATIONS

To receive the Schedule of Late Correspondence received since the publication of the agenda.

9. GLOSSARY OF TERMS (Pages 8 - 12)

10. INDEX OF APPLICATIONS (Pages 13 - 15)

The Committee is asked to note the Index of Applications.

a) Decisions on Applications (Pages 16 - 257)

To consider and determine the attached Schedule of Planning Applications submitted by the Executive Director.

11. DELEGATED DECISIONS (Pages 258 - 298)

To receive the Schedule of Planning Applications determined by the Executive Director.

To: Members of the Planning Committee

Councillors Anota, R Blunt, F Bone, A Bubb, M de Whalley, de Winton, Devulapalli, Everett, Lintern, B Long, Ring, C Rose, J Rust (Chair), Mrs V Spikings (Vice-Chair), M Storey and D Tyler

Site Visit Arrangements

When a decision for a site inspection is made, consideration of the application will be adjourned, the site visited, and the meeting reconvened on the same day for a decision to be made. Timings for the site inspections will be announced at the meeting.

If there are any site inspections arising from this meeting, these will be held on Thursday 8th June 2023 (time to be confirmed) and the meeting reconvened on the same day (time to be agreed).

Please note:

- (1) At the discretion of the Chairman, items may not necessarily be taken in the order in which they appear in the Agenda.
- (2) An Agenda summarising late correspondence received by 5.15 pm on the Thursday before the meeting will be emailed (usually the Friday), and tabled one hour before the meeting commences. Correspondence received after that time will not be specifically reported during the Meeting.
- (3) **Public Speaking**

Please note that the deadline for registering to speak on the application is 12 noon the working day before the meeting, **Friday 2nd June 2023**. Please contact borough.planning@west-norfolk.gov.uk or call (01553) 616818 or 616234 to register.

For Major Applications

Two speakers may register under each category: to object to and in support of the application. A Parish or Town Council representative may also register to speak. Each speaker will be permitted to speak for five minutes

For Minor Applications

One Speaker may register under category: to object to and in support of the application. A Parish or Town Council representative may also register to speak. Each speaker will be permitted to speak for three minutes.

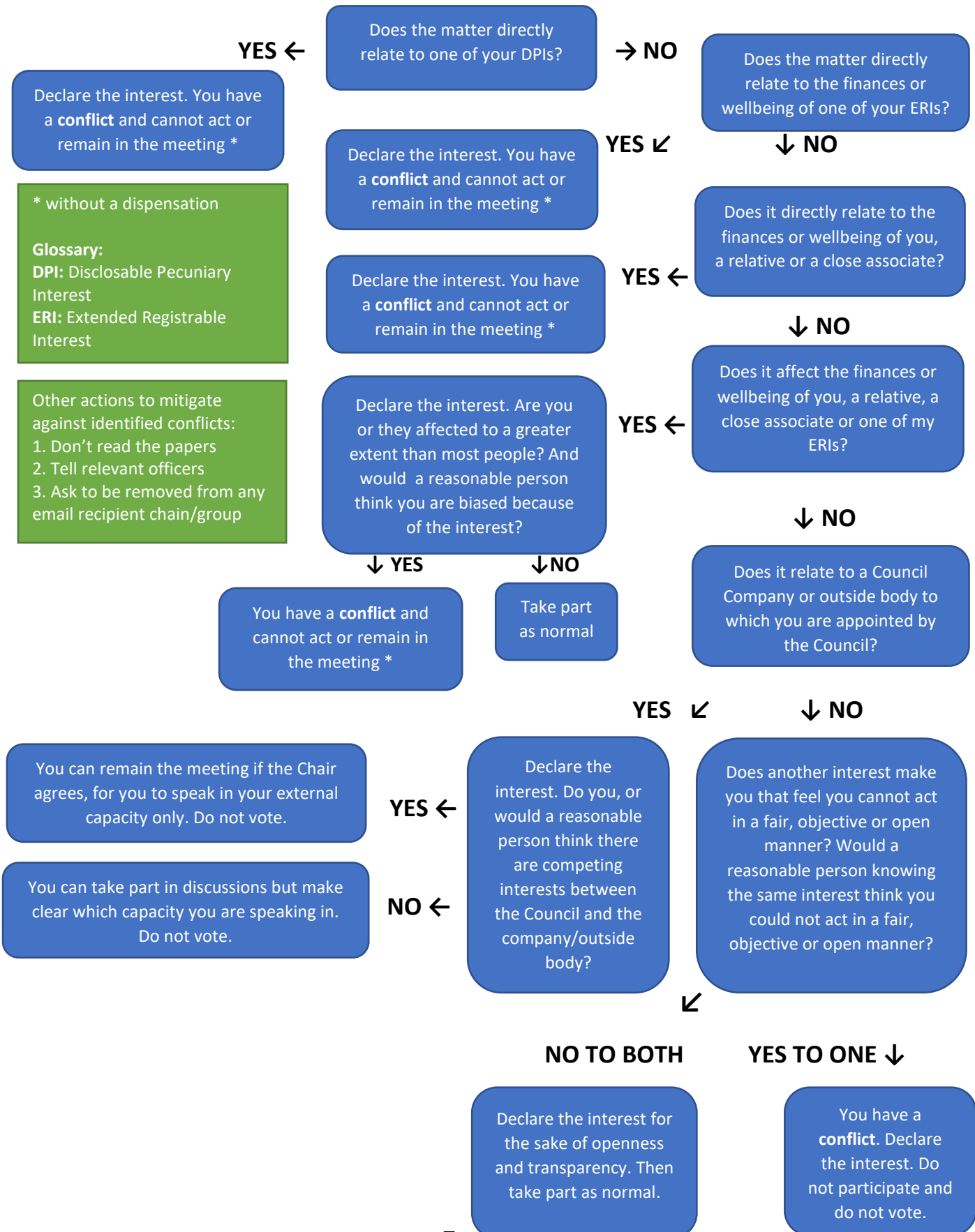
For Further information, please contact:

Kathy Wagg on 01553 616276
Kathy.wagg@west-norfolk.gov.uk



DECLARING AN INTEREST AND MANAGING ANY CONFLICTS FLOWCHART

START



Declare the interest. You have a **conflict** and cannot act or remain in the meeting *

* without a dispensation

Glossary:

DPI: Disclosable Pecuniary Interest

ERI: Extended Registrable Interest

Other actions to mitigate against identified conflicts:

1. Don't read the papers
2. Tell relevant officers
3. Ask to be removed from any email recipient chain/group

Glossary of Terms and Abbreviations	
AIA	Arboricultural Impact Assessment
AMS	Arboricultural Method Statement
AOD	Above Ordnance Datum
AONB	Area of Outstanding Natural Beauty
AQMA	Air Quality Management Plan
ATC	Air Traffic Controller
BCKLWN	Borough Council of King's Lynn and West Norfolk
BCN	Breach of Condition Notice
BNG	Biodiversity Net Gain
BS	British Standard
CA	Conservation Area
CCTV	Closed Circuit Television
CHZ	Coastal Hazard Zone
CIL	Community Infrastructure Levy
CLEUD	Certificate of Lawful Existing Use or Development
CLOPUD	Certificate of Lawful Proposed Use or Development
CRM	Collision Risk Modelling
CS	Core Strategy
CSH	Code for Sustainable Homes
CSNN	Community Safety and Neighbourhood Nuisance
CTMP	Construction Traffic Management Plan
CWS	County Wildlife Site
D and A	Design and Access Statement
DDA	Disability Discrimination Act
DEFRA	Department for Environment, Food and Rural Affairs
DISC	Discharge of Condition
DMPP	Development Management Policies Plan
DS	Design Statement
EA	Environment Agency
EBR	Economic Benefit Report
EIA	Environmental Impact Assessment
EN	Enforcement Notice
EVC	Electric Vehicle Charging

FFL	Finished Floor Level
FRA	Flood Risk Assessment
GCN	Great Crested Newts
GIRAMS	Green Infrastructure and Recreational Impact Avoidance and Mitigation Strategy
GPDO	General Permitted Development Order
HAS	Health and Safety Assessment
HELAA	Housing and Economic Land Availability Assessment
HPG	Historic Parks and Gardens
HRA	Habitat Regulations Assessment
HSE	Health and Safety Executive
IAQM	Institute of Air Quality Management
IDB	Internal Drainage Board
IROPI	Imperative Reasons of Overriding Public Interest
LB	Listed Building
LCA	Landscape Character Assessment
LDFCS	Local Development Framework Core Strategy
LHA	Local Highway Authority
LLFA	Lead Local Flood Authority
LP	Local Plan
LPA	Local Planning Authority
LVA	Landscape and Visual Appraisal
LVIA	Landscape and Visual Impact Assessment
MOD	Ministry of Defence
MUGA	Multi Use Games Area
NCC	Norfolk County Council
NCP	North Coast Partnership
NDG	National Design Guide
NE	Natural England
NHBC	National House Building Council
NMDC	National Model Design Guide
NMP	Noise Management Plan
NNR	National Nature Reserve
NP	Neighbourhood Plan
NPPF	National Planning Policy Framework
NPPG	National Planning Policy Guidance

OIA	Ornithological Impact Assessment
OS	Ordnance Survey
PADHI	Planning Advice for Development near Hazardous Installations
PCN	Planning Contravention Notice
PCPA	Planning and Compulsory Purchase Act
PEA	Preliminary Ecological Appraisal
PINs	Planning Inspectorate
POS	Public Open Space
PPG	Planning Practice Guidance
PROW	Public Rights of Way
PS	Protected Species
PSS	Protected Species Survey
RP	Registered Provider
RPA	Root Protection Area
RS	Ramsar Site
RSS	Regional Spatial Strategy
S106	Section 106 Agreement (Planning Legal Agreement)
S278	Section 278 Agreement (provide the legal mechanism required to carry out highway alterations)
S38	Section 38 Agreement (secure new road adoption by the highway authority)
SAC	Special Areas of Conservation
SADMPP	Site Allocations and Development Management Policies Plan
SCI	Statement of Community Involvement
SD	Sustainable Development
SFRA	Strategic Flood Risk Assessment
SHLAA	Strategic Housing Land Availability Assessment
SHMA	Strategic Housing Market Assessment
SME	Subject Matter Expert
SOS	Secretary of State
SPA	Special Protection Area
SPD	Supplementary Planning Document
SS	Spatial Strategy
SSSI	Site of Special Scientific Interest
SUDS	Sustainable Urban Drainage Scheme
TA	Transport Assessment
TCPA	Town and Country Planning Act

TEMPO	Tree Evaluation Method for Preservation Orders
TPO	Tree Preservation Order
TPP	Tree Protection Plan
TRO	Traffic Regulation Order
UCO	Use Class Order
UU	Unilateral Undertaking
VA	Viability Assessment
VOA	Valuation Office Agency
WHO	World Health Organisation
WSI	Written Scheme of Investigation

Suffixes to Reference Numbers	
A	Advertisement Consent
AG	Agricultural Prior Notification
BT	Adoption/Removal of BT Payphone Box
CM	County Matter
CU	Change of use (where no development is involved)
CON	Consultation by Adjoining Authority
DM	Demolition Prior Notification
F	Full Application (including Householder)
FM	Full Major Application
HZ	Hazardous Substance Application
LDE	Lawful Development Certificate (existing use or development)
LDP	Lawful Development Certificate (proposed use or development)
NMA	Non Material Amendment
O	Outline Application
OM	Outline Major Application
PACU	Prior Notification for a change of use (i.e. barn to dwelling)
PAGPD	Householder Prior Notification (larger home extension)
PAGAA	Householder Prior Notification (increase by adding an additional storey onto a dwelling)
PIP	Permission in Principle
RM	Reserved Matters Application
RMM	Reserved Matters Major Application

S257	Divert/stop up a Public Right of Way
T3	Telecoms Prior Notification
TPO	Application for works to Tree(s) subject to a TPO
TREECA	Application for works to Tree(s) in a Conservation Area

**INDEX OF APPLICATIONS
TO BE DETERMINED BY THE PLANNING COMMITTEE AT THE MEETING
TO BE HELD ON MONDAY
5 June 2023**

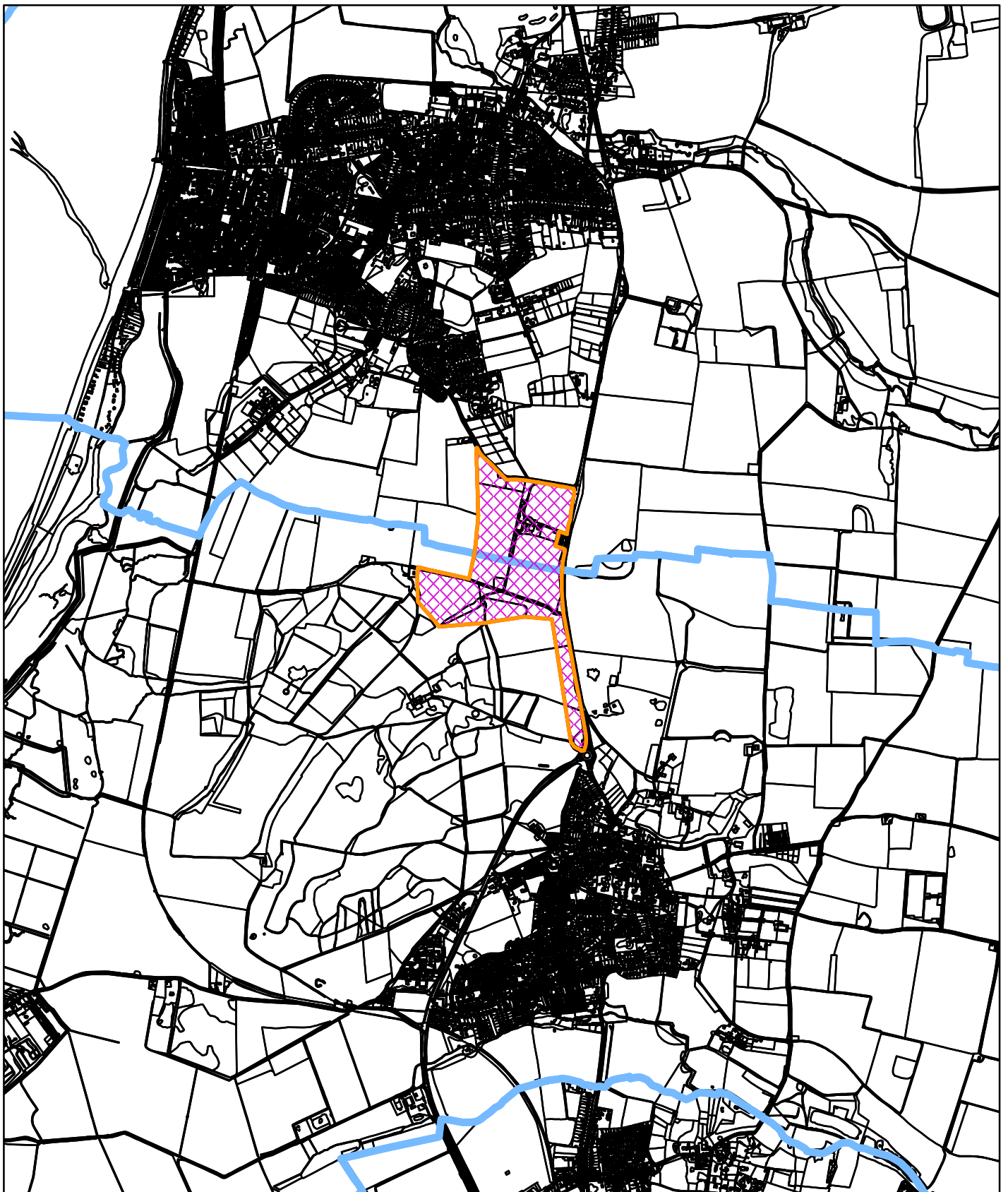
Item No.	Application No. Location and Description of Site Development	PARISH	Recommendation	Page No.
10/1	DEFERRED ITEMS			
10/1(a)	22/01648/FM Heacham Bottom Farm Lynn Road PE31 7PQ Change of use of existing buildings and new buildings to provide - new visitor centre, cafe, event and retail space, indoor play building, bike hire service, change of use of land to play facilities and creation of new bike tracks, woodland edge glamping units, car parking, new landscaping and off-road path.	HEACHAM SNETTISHAM	APPROVE	16
10/1(b)	22/01650/FM Mount Pleasant Farm 25 Lamsey Lane PE31 7LE Change of use of land to provide 20 touring caravan pitches with hard standing; change of use of land to create areas for camping and grass touring caravan pitches; change of use of existing buildings and new building to provide - visitor utility building, reception/retail area and storage area, creation of parking area (temporary parking/drop off) new landscaping and off road path.	HEACHAM SNETTISHAM	APPROVE	83
10/1(c)	22/02113/F Terns 49 Peddars Way PE36 6LD VARIATION OF CONDITION 2 OF PLANNING CONSENT 21/01394/F: Extensions and alterations to dwelling	HOLME-NEXT- THE-SEA	APPROVE	137
10/2	OTHER APPLICATIONS/APPLICATIONS REQUIRING REFERENCE TO THE COMMITTEE			
10/2(a)	22/02214/F Land W of Kenwick Hall and S of Track Station Road PE34 4DH Part retrospective agricultural store	CLENCHWARTON	APPROVE	153

Item No.	Application No. Location and Description of Site Development	PARISH	Recommendation	Page No.
10/2(b)	23/00271/F The Lavenders St Andrews Lane Congham PE32 1DU Change of use from studio to short term holiday let (2 persons)	CONGHAM	APPROVE	167
10/2(c)	23/00078/F Land E of the Memorial Hall High Street PE33 9EJ Construction of one single storey dwelling	FINCHAM	APPROVE	176
10/2(d)	23/00273/F 44 South Moor Drive PE31 7BW Rear extension and garage conversion	HEACHAM	APPROVE	191
10/2(e)	22/00282/F 90 Gayton Road PE30 4ER Change of use of a dwelling house to an 8 room (8 household) HMO	KINGS LYNN	APPROVE	198
10/2(f)	23/00470/CU 20 Woodside PE30 4SD Change of use of open space land to garden land	KINGS LYNN	APPROVE	207
10/2(g)	23/00361/F 2 Two Acres Middleton PE32 1YF Single storey extension to front of house	MIDDLETON	APPROVE	213
10/2(h)	23/00092/F Stoke Ferry Timber Ltd Boughton Road North Stoke Ferry PE33 9BF The siting of 30 '20 foot' containers for local storage (Part-retrospective)	STOKE FERRY	APPROVE	219
10/2(i)	23/00125/CU Stoke Ferry Timber Ltd, Boughton Road North, Stoke Ferry, PE33 9BF A change of use from an agricultural lorry park and outside storage area to a commercial outside storage area for construction materials and items connected with Stoke Ferry Timber Ltd. Lorry parking is not applied for (Retrospective)	STOKE FERRY	APPROVE	229

Item No.	Application No. Location and Development	Description of Site	PARISH	Recommendation	Page No.
10/2(j)	23/00265/CU	10 Folgate Lane Walpole St Andrew PE14 7HY Change of use of bungalow from a dwelling (C3) to a children's home (C2) for up to three children	WALPOLE	APPROVE	240
10/3	TREE PRESERVATION ORDER				
10/3(a)	2/TPO/00647		HUNSTANTON	6 MONTHS EXPIRES 14 JUNE 2023	254

Agenda Item 10a
23/00393/LDP

Heacham Bottom Farm Lynn Road Heacham PE31 7PQ



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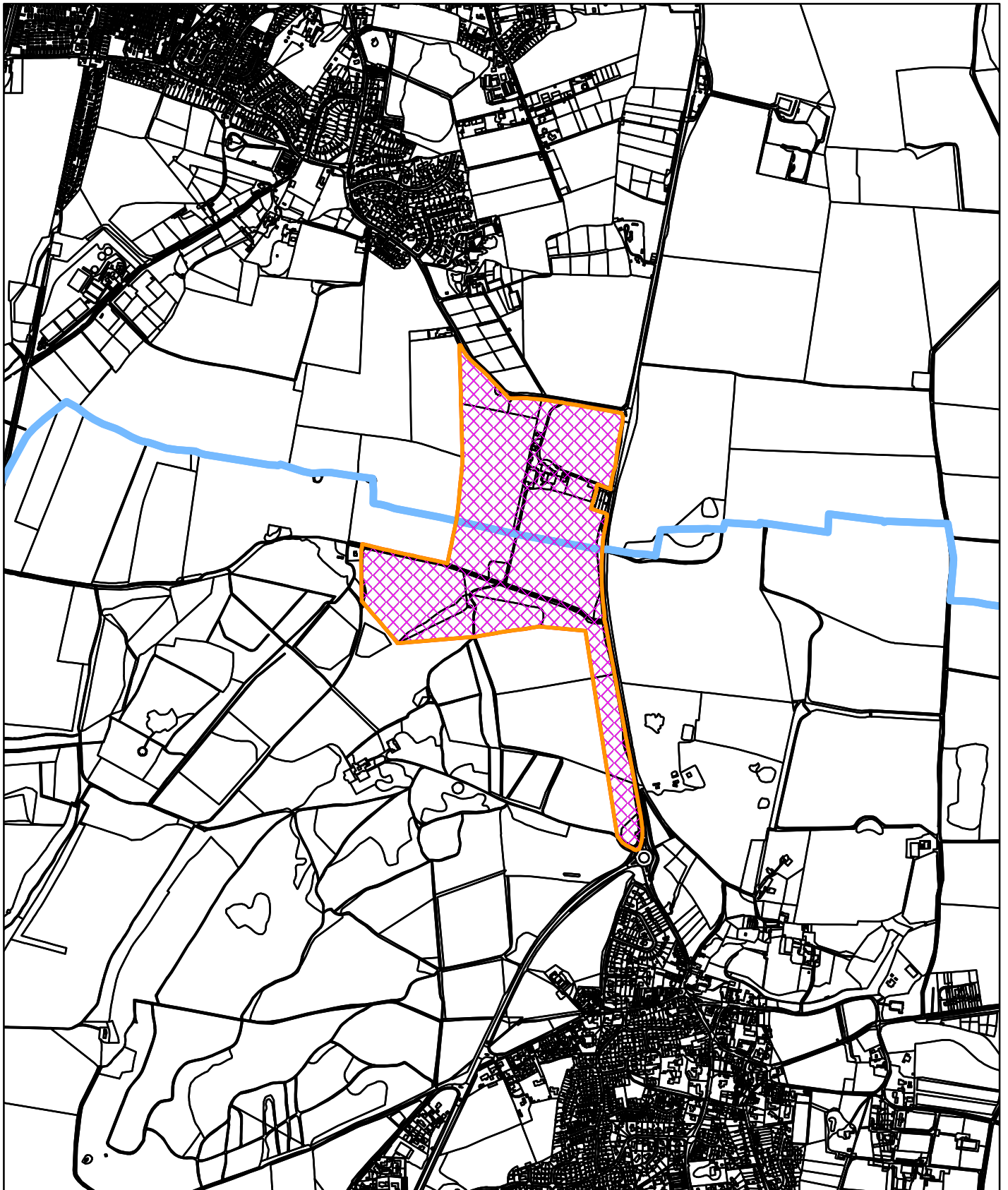


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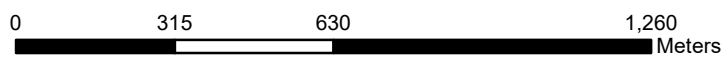


23/00393/LDP

Heacham Bottom Farm Lynn Road Heacham PE31 7PQ



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22/05/2023

17



Parish:	Heacham Snettisham	
Proposal:	Change of use of existing buildings and new buildings to provide - new visitor centre, cafe, event and retail space, indoor play building, bike hire service, change of use of land to play facilities and creation of new bike tracks, woodland edge glamping units, car parking, new landscaping and off-road path.	
Location:	Heacham Bottom Farm Lynn Road Heacham KINGS LYNN	
Applicant:	Wild Ken Hill	
Case No:	22/01648/FM (Full Application - Major Development)	
Case Officer:	Mrs N Osler	Date for Determination: 5 January 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Deferred from April 3rd Committee Called in by Cllr Parish

Neighbourhood Plan: YES

Members Update

Some Members will recall that, although the Local Highway Authority (LHA) concluded that the proposed development would not result in the need to seek significant improvements to Lamsey Lane / Lynn Road junction, this application was deferred from April 3 Committee to enable an update on an ongoing feasibility study by Norfolk County Council for improvements to this junction. Additionally, the Parish Council requested additional traffic information be submitted because they queried the findings of the Automatic Traffic Count (ATC.)

The feasibility study has now been concluded and looked at various traffic light and roundabout options, some with bus priority measures, to improve traffic flow at the junction. The simpler forms of these are lower-cost and require less land take, whereas the roundabout option, which is a typical solution for principal road junctions, would involve higher costs and more land take. A summary of the findings of the feasibility study is attached as Appendix 1 to this report.

The following report has not been updated other than in relation to the latest comments from the Local Highway Authority and minor amendments which have been **emboldened for ease.**

Case Summary

Full planning permission is sought for a new destination day visitor attraction including a small, 10 yurt, glamping site.

The site, that measures c.38ha comprises an area of existing built form in the farmyard, areas of agricultural fields and rough pasture, and a small part of Ken Hill Wood.

Part of the site falls within the Norfolk Coast Area of Outstanding Natural Beauty (AONB.)

The northern part of the site falls within the parish of Heacham, whilst the southern part fall within the Parish of Snettisham.

The site lies outside the development boundary of both villages.

It is suggested that the proposed development would generate 16.25 full time equivalent jobs.

Key Issues

Environmental Impact Assessment

Principle of development

Form and character and impact on the AONB

Highway safety

Impact on neighbour amenity

Ecology / Biodiversity

Drainage

Crime and Disorder

Any other matters requiring consideration prior to determination of the application

Recommendation

A) APPROVE subject to signing of a S.106 legal agreement to secure safeguarded land around the Lamsey Lane junction for potential future highway improvements for a period of 15 years from the date of decision, and the imposition of the conditions set out in the committee report.

B REFUSE Should the S.106 legal agreement fail to be signed within 4 months of the resolution to approve, on the grounds that it fails to secure the safeguarded land.

THE APPLICATION

Full planning permission is sought for a new destination day visitor attraction including a small, 10 yurt, glamping site.

The site measures c.38ha and comprises an area of existing built form in the farmyard, areas of agricultural fields and rough pasture, and a small part of Ken Hill Wood.

The farmyard will be reconfigured to create a visitor centre through the reuse of existing buildings as well as the creation of new buildings.

The proposal would also result in the demolition of two existing agricultural buildings (an existing modern grain store and 'spray shed' buildings) and a silo.

The attraction would comprise:

- A purpose-built Visitor Building containing a destination café, visitor reception, kitchen, cold store, general store, plant room, staff facilities and flexible space as well as an outside dining area

- A purpose-built Retail Building for local businesses containing toilets and storage
- A purpose-built Muster Point Building
- An indoor play building contained within one of the existing buildings which is to be extended and improved (reclad / new openings) with an outdoor play area adjoining it
- A multi-purpose building contained within the existing stone barns and adjacent agricultural building containing office facilities (existing), bike workshop, bike retail, bike rental and covered bike storage areas, a retail unit (existing), storage / staff room and corporate event space
- Ten yurts; seven on the edge of Ken Hill Woods each benefitting from a kitchen facility (bin store) and two benefitting from private showers. Three yurts are within the woods with each benefitting from a kitchen facility and private WC. A further facilities block is also proposed to serve all the yurts and contains a family shower, showers, WCs, stores and a washing up station
- Wild gardens and outdoor walking, activity trails and dog walking area
- Three bike trails including a family pump track and two loop trails of varying difficulty
- A 210-space car park to include 20 disabled spaces and 2 bus parking spaces, and a 110-space overflow car park
- A site wide landscaping scheme
- New access from Lamsey Lane
- A new off-road permissive path to enhance connectivity between Heacham and Snettisham
- Off-site highway improvement works comprising:
 - Additional signage on Lamsey Lane
 - Provision of a new pedestrian footway connection between the Mount Pleasant site (on-site path) and Heacham (existing highway footway)
 - Provision of a pedestrian crossing of Lamsey Lane between the Mount Pleasant & Heacham Bottom sites
 - Widening (to 3m) of the existing footway provision on the southern arm of the A149 (both sides) and the western side of the B1440 and an improved refuge island crossing at Snettisham Roundabout to safely provide an off-road cycle facility linking the B1440 to the new on-site permissive path
 - Provision of a pedestrian refuge island on the A149 and associated sections of footway to provide a link (and safe crossing) from the development site to the bus stops.

The supporting documentation suggests that as well as a destination, the development will create a hub for the range of educational tours already offered at Wild Ken Hill.

The site is located c.1.1 miles to the north of the village of Snettisham, to the west of the A149 and c.1.4 miles south of the village of Heacham.

SUPPORTING CASE

Please see Wild Ken Hill's original applicant statement below. We are grateful for the many positive comments on our proposals from Councillors at Planning Committee in April. We appreciate why Councillors sought to defer the applications to seek a full update from Norfolk County Council (NCC) on its feasibility study even if the delay was disappointing. NCC continue to support our applications and their most recent response confirms the proposals will have no material impact on the A149 junction. Since the committee meeting, I have met with Ward and Parish Councillors and those who spoke at committee to further understand everyone's views, to commit to continued collaboration, and to align around the idea that these proposals could act as the catalyst for solutions to existing transport concerns. Both of these interdependent and linked planning applications are absolutely crucial to the future of Wild Ken Hill

and following the deferral we hope that Committee will support us to continue our pioneering work.

“Last September, we submitted planning applications for facilities at Wild Ken Hill that will allow us to host, engage, and educate a wider range visitors and residents about the exciting nature restoration work taking place here. Our vision is to allow greater connection with nature and the outdoors at Wild Ken Hill, with sustainable facilities that work for all, including a new off-road path to better link Snettisham and Heacham and new wildflower meadows, as well as a dog walking area with free parking. We truly think the plans will be a great boost for people, wildlife, and climate.

Hopefully everyone at the Council will be aware of the pioneering Wild Ken Hill project which began in 2019 and includes a nationally-unique mix of regenerative farming, rewilding, and traditional conservation practices. We have embarked on these changes as we believe land must be used to benefit wildlife, climate, and people. Many will have seen Wild Ken Hill featured on the BBC’s The Watches. Locally, we have also already created 2 new jobs, 15 volunteering opportunities, opportunities for students, a new nature festival, as well as hosting 2-3 open days for local residents, welcoming children on around 200 days per year, and operating 200+ acres of permissive access.

This, however, is just a start – we are extremely constrained by a lack of facilities. In order to fulfil our vision of fighting climate change and restoring biodiversity across the UK, Wild Ken Hill needs the ability to welcome more visitors for higher quality, longer stays. In addition, several key strands of our land management work – in particular the rewilding project – are only funded until 2028. Developing a sustainable, year-round, nature-based tourism business through these proposals would create certainty for the financial future of this important nature recovery work, and indeed allow us to fund more projects and community engagement work in our local area. Without it, however, the future of the Wild Ken Hill project would be an uncertain one.

As such, the intention behind our proposals at Heacham Bottom Farm and Mount Pleasant is to create a high quality built and natural environment which serves as an exemplar for sustainable, nature-based tourism and education which contribute significantly to the local area. The planning applications are intrinsically linked and interdependent. They have only been made following detailed engagement with Planning, Economic, Highways, and Natural Environment Officers across the Borough Council and other organisations.

We are pleased to note wide ranging support for the proposals. On ecology and landscape, the Norfolk Coast Partnership (NCP) stated “The NCP is supportive of development that serves to help boost the local economy and improve access to and understanding of the AONB whilst protecting and enhancing the special qualities”, and the Norfolk County Council (NCC) Protected Landscapes stated “We believe that the proposals submitted by Wild Ken Hill not only are compatible with the LAC (Limits of Acceptable Change) framework, but could be used as a case study for how the LAC should be applied, and even an exemplar project to showcase externally, including to other developers.” Wild Ken Hill is also pleased to note that Natural England and the RSPB have raised no objections to the proposals.

Regarding tourism, the Borough Council’s Regeneration and Economic Development Team stated: “The Wild Ken Hill proposal demonstrates a focus on sustainable tourism and local environmental issues which has already gained national exposure and interest. The proposal will (therefore) support delivery towards the strategic tourism and economic objectives of the Borough Council.”

Following early-stage and thorough engagement with NCC Highways we are pleased that it offers no objection to the applications. Our proposals include an off-road path which will create

the safest and most direct sustainable route between Snettisham and Heacham, new traffic signage, and improvements to the Heacham Bottom bus stops on the A149. In addition, we will be closing two accesses from the farmyard onto the A149 and removing agricultural vehicle movements.

Officers of the Borough Council and County Council have rigorously assessed our applications. There is no objection to the applications from any technical consultee.”

PLANNING HISTORY

20/00001/FLEXI: Flexi Notification - No Further Action: 14/05/20 - Change of use of agricultural building to a flexible commercial use (Schedule 2, Part 3, Class R)

14/01344/F: Application Permitted: 12/11/14 - Installation of 2No dipole antennas, a 1.8m diameter satellite dish, an equipment cabin and development ancillary thereto including 2No GPS antennas, cable gantry and gantry poles

2/03/1853/F: Application Permitted: 29/10/03 - Erection of general-purpose agricultural grain store

RESPONSE TO CONSULTATION

Heacham Parish Council: OBJECT We applaud Ken Hill in their ReWilding and have supported those projects which have brought much needed tourism to Heacham.

This application would also bring further tourism to Heacham, and the adjacent parish of Snettisham.

Our concerns and objections though are these:

The plan states a 100-vehicle overspill car park, looking at the car park plan around 300 vehicles on-site parking, plus 2 coach spaces. This number of vehicles accessing the site off the A149 to Lamsey Lane, which after 50m is national speed limit (60MPH!) will cause problems at the junction A149 / Lamsey Lane. Although they may not all turn up at once, you can never predict the holiday traffic. Listen to road reports on local radio for the A149. Add to this the second application of Mount Pleasant by Ken Hill, and up to 20 touring caravans plus camping enthusiasts also wishing to traverse this road, it will be a nightmare, not only for visitors to Hunstanton 3 miles north, local residents, who use the road, but the bus service which uses Lamsey Lane to service the whole village via this junction.

We think it will also impact on nature conservation, which Wild Ken Hill is all about, and impact the village. If the queue for the exit and the A149 junction blocks up, drivers will exit left and drive through the village to the controlled junction at the Lavender centre. This is something Ken Hill have said they do not wish to happen.

The current road structure does not support a scheme as presented here.

The transport assessment states no impact as there are bus services and footpaths. Heacham, prior to 1969, used to have a railway link to King's Lynn, but as people bought cars and preferred to drive, this section of the line was closed. Nothing has changed, people still prefer to go out for the day by their own transport.

Anglian Water state that the site is not connected to their system. How will this be dealt with?

Planning Committee
5 June 2023

Business Assessment is needed to show any impact on the village businesses.

Confirmation needed that the Bike Hire business is solely for use on the Heacham Bottom site and will not impact on village hire companies.

The 10 glamping yurts were not mentioned when this scheme was first presented to the Parish Council, only the Bell Tents in the Mount Pleasant application.

Plans are unfortunately considered in isolation, but what also has to be considered here, is that the Plans for the 160 plus dwellings approved for Cheney Hill will also add traffic to A149 / Lamsey Lane junction.

The following statement was submitted as late correspondence at the Planning Committee meeting of 3rd April: This information covers the Transport assessment of both parts of the development Heacham Bottom and Mount Pleasant, as the Transport assessment is for both sites.

Whilst we are objecting to this application as it stands, we don't object to the principle of this development. In fact, we think it will be good for the area; provide awareness of the countryside, promote a healthy lifestyle, be good for tourism and could provide local jobs. Despite this we believe the application as it stands is flawed.

The very busy Village of Heacham really has only two ways in and out. Lamsey Lane and the Norfolk Lavender junction. Due to poorly designed road layouts large vehicles can struggle to enter either of these roads when traffic is queueing to exit.

In its conclusion the applicants Transport Assessment states there is significant spare capacity at the nearby Lamsey Lane / Lynn Road junction, including during high tourist season; If you travel along the A149 from Kings Lynn to Heacham, at weekends and holidays, when this site will be at its busiest, it's common for traffic to be backed up to Knights Hill roundabout. This road to the coast is like a cul-de-sac, it's really the only way in and out.

Turning right out of Lamsey Lane can be challenging on a quiet day due to the speed of traffic, but on a busy day it's a nightmare. This is a main bus route, and they can often struggle to exit safely. The junction needs significant improvement, and we consider this should be funded out of a section 278 highways agreement, by the developer.

I spent 13 years as senior project lead for Cambridgeshire Highways, so I understand the complexities of The Transport Assessment, which is 306 pages long, a complex technical document. It relies completely on collected traffic data for its assumptions and that is data is very wrong.

Two traffic counts were carried out for the Transport Assessment, in July 2021, the last week of school term and the first week of school holidays. However, the DfT's website on Road Traffic Statistics under summary (<https://roadtraffic.dft.gov.uk/>) states that despite a rise from 2020 levels, car traffic for 2021 remained 15.8% lower than before the pandemic and were lower than 2011 traffic levels.

15.8% is a significant amount and this has been effectively ignored as part of the Traffic Assessment and therefore, makes it unsafe to rely upon for its conclusions. Traffic levels have largely returned to normal and are rising at about 2.2% annually so effectively the data could be wrong by as much 20% and has a knock-on effect

throughout the report. It's impossible to see how a safe planning decision can be made on this basis.

In conclusion

We do want this project to go ahead. We believe it offers a worthwhile and beneficial uplift to the area. But only when we understand the road traffic safety implications, and after road safety issues have been properly assessed. Our primary concern is the impact on road safety, something that cannot be assessed with the current information provided and the safety and wellbeing of the community. We would ask that this application is delayed until a Transport Assessment with a corrected dataset is re-evaluated and mitigation measures properly considered.

Snettisham Parish Council: OBJECT At a council meeting on 28th February 2023 Councillors considered the application and resolved to object to the planning application, they were concerned about the safety of pedestrians and other road users at the Lamsey Lane Junction and along routes to Snettisham. It was considered that this junction was already very busy and considered a danger locally. The increase in traffic brought by the development would also cause significant problems on already overburdened local infrastructure.

Highways Authority: NO OBJECTION Further to my previous responses, and subsequent additional information submitted (namely the Highways Positions Statement 4th Issue, received on 12 May 2023), please find below the further comment of the Highway Authority.

As you will be aware, the position statement has been produced following consideration of the proposals at KLWNBC Planning Committee where the application was deferred following concerns raised, in late correspondence, by Heacham Parish Council. The primary focus of the concerns, raised by the Parish Council, were in relation to the assessment, submitted within the Transport Assessment (TA), and the validity of the data used.

A149 / Lamsey Lane junction

To address these concerns and robustly assess the likely impact of the proposals the applicant has produced a Highways Position Statement. Within the statement the applicant has provided comment regarding the methodology and data (which was collected during the easing of the COVID lockdown) to explain and provide assurance regarding the validity of the initial assessment.

In addition, the applicant has undertaken additional ATC surveys (between 24 April and 3 May 2023) and further junction analysis to robustly assess the development proposals.

The latest ATCs identified Saturday peak hours as 11-12 and 14.30-15.30 and identified an increase in traffic flow on Lynn Road and a decrease on Lamsey Lane (when compared to the previous traffic counts). The differences are not considered to be significant and would be expected as daily variances.

As with the initial analysis, variance factors and seasonal uplifts have been applied to the 2023 data, and the associated junction traffic modelling carried out which identified no material change.

I can confirm that the County Council officers have fully considered the concerns of the Parish Council (outlined within their late correspondence) and assessed the additional information provided. It remains our professional opinion that there will be no material impact, at the junction, as a result of the proposals. As such, we

acknowledge that we could not substantiate an objection on these grounds to the application.

Whilst it would not be reasonable to seek significant improvement to the junction as a direct result of the current application, you will be aware that NCC has recently carried out a feasibility study at this location. The feasibility study, which has now concluded, looked at various traffic light and roundabout options, some with bus priority measures, to improve traffic flow at the junction. The simpler forms of these are lower-cost and require less land take. Whereas the roundabout option, which is a typical solution for principal road junctions, would involve higher costs (estimated in to be in the region of £2.5 -4.7 million) and involve more land take.

For the avoidance of doubt, I would then add that, although NCC have already secured BSIP funding of around £300,000 for improvements to this junction, both signalised and roundabout, the options identified would cost in excess of this amount to implement. They would therefore require the securing of further funding which at this stage has not been sought.

You will of course be aware however that, should the current planning applications be approved, the applicant has guaranteed that the land required to implement the above improvements would be available for the next 15 years.

For your information, I have attached a copy of the summary of feasibility findings to this response.

The applicant has however proposed some low-key signing improvements on Lamsey Lane (see section 5 within the TA) on the approach to the A149 junction.

Offsite Highway Improvements

As you will be aware from my previous responses, the applicant is proposing a series of offsite highway improvements to enhance the sustainable links to the site for pedestrians, cyclist and bus users.

The proposals, as outlined, offer a mixture of a day visitor attraction and glamping. Given the extensive attractions and cycle routes on site, it is reasonable to assume that guests / visitors would wish to cycle to / from the site.

The sustainable links to the site has been a key consideration for the Highway Authority (HA) throughout all informal discussions with regard to the site. Whilst it is of course acknowledged that the development represents rural tourism, it is essential that sustainable links to the site are actively encouraged and maximised.

In support of the application, a new permissive path will be provided to create an off-road link between the settlements of Heacham and Snettisham, via the Heacham Bottom site.

The applicant has outlined that this path, which links Heacham to Snettisham, would be a permissive path and open / available for the public to use.

The permissive path and continuous links from / to both sites from Heacham & Snettisham are considered essential to make the proposals acceptable.

Pedestrian Links to Heacham To ensure that suitable links to the existing footway network in Heacham are available, a new short section of 2m wide footway link (see

Drawing 026 (within the TA addendum)) from the existing highway footway provision in Heacham to the onsite paths within the Mount Pleasant site will be provided.

This link is not only required to make this application acceptable but also in association with the proposals at Mount Pleasant (22/01650/FM).

Whilst the drawing is only indicative and would of course be the subject of detailed design considerations & Safety Audit should planning permission be approved, this does look to be acceptable in principle (for planning purposes) to allow this to be conditioned and provides confidence that this a realistic expectation that such a facility could be provided with existing highway / land under the applicant's control.

It should be noted that the alignment of the onsite path (shown on the landscape master plan (WKH-DIG-00-PL-0001)) will need to be amended to ensure that it meets with the new highway footway proposed.

Pedestrian / Cycle Links to Snettisham To link the onsite path to Snettisham the applicant is proposing a scheme of improvements to the existing footway and crossing at the A149 / B1440 roundabout.

The scheme, which is indicatively outlined on drawing 2021-F-015-030 Rev B, would involve widening the existing footway provision on the southern arm of the A149 (both sides) and the western side of the B1440 and an improved refuge island crossing at the roundabout. This would safely provide an off-road cycle facility linking the B1440 in Snettisham to the new on-site path.

Whilst the exact details would of course be the subject of S278 detailed design considerations (and a full Safety Audit) should pp be approved. We are satisfied that there is sufficient available highway to widen the existing footway (either into the verge of carriageway) and island to agree these improvements in principle at this stage.

For the avoidance of doubt however we have confirmed that, as part of the detailed design process, we will look to ensure that a minimum 2.5 m width is secured for the enhanced crossing and that we would seek to secure that the cycle facility is wider on the approaches to the crossing.

Links to the site by Bus To provide safe access to / from the site by public transport, the applicant is proposing to significantly improve the pedestrian links to the existing bus stops on the A149.

At present, a pair of bus stops exist on what is a fast section of the A149, which as demonstrated carries significant volumes of traffic particular during the tourist peaks. Clearly given the level of tourism within the immediate vicinity, it is reasonable to assume that visitors would travel to the site by bus from both the north and the south, so access to the bus stops should be both safe and attractive.

It is proposed to provide a pedestrian refuge island on the A149 and associated sections of footway to provide a link (and safe crossing) from the development site to the bus stops - as indicatively outlined on drawing 2021-F-015-029 Rev A.

As with the other offsite highway mitigation, the exact details would of course be the subject of S278 detailed design considerations (and a full Safety Audit) should planning permission be approved.

Vehicular Access to site It is proposed to serve the site via a main visitor access from Lamsey Lane. I can confirm that, having considered the submitted drawings, suitable levels of visibility are proposed in line with the recorded vehicle speeds.

It should be noted that all other existing vehicular accesses into the site will need to be permanently closed and reinstated.

Considering the above, the highway authority recommends no objection subject to the conditions relating to: - construction traffic management including construction routes, construction, construction worker parking, off-site highway improvement works, new access provision and specification including gradient / visibility splays and closure of other accesses, means of obstruction, parking / cycle parking, loading / unloading, serving, etc., being appended to any permission granted.

PROW: NO OBJECTION Further to the applicant obtaining a Highway Boundary plan of the legal alignment of the Public Right of Way, known as Heacham Footpath 15, we are now content to remove our holding objection.

It should be noted that no structures are permitted within the legal alignment of the public right of way as this would constitute an illegal obstruction. The full legal extent of this footpath must remain open and accessible for the duration of the development and subsequent occupation.

Natural England: NO OBJECTION SUBJECT TO APPROPRIATE MITIGATION BEING SECURED.

It should be noted that Natural England amended their advice in relation to necessary mitigation when taking this site in isolation via an email received on 10 March. The below outlines the amended advice.

We consider that without appropriate mitigation the application could have an adverse effect on the integrity of:

- The Wash and North Norfolk Special Area of Conservation (SAC)
- The Wash Special Protection Area (SPA)
- The Wash Ramsar
- North Norfolk Coast SPA
- North Norfolk Coast Ramsar
- Damage or destroy the interest features for which the following Sites of Special Scientific Interest (SSSI) have been notified
 - The Wash Site of Special Scientific Interest (SSSI)
 - North Norfolk Coast SSSI

In order to mitigate these adverse effects and make the development acceptable, the following mitigation measures are required to be secured:

- A financial contribution to be paid into the Norfolk Green Infrastructure and Recreational Avoidance Mitigation Strategy (GIRAMS).
- Provision of leaflets to all visitors and provision and maintenance of permanent information boards within the site indicating nearby public rights of way and alternative visitor attractions not in the proximity of designated sites, as well as the details of nearby designated sites and recreational pressures upon them.

We advise that an appropriate planning condition or obligation is attached to any planning permission to secure these measures.

Natural England's further advice on designated sites/landscapes was also given with an acknowledgement that whilst the Appropriate Assessment was not produced by the LPA they [NE] considered it was acceptable for the LPA to adopt it to fulfil our duty as competent authority.

PROTECTED LANDSCAPES: ...We advise that you consult the relevant AONB Partnership or Conservation Board. Their knowledge of the site and its wider landscape setting, together with the aims and objectives of the AONB's statutory management plan, will be a valuable contribution to the planning decision.

Norfolk Coast Partnership: NO OBJECTION The proposal covers two sites, Heacham Bottom and Mount Pleasant.

The AONB falls partially to the south of the former area where wildflower meadows, bike trails and a small glamping area of 7 yurts skirting the woodland edge and 3 inside the woodland along with service huts are proposed.

The proposal to the north of the AONB seeks to retain and reuse some of the more traditional buildings as well as removal of the grain store which will be replaced by a building of lower profile. As well as this, proposals include a visitor centre, retail space, an indoor play centre, bike hire and further camping to the north of the Mount Pleasant area.

Although much of the development is technically outside of the AONB boundary, it is close enough to it to have a direct impact.

NPPF para 176 states that 'Great weight should be given to conserving and enhancing landscape and scenic beauty in National Parks, the Broads and Areas of Outstanding Natural Beauty which have the highest status of protection in relation to these issues. Development within their setting should be sensitively located and designed to avoid or minimise adverse impacts on the designated areas'.

The Landscape and Visual Appraisal has demonstrated that visual impact is relatively contained due to the site's topography, tree cover and hedgerows which all help to minimise impact. Much of the development reuses existing buildings and the Visitor Building and Retail Unit will replace a higher profile grain store which will lessen visual impact to an extent. The land in the AONB falls in the Wooded Slopes and Estate land character area.

There will be an increase in movement on site as well as associated works such as car parking, lighting, noise, and other structures associated with the development. This needs to be considered in the wider context.

The AONB has for a while seen a proliferation of small isolated camping / holiday sites which cumulatively has a big impact on the designation and the social and economic impact does not outweigh the environmental impact to the landscape. Here the focus has been on making a contribution to the enhancement of the landscape and the wider aims of the estate in its sustainable farming practices and rewilding project helping to meet CS12 and para 176 of the NPPF. There is a net gain of 27.37% in terms of habitat creation on site as well as access improvements both on site and by way of creation of a new path that will link Snettisham and Heacham. 24 new jobs will be created, the complex will provide informal and formal recreational areas and there is an environmental educational element in conjunction with the wider work on the estate. The Norfolk Coast Partnership is supportive of development that serves to help boost the local economy and improve access and understanding to and of the AONB whilst protecting and enhancing the special qualities.

DM 11 states 'Small scale proposals for holiday accommodation will not normally be permitted within the Norfolk Coast Area of Outstanding Natural Beauty (AONB) unless it can be demonstrated that the proposal will not negatively impact on the landscape setting and scenic beauty of the AONB or on the landscape setting of the AONB if outside the designated area'.

It is my opinion that glamping site just inside the AONB at Heacham Bottom will be adequately screened being inside and alongside the woodland edge effectively hidden from view unless inside the contained glamping area. Taking into account the habitat enhancement element of the wider site, this would compensate for any small-scale disturbance here.

Strategically the wider site will serve as accessible green infrastructure to entice people away from the more sensitive sites on the coast in the AONB where visitor pressure can damage habitats and disturb species. The Appropriate Assessment has scoped out impact to designated sites and it may be that the site has the potential to lessen impact and help to educate people about this special area through resources and on-site interpretation and comms.

Care needs to be taken particularly in the new visitor and retail building that glazing does not increase light pollution on site which would impact the AONB dark skies, a special feature of the AONB. This can be achieved through smart glazing, recessed openings or a reduction in large areas of glazing (such as the retail building SE elevation). All external lighting on site should be conditioned.

The car park could also benefit from further screening through planting to mitigate visual impact. Again, any lighting here will need to comply with the above suggested condition.

LLFA: NO OBJECTION I can confirm that the County Council as Lead Local Flood Authority (LLFA) has no comments to make.

Internal Drainage Board: NO OBJECTION Having screened the application, the site in question lies outside the Internal Drainage District of the King's Lynn Internal Drainage Board and as per our Planning and Byelaw Strategy the proposed application does not meet our threshold for commenting. Therefore, the Board has no comments to make.

Anglian Water: NO OBJECTION Having reviewed the development, there is no connection to the Anglian Water sewers, we therefore have no comments.

Historic Environment Service: NO OBJECTION In broad terms we concur with some of the conclusions of the Heritage Statement and archaeological desk-based assessment. There is potential for previously unidentified heritage assets with archaeological interest (buried archaeological remains) to be present within the current application site and that their significance would be affected by the proposed development.

If planning permission is granted, we therefore ask that this be subject to a programme of archaeological mitigatory work in accordance with National Planning Policy Framework (2021) that should be secured by condition.

Environmental Health & Housing – Environmental Quality: NO OBJECTION

Air Quality: As previously explained the concern in terms of air quality from additional traffic arises when the changes in daily traffic movements (as 24-hr Average Annual Daily Traffic or AADT) are significant and in excess of IAQM EPUK (2017) indicative criteria. The transport assessment had suggested that changes in traffic would be significant during the summer months (March-Aug) and a business case based on car park capacity of 320 spaces with average dwell time based on 4-hrs turn-around.

In the absence of an Air Quality Management Area (AQMA) adjacent to the site, IAQM EPUK (2017) refer to significant traffic as around 500 light duty vehicles per day. The applicant has explained that additional traffic will be around an average of 408 AADT as a result of these developments. Based on the background air quality levels as quoted and absence of an AQMA in this area this is not sufficient to warrant a more detailed assessment for the changes to air pollution occurring in the area. I would therefore have no objection to this part.

We however mentioned that the principles of minimising emissions according to best practice apply to all developments, and especially, the larger major applications as in this case as set out by IAQM. We explained that whilst the development does not fall within a Smoke Control Area there still can be matters that are of material concern especially where they are not controlled elsewhere. Smoke emissions can be controlled via condition.

Finally, we did comment on electric vehicle (EV) charging infrastructure for the additional car-parking spaces including the 320-space car park with average dwell times quoted as around 4hrs. Norfolk CC parking guidelines refers to EV charging in such scenarios to be based on dwell time and travel distances i.e., it is for the applicant to define this. The information that has been provided within the air quality assessment only refers to the minimum will be provided. To ensure there is adequate EV charging and cable routes where necessary to meet the needs of all users and help future proof this development towards ultra-low emission vehicles further information is necessary. This can be suitably conditioned.

Contaminated Land: The application is for the demolition of a silo and 2 farm buildings and construction of replacement building along with a change of use to visitor centre, cafe, event and retail space, indoor play building, bike hire service, bike tracks, glamping units and associated works.

The applicant has provided a design and access statement and drawing package which provided information on the proposal and illustrates the works. A Phase 1 Geo-environmental Assessment by pwa geo-environmental dated July 2022 is also provided which states the risk of contamination to be low but indicates the potential from a previous chemical and fertiliser store and diesel tanks. The report recommends further investigation to better characterise the site and attempt to reduce the risk to very low.

We have reviewed our files and the visitor section of the proposed site is on land that has been developed for the duration of our records first seen labelled as Horsewell farm. The other areas of the site are not seen developed on our records. The surrounding landscape is largely agricultural with some residential properties.

Following the findings of the Phase 1 Geo-environmental Assessment we recommend the full suite of contamination conditions, and due to the age of the properties on site there is the potential for asbestos containing materials to be present. With this in mind we also recommend an informative relating to Asbestos.

Arboricultural Officer: NO OBJECTION No trees are to be removed, and therefore I have no objection.

Fire Safety Carrow Fire Station, Norwich: NO OBJECTION Detailed correspondence submitted relating to Building Regulations.

Norfolk Fire and Rescue: NO OBJECTION With reference to the proposed development, based on the location and infrastructure already in place and the type of building proposed, our minimum requirement is for 1 fire hydrant on potable water main.

Norfolk Constabulary: NO OBJECTION Detailed information sent to the applicant in relation to how to meet Secured by Design standards.

RSPB: NO OBJECTION The Royal Society for the Protection of Birds (the RSPB) is a registered charity that takes action for wild birds and the environment. We are the largest wildlife conservation organisation in Europe with a membership of over one million. The principal objective of the RSPB is to save nature.

Information in Support of Habitats Regulations Assessment and Shadow Habitats Regulations Assessment – RSPB comments

In our letter dated 26 October we suggested several lines of enquiry for further investigation. We are happy that these enquiries have been addressed and we have spoken to Wild Ken Hill about habitat enhancement for non-breeding birds such as curlew, a species which they support through habitat management of their own wetland and farmland, but also via the curlew head-starting programme and the range of valuable conservation activities associated with that project. We are also pleased to see further information about mitigation options and a commitment to making a contribution to the Green Infrastructure and Recreational Impact Avoidance and Mitigation Strategy (GIRAMS) in the Shadow HRA.

We understand it is likely that Wild Ken Hill will be required to create a Landscape Management Plan and the RSPB would be pleased to consult with them on any habitat creation and enhancement aspects, including any possible additional mitigation measures, as they finalise a detailed Landscape Masterplan.

As competent authority, it is the Borough Council's responsibility to assess the findings of the assessment and to make its own conclusions regarding the likelihood of significant effects arising from the proposal, either alone or in-combination. With appropriate mitigation in place, the RSPB is satisfied that the proposals are not likely to result in significant adverse impacts on IIVSs.

NCC Minerals: NO OBJECTION While the site is partially underlain by a safeguarded mineral resource (carstone), due to the nature of the proposed development it is considered the application would be exempt from the requirements of Policy CS16-safeguarding of the adopted Norfolk Minerals and Waste Core Strategy.

NCC Protected Landscape Team (PLT): SUPPORT The Protected Landscapes Team at Norfolk County Council works to restore, enhance, protect and sustainably promote designated areas across Norfolk. We believe environmental protection and thriving rural communities and economies can exist side by side; and are supportive of sensitive and sustainable development which can bring net gain to each. We also work to create new opportunities for people of all abilities and backgrounds to access Norfolk's unique nature and culture, and the associated health and wellbeing benefits.

Additionally, comments relating to a Limits of Acceptable Change (LAC) study were also submitted of which the PLT concludes that *Due to the numerous ways in which the proposals submitted by Wild Ken Hill are consistent with the LAC framework, I believe there is potential to showcase these proposals and the wider Wild Ken Hill project as an exemplar for the LAC, demonstrating to external stakeholders and other developers how it should be interpreted and implemented. I have discussed the above with Wild Ken Hill, who have indicated they would be keen to collaborate on this.*

Furthermore, PLT acknowledges that the proposals are aligned to several local sustainable tourism initiatives and concludes with support for the two planning applications submitted by Wild Ken Hill on the basis that they represent an exemplar for the new LAC framework, are a

model for the future of sustainable tourism, and would enhance sustainable all abilities access to the area.

CPRE: OBJECT Despite supporting most aspects of this proposal and those for neighbouring Mount Pleasant Farm, along with the overarching intentions of the Wild Ken Hill Project in terms of nature and landscape restoration and recovery, CPRE Norfolk objects to this planning application as approval would go against various policies within the adopted Local Plan, Heacham Neighbourhood Plan and the National Planning Policy Framework (NPPF). The elements of this application which CPRE Norfolk objects to are the glamping site and the access/traffic impacts.

REPRESENTATIONS

King's Lynn and West Norfolk Bike Users Group (KLWNBUG): OBJECT While KLWNBUG The Norfolk and Fens Cycling Campaign is generally supportive of this application and especially the link between Snettisham and Heacham passing a bike shop, cafe and visitor centre, regrettably we object to the dangerously substandard layout of the Snettisham crossing point shown on drawing 2021-F-015-030 B dated 27 Feb 2023. We refer everyone to Local Transport Note 1/20 in the following: <https://www.gov.uk/government/publications/cycle-infrastructure-design-ltn-120>.

In particular:

1. The refuge point on the A149 island is only 2m long when the Cycle Design Vehicle (Local Transport Note 1/20 Table 5-1) is 2.8m long.
The 2m short refuge point means that many tandems, cargo bikes or child trailers would be unprotected, projecting into a carriageway while waiting to cross, where they could easily be struck by impatient or width-misjudging motorists. That is dangerous. Even the rider of a common 1.8m bicycle would need to judge their braking exactly and have good balance to stop themselves exactly on a 2m island with dipped kerbs front and back.
2. The turns onto the crossings have zero inner radius and maximum 3m outer radius. The Cycle Design Vehicle has 2.5m inner radius and 3.2m outer radius and the tighter radii mean it would not be possible for wider or longer cycles to enter the crossings at right angles and riders would have to perform a cumbersome S-turn mid-crossing. This is uncomfortable and requires extra time to cross, increasing the crossing hazard. This also would limit the capacity of the crossing and mean people cannot easily cross in both directions at once while keeping left, which is contrary to Summary Principle 5 of LTN 1/20 (page 10).
3. People riding northbound would approach the crossing of the A149 with their back to the traffic flow they are about to cross. This is obviously dangerous due to the reduced intervisibility.
4. People riding southbound would approach the crossing of the B1440 with their back to the traffic flow in the far lane that they need to merge into. This is also dangerous due to the reduced intervisibility.

Ideally, in line with Norfolk County Council's public commitment to follow LTN 1/20 in all its highway developments, we will see a layout like Figure 10-37 used. There seems plenty of space in this roundabout area for such a layout. Exit cycle lanes to the A149 could be omitted to encourage people to cycle on the B1440 and the new route, until such time as the A149 is upgraded.

As a minimum, the crossings could be moved near the roundabout ends of the splitter islands to remedy the refuge problems (1 and 4), and the crossing approaches realigned to enable perpendicular entry angles and remedy problems 2 and 3. Showing the vehicle tracks of two

Cycle Design Vehicles riding opposite directions through the junction would confirm that the alignments are correct and crossings wide enough. The crossings should be painted to encourage motorists not to stop blocking them.

Please, bring this design up to current standards and don't curse this exciting new development with a dangerous 1990s-style crossing complex.

Additionally, **20 letters of OBJECTION / CONCERN** have been received from third party representatives. The reasons for refusal can be summarised as:

- Direct contravention with the recently adopted Heacham in relation to holiday accommodation and AONBs
- Car parking, glamping, bike trails etc. are nothing to do with rewilding and everything to do with making money
- Why are there two applications, is one a Trojan Horse for the other?
- Destroying a field for car parking, where earlier in the year sheep were grazing there is unacceptable, and will become an eyesore
- The entrance will become a bottleneck
- Lamsey Lane is a blackspot for accidents
- The number of visitors anticipated is frightening and will increase congestion and queuing in the locality
- How will Lamsey Lane be crossed? It is a 60MPH road
- Chris Packham's support (Springwatch) is not a valid reason for more destruction of the countryside. How can this development be beneficial to wildlife?
- The rewilding image seems to be turning into that of a theme park
- Ken Hill already have a car part at South Beach which already brings day-trippers / holidaymakers to the village and has increased traffic within this area substantially
- The development is bound to affect local struggling businesses
- The offer of signs to warn people of queues is not going to prevent the queues from forming
- Alternative access should be considered
- The assertion in their conclusion (p34) by the Ken Hill highway consultants that 'the highway has a good safety record' is disingenuous
- Why have the applicants not included a roundabout or traffic lights at the junction or Lamsey Lane with the A149?
- Why haven't bus companies been consulted? The development will definitely have an impact on them
- Have wider traffic considerations been taken into account e.g., congestion at both the Norfolk Lavender junction and the more northerly Church Lane crossing?
- A more robust and independent Traffic Study is required
- The Historic Environment Officer's comments need to be strictly adhered to
- There is much mention of bicycling facilities on the site, but no mention of a bike track through the sites to enable bicyclists to avoid having to travel along the A149
- An increase in visitors will inevitably add infrastructure pressure on doctors and other essential services
- The development would increase traffic into Heacham
- **Unlikely investors will be interested in a project that only attracts 80,000 visitors**
- **Grave doubts about the accuracy of the applicant's figures, for example the 24 FTE employees and the turnover, which is unrealistic**
- **Must be subject to a S.278 agreement so the developers make a contribution to the subsequent costs of future improvements, including a financial contribution**
- **Talk of traffic lights and a roundabout for years, and any improvements will also cause further delays on the A149**
- **Developers should provide traffic lights or a roundabout.**

The following letter of **SUPPORT** has been received from The Regeneration and Economic Development Team of the BCKLWN: *[we] welcome proposals which will support the development of a green tourist offer further in West Norfolk. The proposals align with the west Norfolk Tourism Development Plan 2022-2026 (draft) strategic aim to support west Norfolk sustainability for future visitor footfall and responsible tourism.*

The proposals are also aligned with the West Norfolk Investment Plan (2021) priorities:

- *Leading as a centre of Excellence for the Visitor Economy.*
- *Embedding approaches that are active, clean and green.*

The proposal will therefore support delivery towards the strategic tourism and economic objectives of the Borough Council.

The following letter of **SUPPORT** has been received from the EXPERIENCE project: *Wild Ken Hill has actively engaged with our project and has been the first enterprise to submit experiences to the project.*

Wild Ken Hill works on developing a strong connection between people and nature, providing visitors with a unique experience while protecting the environment. Some activities are tailored to be best experienced during the low season, and the focus on cycling is of interest to the project. All this impacts positively the sustainability of tourism in Norfolk and echoes the founding principles of our project.

Through the EXPERIENCE project we look to support businesses develop off-season sustainable experiences. Our aim is to increase the number of visitors in Norfolk, and support a year-round visitor economy, providing lasting benefit for the local economy, our environment and the community.

We believe the application put together by Wild Ken Hill will go towards helping our project achieve those goals and help Norfolk be more sustainable.

We are committed to supporting any tourism business wishing to become more sustainable, and this letter confirms this.

Conversely (to the previous objection from KLWNBUG), members of KLWNBUG The Norfolk and Fens Cycling Campaign were broadly supportive of the visitor centre, cafe, event and retail space, bike hire service and off-road path, but requested that the calculations of cycle parking spaces meet parking standards and that the off-road path to Snettisham should be an all-weather surface.

Additionally, 18 letters of **SUPPORT** have been received from third party representatives. The reasons for support can be summarised as:

- The development would grow Wild Ken Hill's ambitious nature restoration programme by providing the project with diversified, sustainable long-term income and supporting them to engage and educate visitors
- These developments are required to enable Wild Ken Hill to keep delivering its ambitious programme of work
- The development would be a great additional to the local area by providing greater access to green space, providing educational benefits and jobs as well as supporting long-term aims to restore nature and flight climate change

- All local businesses both in retail and hospitality can only benefit from having such an amenity on their doorstep, and as such the communities will benefit too
- The developments would allow connectivity between Heacham and Snettisham
- Wild Ken Hill is a jewel in the crown of West Norfolk and should be supported
- The development would be an asset to Heacham
- Although it is possible that the Lamsey Lane junction could become busier, we also have to think about the positive impacts on our roads. For one, there won't be any more agricultural traffic originating from the farmyard which can be dangerous. The scheme would also take visitors off the A149 earlier, releasing pressure on the road network further around the coast
- to have a general public that behaves respectfully in the countryside we first need to educate and connect them with nature, and I think that's exactly what these proposals seem to do
- The bike trails will provide youngsters and adults with an opportunity to challenge and improve their cycling skills in a safe off-road environment. Hopefully this will discourage local youngsters from attempting dangerous stunts on the public roads
- Wild Ken Hill has played an active part in several conservation projects including the Plovers in Peril, a project to reverse the decline in the breeding of this red-listed species
- Future public funding through Environmental Land Management Schemes remains very uncertain. There is a fear that by 2028 public funding initiatives for sites such as Wild Ken Hill may end or have been significantly reduced; as such developments such as this are very important
- Many of the aspirations of A Green Future: Our 25 Year Plan to Improve the Environment, launched by DEFRA in 2018, would be delivered through these proposals
- Only a relatively small area of the proposed development falls within the Norfolk Coast AONB. There is provision for 10 glamping units, which is a fully reversible part of the development proposal
- Overall the proposal sets out a reduction in the built footprint. The existing grain store building, which is of significant scale and elevation, will be demolished and a replacement building with a lower profile enhancing the quality of the landscape, with improved views to and from the site will be constructed
- The most significant development proposal in the masterplan is the construction of a new car park for some 320 cars. With modern construction techniques and SUDS requirements, it is hoped that this facility, essential in terms of accommodating visitors, will be designed to a high specification ensuring it ultimately blends in with the surrounding agricultural landscape
- The provision of a footpath linking Heacham to Snettisham is a welcome addition for local residents. It is unfortunate however that the proposal doesn't extend the proposed footpath further south along the A149 by-pass to link with the designated right of way Snettisham FP4a. This would create a long circular walk between the two villages
- This proposal would be good for the local community, attracting tourism and therefore benefiting the local economy.

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS10 - The Economy

CS11 – Transport

CS12 - Environmental Assets

CS13 - Community and Culture

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

DM2 – Development Boundaries

DM9 - Community Facilities

DM10 – Retail Development

DM11 – Touring and Permanent Holiday Sites

DM15 – Environment, Design and Amenity

DM17 - Parking Provision in New Development

DM19 - Green Infrastructure/Habitats Monitoring & Mitigation

NEIGHBOURHOOD PLAN POLICIES

Heacham Neighbourhood Plan

Policy 5: Design Principles

Policy 9: Holiday Accommodation

Policy 11: Green Infrastructure

Policy 12: Provision of Electric Vehicle Charging

Policy 13: Dark Skies

Policy 14: Community Facilities

Policy 15: Settlement Breaks

Policy 18: Road Up-Grades and Improvements

Snettisham Neighbourhood Plan

Policy NP05 – Materials and Design

Policy NP09 – Natural Environment

Policy NP10 - Transport

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

Environmental Impact Assessment
Principle of development
Form and character and impact on the AONB
Highway safety
Impact on neighbour amenity
Ecology / Biodiversity
Drainage
Crime and Disorder
Any other matters requiring consideration prior to determination of the application

Environmental Impact Assessment (EIA):

The development falls within Schedule 2 of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (as amended) as consisting of tourism and leisure uses exceeding 1ha. Whilst the land take is extensive, in this instance the majority of the area will remain devoid of any / extensive operational development and the amount of new / retained buildings (excluding the 10 yurts) is less than existing. No new or converted buildings will be taller than existing. The car park is the largest engineered space, but this is ground level with ample areas of separation to enable landscaping. In summary whilst the land take is substantial the development itself is not intensive in relation to operational development.

EIA thresholds suggest further consideration of tourism and leisure development are required when the threshold of visitors is in the region of 250,000 visitors per year where impacts on ecosystems and transportation routes could be significant. In this regard the development is well below this, generating 60,000 per annum in the first five years, growing to 80,000 in year five and incrementally from then. As such, in terms of EIA, these impacts (ecosystems / transportation) do not need further investigation via the EIA route and can be fully considered as part of the planning application.

The development was screened both in isolation and in combination with the proposed development at Mount Pleasant and the Cheney Hill residential developments.

The in-combination impacts were likewise ruled out as all four developments fall below threshold.

The development was therefore not considered to be EIA development for the purposes of the Regulations.

Principle of Development:

The application falls under the 'banner' of both farm diversification and rural tourism. Both of which are supported at national and local level. Other relevant policy and guidance primarily revolves around protecting the natural environment (AONB and countryside), although other

issues such as highway safety and residential amenity are obviously key material considerations too.

The NPPF covers the rural economy at paragraphs 84 and 85:

84. Planning policies and decisions should enable:

- a) the sustainable growth and expansion of all types of business in rural areas, both through conversion of existing buildings and well-designed new buildings*
- b) the development and diversification of agricultural and other land-based rural businesses*
- c) sustainable rural tourism and leisure developments which respect the character of the countryside and*
- d) the retention and development of accessible local services and community facilities, such as local shops, meeting places, sports venues, open space, cultural buildings, public houses and places of worship.*

85. Planning policies and decisions should recognise that sites to meet local business and community needs in rural areas may have to be found adjacent to or beyond existing settlements, and in locations that are not well served by public transport. In these circumstances it will be important to ensure that development is sensitive to its surroundings, does not have an unacceptable impact on local roads and exploits any opportunities to make a location more sustainable (for example by improving the scope for access on foot, by cycling or by public transport). The use of previously developed land, and sites that are physically well-related to existing settlements, should be encouraged where suitable opportunities exist.

Core Strategy Policy CS06 states: *The strategy will be supportive of farm diversification schemes and conversion of existing buildings for business purposes in accordance with Policy CS10 providing any proposal:*

- meets sustainable development objectives and helps to sustain the agricultural enterprise*
- is consistent in its scale with its rural location*
- is beneficial to local economic and social needs*
- does not adversely affect the building and the surrounding area or detract from residential amenity.*

Core Strategy Policy CS10, where it relates to tourism, states: *The Council will promote opportunities to improve and enhance the visitor economy:*

- Supporting tourism opportunities throughout the borough*
- Promoting the expansion of the tourism (including leisure and culture) offer in Hunstanton to create a year-round economy*
- Smaller scale tourism opportunities will also be supported in rural areas to sustain the local economy, providing these are in sustainable locations and are not detrimental to our valuable natural environment.*

The Council will permit the development of new tourism accommodation in rural areas subject to the following criteria being met:

- It should be located in or adjacent to our villages and towns*
- It should be of a high standard of design in line with national guidance*
- Will not be detrimental to the landscape*
- Mechanisms will be in place to permanently retain the tourism related use.*

The application falls under the 'banner' of both farm diversification and rural tourism. Given that agricultural enterprises are more often than not, not adjacent to the towns and villages there is some conflict between these two policies. However, taking a pragmatic approach as

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to how these two policies interrelate, it is considered that the development accords with Policy CS10 in so far as tourism as it relates to farm diversification is concerned.

Development Management Policy DM2 allows development within the countryside where it complies with Core Strategy Policies CS06 and CS10.

Development Management Policy DM11 states: *Proposals for new holiday accommodation sites or units or extension or intensification to existing holiday accommodation will not normally be permitted unless:*

- *The proposal is supported by a business plan demonstrating how the site will be managed and how it will support tourism or tourist related uses in the area*
- *The proposal demonstrates a high standard of design in terms of layout, screening and landscaping ensuring minimal adverse impact on visual amenity and the historical and natural environmental qualities of the surrounding landscape and surroundings*
- *The site can be safely accessed*
- *It is in accordance with national policies on flood risk*
- *The site is not within the Coastal Hazard Zone indicated on the Policies Map, or within areas identified as tidal defence breach Hazard Zone in the Borough Council's Strategic Flood Risk Assessment and the Environment Agency's mapping.*

Small scale proposals for holiday accommodation will not normally be permitted within the Norfolk Coast Area of Outstanding Natural Beauty (AONB) unless it can be demonstrated that the proposal will not negatively impact on the landscape setting and scenic beauty of the AONB or on the landscape setting of the AONB if outside the designated area. Proposals for uses adversely affecting Sites of Special Scientific Interest (SSSIs) or European Sites will be refused permission.

Policy DM11 also requires certain restrictive conditions relating to holiday use.

In relation to the points raised above the LPA responds as follows:

1. A suitable business plan accompanied the application
2. The re-use of some buildings and design of new buildings (to reflect the agricultural nature of the locality) along with landscaping that will be conditioned if permission is granted suggests that the development would not have an unacceptable impact on the locality
3. The local highway authority raises no objection in relation to safe access or highway safety in general subject to conditions
- 4 and 5. The site does not lie in an area at risk of flooding or within the Coastal Hazard Zone.

The Business Plan suggests the operation of the site will be a family-run enterprise. The family will work with experienced, preferably local staff and operators to run the experience day to day, under the leadership of the General Manager. The operational team will work closely with the existing farm manager who plays a key role in managing the wider land holding and associated operations.

The operating model is for a pay to access visitor experience, predominantly open from 9am to 6pm, with some occasional evening events. These hours and the number of evening events would be suitably conditioned if permission were granted.

The opening hours and days of the week will be dictated by the seasons with the visitor centre being open 7-days a week during the summer months and school holidays.

Visitor numbers will be controlled via pre-sales and the pre-booking of accommodation.

A small area of the site falls within the AONB; this is the area to the south, where the yurts are proposed. Some of the trails extend into this area too, as does the permissive footpath (that will be discussed later in this report.) It is not considered these elements would negatively impact on the landscape setting and / or scenic beauty of the AONB. This is supported by the Norfolk Coast Partnership's comments. This is considered more fully later in this report.

It is therefore considered that the proposed development accords with Policy DM11 of the SADMP.

Both Heacham Neighbourhood Plan (HENP) and Snettisham Neighbourhood Plan (SNP) are adopted and therefore form part of the Development Plan and must be given substantial weight in the decision-making process.

The site falls within both parishes, with the southern element, including the yurts, southern trails and the permissive footpath, falling within Snettisham and the remainder falling within Heacham. Both plans have been given due consideration, although if they differ, weight is given to the plan in which the area falls.

Heacham Neighbourhood Plan

Heacham Neighbourhood Plan Policies, relevant to this application, are Policies: 5 (Design Principles), 9 (Holiday Accommodation), 11 (Green Infrastructure), 13 (Dark Skies) 14 (Community Facilities) and 17 (Settlement Breaks).

Taking each in turn:

Policy 5: Design Principles is a general policy with 18 criteria, some of which are relevant to this application. Of those considered relevant, officers comment as follows: *'Development proposals should deliver high quality design. As appropriate to their scale, nature and location development proposals should:*

1. Preserve or enhance the village of Heacham, be sensitive to its surroundings, and demonstrate that it minimises adverse impacts on neighbouring residences – considered acceptable and covered later in this report

2. Recognise and reinforce the character of the local area in relation to height, scale, spacing, layout, orientation, design, and materials of neighbouring buildings - The parameters of the replacement buildings are very similar to those they replace and are of an appropriate height, scale, etc. Materials are considered acceptable and would be suitably conditioned if permission is granted. Further consideration is given later in this report.

5. Incorporate measures which increase energy efficiency and which reduce energy and resource loss, e.g. installation of solar panels, use of grey water, use of alternatives to plastic - Energy efficient solutions are incorporated into the development e.g. solar panels are proposed on the southern and western roof slopes of the Visitor Building, Retail Building and the western elevation of the Multi-Purpose Building

6. Provide sufficient external space for:

- **refuse and recycling storage**
- **bicycle parking**
- **child and disabled facilities where appropriate**
- **the integration of meter boxes, lighting, flues and ventilation ducts, gutters and pipes, satellite dishes, aerials and telephone lines -** Appropriate for the type of development sought

10. There is no unacceptable adverse impact (visual or otherwise) on the area's landscape, and proposals for development will be expected to demonstrate how they have minimised landscape impacts on the open countryside and coastline – considered acceptable and covered later in this report

11. Incorporate adequate landscaping to mitigate the visual impact of the development and to ensure that proposals are in keeping with the existing village context. Where possible, sites are screened through the use of landform, native trees and locally appropriate planting - Would be suitably conditioned if permission is granted

13. Where practicable, provide adaptable homes through the lifetime homes standard in order to cater for a changing demographic – N/A

14. Where practicable, make better connections to other areas of the parish, including access to local services and public open spaces – considered acceptable and covered later in this report

15. Retain mature or important trees (NPPF 2019 Section 175 applies) - no trees will be removed

16. Ensure new boundary treatments reflect the distinct local character and incorporate semi-mature street planting and hedges to boundaries with open countryside - would be suitably conditioned if permission is granted

17. Access to the site is provided/improved to highway authority standards – considered acceptable and covered later in this report

18. Where appropriate, proposals make a positive contribution towards open spaces, whether respecting the amenity, recreational and wider environmental value of existing spaces or, especially for developments of more than 8 dwellings, provide additional public open space to meet the needs of new residents - This is a key component of the application.

Notwithstanding issues that are covered later in this report, it is considered that the proposed development is in general compliance with Policy 5 of the HNP.

Policy 9: Holiday Accommodation

In order to maintain and improve Heacham's attraction as a quiet uncommercialised holiday centre, applications for further holiday accommodation beyond existing defined holiday areas, will only be supported where the proposals:

1 Maintain the distinction between the contrasting holiday centres of Heacham and Hunstanton and do not diminish the physical separation between these centres - The site does not diminish the physical separation between these centres

2 Do not have any unacceptable impact on local infrastructure, including green infrastructure - Heacham is a Key Rural Service Centre with many services and facilities. It is not considered that the scale of the proposed development would have an unacceptable impact on local and green infrastructure. Highways issues are covered in more detail later in this report

3 Minimise any visual and physical impact on the village by including, where appropriate, a landscaping plan incorporating the use of landform, native trees and locally appropriate planting - A detailed landscaping plan would be suitably conditioned if permission is granted

4 Are not directly adjacent to any residential areas - considered acceptable and covered later in this report

5 Do not need to be accessed through the village centre of Heacham - The site does not have to be accessed through the village centre

6 Incorporates high quality accommodation for which adequate parking and servicing arrangements are provided - The proposed units are a scale, mass, design and utilise appropriate materials, and appropriate parking is proposed

Can demonstrate a link to wider tourism or land use initiatives that provide demonstrable benefits to the local area - The development would provide a unique tourism offer, with some limited tourist accommodation and provide new employment opportunities.

In relation to the latter point, it is suggested this development would provide 16.25 full-time equivalent (FTE) jobs and, in combination with the Mount Pleasant application could generate an additional £4.3 million of visitor spending per year into the local economy (based on Visit

Britain figures for average spend per overnight stay of £67 and a day trip of £40(2019 figures) and visitor number projections of 80,000 per annum for Heacham Bottom and 16,500 per annum overnight accommodation.)

Staff	FTE
General Manager	0.75
Accommodation Lead	0.25
F&B Lead	0.75
Activity Lead	1
Admin / bookkeeping	1
Sales & Marketing	1
Cleaning & Maintenance	2.5
Operational	4
Tour Staff	2
Seasonal	3

Policy 11: Green Infrastructure:

As appropriate to their scale, nature and location, development proposals should protect and where practicable enhance existing green infrastructure and where practicable provide new green infrastructure facilities. In particular, support will be given to proposals that further enhance:

- 1. The quality, accessibility and usage of public open spaces, allotment provision and areas of sport provision*
- 2. Existing public rights of way within the parish, and to seek opportunities to create new public rights of way to create linkages to the beaches, and into the wider countryside locally*
- 3. The preservation and enhancement of Area of Natural Beauty and local habitats*
- 4. Increasing the number of trees in the village and enriching green areas with wildflower planting*
- 5. Maintain existing grass verges where possible, e.g., where there is a footpath on the opposite side of the road...*

The development accords with the overarching aims of this policy.

Policy 13: Dark Skies

External lighting associated with development proposals should be sensitively designed to safeguard the dark skies environment of the neighbourhood area and minimise the extent of any light pollution...

Lighting would be suitably conditioned if permission is granted to ensure compliance with Policy 13 of the HNP.

Policy 14: Community Facilities

Proposal to enhance existing, or develop additional community facilities will be supported particularly:

- Health services, dental practice*
- Facilities for children, teenagers and young adults...*

The development proposes an additional community facility.

Policy 17: Settlement Breaks

Development proposals outside the development boundaries of Heacham (and as shown in Inset G47 of the SADMP) will only be supported where they:

- *do not cause unacceptable harm to the landscape setting and distinct identity of Heacham*
- *do not detract from the visual separation of Heacham from Hunstanton*
- *do not detract from the views or settings of the Norfolk Coast AONB*
- *New development must not result in the coalescence of Heacham with Hunstanton to the north.*

In relation to the policy criteria above the LPA comments as follows:

- Impact on the landscape (and AONB in particular) is covered in more detail later in this report
- The development would not detract from the visual separation between the settlements.

Snettisham Neighbourhood Plan

Snettisham Neighbourhood Plan Policies, relevant to this application, are Policies: NP05 (Materials and Design), NP09 (Natural Environment) and NP10 (Transport)

Taking each in turn:

NP05: Materials and Design

All development should comprise high quality design. Residential development should make use of local materials, for example carrstone. All development should reflect and respond positively to local character.

The parameters of the replacement buildings are very similar to those they replace and are of an appropriate height, scale, etc. Materials are considered acceptable and would be suitably conditioned if permission is granted. Further consideration is given later in this report.

NP09: Natural Environment

The enhancement of the public rights of way network, including access to it, will be supported. All development within the Norfolk Coast AONB should protect and enhance the AONB.

The proposal includes permissive footpaths, crossings and bus stop improvements, all of which are covered later in this report.

A small area of the site falls within the AONB; this is the area to south, where the yurts are proposed. Some of the trails extend into this area too, as does the permissive footpath (that will be discussed later in this report.) It is not considered these elements would negatively impact on the landscape setting and / or scenic beauty of the AONB. This is supported by the Norfolk Coast Partnership's comments. This is considered more fully later in this report.

NP10: Transport

The enhancement of pedestrian access to the village centre will be supported.

Considered in more detail later in this report.

Summary – Principle of Development:

In summary it is considered that the principle of development is to be supported and that the development accords with the overarching national and local policy criterion outlined above.

However, part of the site lies within an AONB and therefore careful consideration needs to be given to the impact of the development on this nationally designated area.

Form and Character and Impact on the Area of Outstanding Natural Beauty (AONB):

The Design and Access Statement (DAS) that accompanied the application suggests that careful consideration was given to which buildings to retain and repurpose, and which to demolish in order to protect and enhance the landscape to create a high-quality development and enhance views to and from the site.

The existing grain store, that measures 9.4m to ridge, 6.2m to eaves and is 42.3m long x 24.4m wide, and is constructed from steel portal frame with profiled metal cladding, is of a greater scale than its replacement, reoriented building (the Visitor Building) that measures 6.4m to ridge, 2.4m to eaves and is 39.2m long x 12m wide, with a 12m x 12m gable projection and is to be constructed from a mixture of vertical standing seam cladding and vertical profile cladding under a profiled metal roof. This will help to ensure the impact on the overall landscape is not increased by the new development and overall, the quantum of built form on site is reduced.

It is suggested that the large, glazed openings will bring natural light into the buildings and give views onto the adjacent courtyard and wider views.

The other new structure (Retail Building) will replace the existing brick and corrugated roof 'spray sheds' that are in a dilapidated condition and not of architectural note. The existing spray shed building measures 4.6m to ridge, 3.2m to eaves and is 31.9m long x 9.6m wide and has a gable projection. The building is constructed from red brick and concrete block under a profiled metal roof. The new retail building is to be built in the same style and materials as the Visitor Building and will measure 5m to ridge, 2.4m to eaves and is 31.6m long x 7.4m wide, with a 7.4m x 4.9m gable projection.

The more traditional stone barns are to be retained, restored and incorporated into the scheme to provide the Multi-Purpose Building.

Materials in general comprise profiled metal roofing, vertical standing seam cladding, vertical profile cladding and vertical timber cladding combined with the existing materials of brick, flint and red Norfolk clay pantiles.

There are substantial areas of landscaping, both soft and hard, including the car park (which would include car park surfacing for both the formal and overspill car parks), internal access ways, that are indicatively shown and will require further detailed consideration. However, this can be suitably covered by condition.

Specific trail details accompanied the application and these will be suitably conditioned if permission is granted.

The indicative biodiversity enhancement plan shows:

- Hedge planting
- Enhance existing meadow by sowing additional species-rich seed mix
- Planting of a wild garden (species to be confirmed)
- Restoration of wildflower meadows around the walking trails
- Tree planting adjacent to existing woodland, to the northeast of the proposed car park to create a 'wild' orchard pasture, and to the west of the access to create a woodland pasture setting

- A mix of seeding, planting and natural regeneration methods to result in a dynamic habitat of open glades, wood pasture and scrub around the bike trails.

Likewise details of the outdoor play are indicative only, and whilst the indicative proposal is acceptable, the details will need to be fully considered. This can also be suitably conditioned if permission is granted.

Other details that will need to be conditioned are, lighting, internal signage and service track details.

In relation to lighting, a lighting statement accompanied the application confirming that lighting will be kept to a minimum with low-level bollards with downcast lighting being placed where necessary for the car parking and primary pedestrian routes. No external façade lighting is proposed on the buildings and all lighting will be switched off at 9pm (curfew). Additionally, all light sources shall be shielded from direct external view or shall exhibit a maximum source intensity below 2,500cd (as suggested by the Institute of Lighting Professional Guidance Note GN01.)

In relation to the service track, it is not envisaged that any engineering works will be required, and that the tracks will simply be field tracks, and this was confirmed during a site meeting. However, this does require formal clarification to ensure this is the case. Likewise, this can be suitably conditioned if permission is granted.

The ten yurts would be canvas and would be positioned on timber decks. Three of the yurts would be within the woodland, situated on elevated platforms (2 metres high) to provide a 'treehouse' experience. The yurts will be serviced by a mixture of both private and shared facilities (WCs, showers, kitchen facilities (bins)).

The yurts would vary in size with the maximum dimensions being 7m wide by 4 metres high. These dimensions would be conditioned if permission is granted.

Paragraph 176 of the NPPF states: *Great weight should be given to conserving and enhancing landscape and scenic beauty in National Parks, the Broads and Areas of Outstanding Natural Beauty which have the highest status of protection in relation to these issues...The scale and extent of development within all these designated areas should be limited, while development within their setting should be sensitively located and designed to avoid or minimise adverse impacts on the designated areas.*

Paragraph 177 continues by stating: *When considering applications for development within National Parks, the Broads and Areas of Outstanding Natural Beauty, permission should be refused for major development* other than in exceptional circumstances, and where it can be demonstrated that the development is in the public interest. Consideration of such applications should include an assessment of:*

- a) the need for the development, including in terms of any national considerations, and the impact of permitting it, or refusing it, upon the local economy*
- b) the cost of, and scope for, developing outside the designated area, or meeting the need for it in some other way; and*
- c) any detrimental effect on the environment, the landscape and recreational opportunities, and the extent to which that could be moderated.*

*the NPPF states that 'major development' is a matter for the decision maker, taking into account its nature, scale and setting, and whether it could have a significant adverse impact on the purposes for which the area has been designated or defined.

Your officers do not consider that the development is major development for the purposes of the NPPF. However, there is still a need for all development to either conserve or enhance the AONB.

Protection of the countryside and AONB's is reiterated in both the Development and Heacham and Snettisham Neighbourhoods Plans.

As previously stated only a small part of the site actually falls within the AONB, including the yurts, parts of the trails and part of a permissive path. However, the NPPF requires the setting of the AONB to be likewise protected.

In this regard the sensitive reuse and construction of new buildings (in terms of scale, mass, appearance and materials), all contained within the existing farmyard setting, suggests the main built form would incorporate into its setting in an organic manner.

The footprint of the new buildings closely follows the existing thus reducing the impact on the character of the surrounding area and landscape.

Detailed landscaping, that would be conditioned if permission was granted, will ensure any impacts are reduced. As mentioned elsewhere in this report, no trees are required to be removed to enable the proposed development.

Whilst it is considered that the built form is acceptable and would not in its own right be of detriment to the AONB, the use of the buildings and wider site will have an impact on this protected locality.

This will be principally by activity associated with the use. There will be greater vehicular activity, general activity and light pollution, although it is noted that the latter would be limited and suitably conditioned if permission was granted.

In relation to the former issue, activity, Members will need to consider whether this would result in conservation of the AONB and if the benefits to the existing agricultural enterprise and wider economy outweigh the harm these activities will have on the character of the AONB.

A detailed Landscape and Visual Appraisal (LVA) accompanied the application. The LVA assessed a total of 13 viewpoints comprising transport, recreational, residential and Public Right of Way (PROW) receptors.

The LVA concluded overall as follows:

OVERALL SUMMARY

It is considered that as the proposed development has followed a landscape led approach which adopts rewilding, natural regeneration and traditional conservation approaches in order to successfully integrate new nature-based tourism within the wider agricultural setting. This will facilitate the sensitive integration of the new facilities to support its intended function and has demonstrated a successful approach to accommodating the level of change within the landscape without establishing important levels of harm to the landscape characteristics and those elements which define the site.

It is considered that the proposed development can be accommodated to form an acceptable day visitor and tourist attraction together with the wider beneficial approach to integrating rewilding characteristics at the heart of the proposals.

The potential for adverse landscape and visual effects which would be considered at most, moderately harmful at day one, can be appropriately mitigated through the implementation of the site-specific landscape proposals.

It is therefore considered that the level of potential adverse landscape and visual effects associated with the proposed development has been reduced to an acceptable low level.

Your officers, along with the Norfolk Coast Partnership and NCC Protected Landscape Team consider the impacts are acceptable and that the development would conserve both the AONB and its setting, and that any harm is outweighed by the benefits of the proposed development.

It is therefore considered that the development therefore accords with the relevant national and local planning policy outlined above in relation to impacts on protected landscapes and visual impact in general.

Highway Impacts:

In relation to transport, paragraphs 85, 104, 105, 110 - 113 of the NPPF, whilst acknowledging that sites to meet local business needs, including tourism, may be beyond existing settlements and not be well served by public transport, require developments to be safe, offer alternative modes of transport, enable charging of plug-in and other ultra-low emission vehicles, and concludes that *Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.*

The requirements are reiterated in Development Plan Policies CS11 and DM12 and Heacham Neighbourhood Plan Policies 5, 12 and 18.

Highway safety, impacts and congestion are some of the most contentious issues associated with this development.

Vehicular access, including for farm traffic, is currently via three single-track access roads from the A149, with secondary access from Lamsey Lane via an unmade track.

The proposed development would result in the current accesses being used only for emergency vehicle access and to access the residential properties to the east of the farmyard; with the only visitor access utilising the Lamsey Lane entrance that will be widened and upgraded in accordance with the Local Highway Authority's requirements.

A 210-space car park that includes 20 disabled spaces and 2 coach parking spaces is proposed along with a 110-space overflow car park, providing a maximum capacity of 320 car parking spaces.

Additionally, a new permissive footpath is proposed to link Heacham and Snettisham. This links the Heacham Bottom site with the Snettisham roundabout to the south via a permissive path that runs adjacent to the eastern boundary of the site. Additionally, if permission were to be granted for the Mount Pleasant application, it would continue to the northwest corner of the Mount Pleasant site directly adjacent to the eastern edge of Heacham.

The following off-site highway improvements works are also proposed, the details and implementation of which will be secured by condition if permission is granted:

- Additional signage on Lamsey Lane
- Provision of a new pedestrian footway connection between the Mount Pleasant site (on-site path) and Heacham (existing highway footway)

- Provision of a pedestrian crossing of Lamsey Lane between the Mount Pleasant & Heacham Bottom sites
- Widening (to 3m) of the existing footway provision on the southern arm of the A149 (both sides) and the western side of the B1440 and an improved refuge island crossing at Snettisham Roundabout to safely provide an off-road cycle facility linking the B1440 to the new on-site permissive path
- Provision of a pedestrian refuge island on the A149 and associated sections of footway to provide a link (and safe crossing) from the development site to the bus stops.

A Transport Assessment (TA), that considered existing levels and characteristics of traffic demand, development proposals and associated traffic attraction and operational assessment including junction capacity (Lamsey Lane / Lynn Road (A149)), accompanied the application. Additionally, the TA states that no operational impact is anticipated at locations further afield than the Lamsey Lane / Lynn Road (A149) junction.

The TA states that Lamsey Lane is a rural road with no footways or streetlighting and a carriageway width generally of c.5.8m. It connects the site and southern part of Heacham with the A149, the latter of which is sited a short distance to the east of the site and runs broadly in a north-south alignment. The speed limit of Lamsey Lane in the vicinity of the Heacham Bottom access is 60mph.

In relation to visibility, the TA concludes, when applying the 85th percentile speeds recorded, that the requirements are 2.4m x 102m to the left and 2.4m x 110m to the right. It is suggested that these can be achieved with suitable hedge cutting.

In relation to visibility from Lamsey Lane and Lynn Road (A149), splays are stated to be 2.4m x 415m to the left (north), and 2.4m x 460m to the right (south.) These splays are considered acceptable, and no further improvements are proposed or required by the Local Highway Authority.

The junction of Lamsey Lane with the A149 comprises a priority T junction and benefits from both a ghost island right-turn lane and a left-turn slip. Two solid direction islands are provided comprising:

- A central island on Lamsey Lane, with a bollard and 'Keep Left' sign
- A segregation direction island on Lynn Road, both directing and protecting left turning traffic onto Lamsey Lane.

There is no streetlighting on Lynn Road, however there is a narrow footway on the western carriageway edge.

The TA acknowledges that the development is likely to attract most visitors by car, although there are existing bus stops adjacent to the site's existing eastern entrance onto the A149 which would provide for an alternative mode of transport to the site.

The TA concludes 117 vehicles per hour would be the maximum two-way trip movement associated with the proposed development and that this would occur between the hours of 2pm and 4pm. These figures have been worked out on the maximum capacity of the car park and overflow car park of 320 spaces and the assumption that more people are likely to arrive in the morning than the afternoon.

To identify traffic flow characteristics both manual (MCC) and automatic (ATC) traffic count surveys were undertaken.

The ATC surveys were undertaken over the two weeks straddling the school summer holiday breakup in 2021 (i.e., the last week of term and the first week of the summer holidays), **with a further ATC taking place between 24 April 2023 to 3 May 2023.**

The ATC concluded that school term-time (last week of school (Thursday 15th to Wednesday 21st July)), two-way, weekday movements adjacent to the Heacham Bottom access were 4,031 with Saturdays being 4,809. Most movements were from light vehicles (e.g., cars); and 3,983 and 3,558 during the school holidays (first week of summer holidays (Thursday 22nd to Wednesday 28th July.))

Growth factors were added to the MCC figures to investigate capacity at the Lamsey Lane / Lynn Road (A149) junction with the TA concluding that there are no capacity concerns, and no significant queuing is forecast.

Seasonality calculations were also used to produce factors to assess the impact of summer holiday traffic, with the same outcome (i.e., no capacity issues or significant queuing forecast.)

Further explanation was sought in relation to how these conclusions were reached. The applicant's highway advisor explained as follows: *"Concerning the statement in the report about the flows not raising any capacity concerns, this is simply due to the low numbers of vehicles concerned. The surveys showed peak hour link flows of just over 200 vehicles per hour westbound and round half this eastbound. In round numbers this demonstrates only around 3 vehicles per minute on average, westbound, therefore having an average vehicle headway of around 20 seconds, which is easily adequate for a turning vehicle to safely undertake its manoeuvre. To compound this, the forecast traffic generation peaks at only 117 per hour (1.9 per minute on average) and during the existing Saturday background peak hour only 102 (1.7 per minute on average). It is very clear that these give no concern regarding traffic capacity, and I am confident that Jon [Jonathan Hanner, NCC Local Highway Officer] will confirm the LHA's agreement on this matter."*

In response to concerns expressed in relation to frequency of nose-tail shunts and their severity, the TA, whilst concluding that there is no reason to suggest either of these would occur, proposes the erection of warning signs for eastbound traffic on Lamsey Lane, identifying the potential for Queuing Traffic Ahead. These would be located west of the crest on Lamsey Lane such that an approaching driver can moderate their speed in the knowledge that stationary traffic may cause an obstacle ahead.

In summary the TA concludes that the development:

- Would produce levels of forecast trips that will not cause issues on the local highway network
- Has significant spare capacity at the nearby Lamsey Lane / Lynn Road (A149) junction, including during high tourist season
- Can achieve safe visibility requirements at the proposed access
- Is located on highway with a good safety record
- Proposed the erection of signate to assist with maintaining the high standard of highway safety
- Is sustainably located for its rural location with the potential to increase sustainability
- Is compliant with local and national policy
- There is no defensible reason for refusal on the grounds of traffic, transport or highway safety.

The Local Highway Authority has found the findings of the TA to be an acceptable basis on which to make their recommendation [no objection] subject to conditions relating to construction management / parking, off-site improvement works, new access provision and

specification including gradient / visibility splays and closure of other accesses, means of obstruction, parking, loading / unloading, serving, etc. and provision of cycle parking being appended to any permission granted.

In late correspondence at Planning Committee of 3rd April, it was reported that the applicant confirmed that they would offer land around the Lamsey Lane junction, with a condition that highway works commence within 15 years, to help enable junction improvements. This has now been confirmed within an updated Highways Position Statement (V4), and the recommendation on this application has been amended to secure this.

Whilst the ATC findings were not a formal reason for deferral, the applicants undertook a further ATC between 24th April 2023 to 3rd May 2023. This is attached as Appendix 2.

The Local Highway Authority (LHA) has confirmed that the originally submitted ATC was robust and its findings, including projections for future traffic generation, were valid with any differences between the projections and the 2023 ATC not considered to be significant and to be expected as daily variances. The LHA then state that, as with the initial analysis, variance factors and seasonal uplifts have been applied to the 2023 data, and the associated junction traffic modelling carried out which identified no material change. The LHA concludes that their professional opinion remains that there will be no material impact at the junction as a result of the proposals. The LHA's comments can be read in full in the '*Representations*' section of this report.

The Local Highway Authority has confirmed that they agree that the findings of the latest ATC support the findings of the original ATC with any differences being insignificant.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to highway impacts.

Impact on Neighbour Amenity:

The NPPF requires development to have a high standard of amenity for existing and future users (para 130f.) This is reiterated in Development Plan Policies CS06 and DM15 and Heacham Neighbourhood Plan Policy 5.

There are four dwellings located to the east of the farmyard (Heacham Bottom Cottages, 61, 63, 65 and 67 Lynn Road.) These cottages are in the ownership of Ken Hill, but not associated with the use of the site.

Currently farm traffic can access the site via an access track that runs to the north of these dwellings. The proposal would remove this farm traffic, and all traffic associated with the development would access the site from Lamsey Lane.

This is likely to have a positive impact on occupiers of those dwellings.

However, there will be activity associated with the proposed use in relatively close quarters to these dwellings, with parking to the northwest and the outdoor play area to the west. However, for reference, the distances involved are 20m to the eastern edge of the car park (the area of car park furthest away from the main visitor building and therefore likely to be the least used) and 130m to the outdoor play areas.

It is not considered that the impacts would be significant, and no objections have been received from occupiers of these dwellings. Furthermore, operation of the site, in line with the addendum received in relation to the Business Plan that accompanied the application, would be conditioned if permission were granted.

Additionally, if a statutory nuisance did occur then the Local Authority has powers to intervene.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to impacts on neighbour amenity.

Ecology / Biodiversity:

The NPPF, at Chapter 15, requires planning policies and decisions to protect and enhance biodiversity. This is reiterated in Development Plan Policies CS01, CS12 and DM19 and Hunstanton Neighbourhood Plan Policy 11.

The application was accompanied by a detailed Shadow Appropriate Assessment, Biodiversity Net Gain Assessment, Preliminary Ecological Appraisal and Bat Risk Assessment.

Natural England confirmed that it would be appropriate for the LPA to adopt the Shadow Appropriate Assessment as its Appropriate Assessment (as the competent authority.)

Appropriate Assessment is the method of assessing whether a development would have a likely significant effect on protected sites. In this instance the sites are:

- The Wash and North Norfolk Special Area of Conservation (SAC)
- The Wash Special Protection Area (SPA)
- The Wash Ramsar
- North Norfolk Coast SPA
- North Norfolk Coast Ramsar.

Note – A Ramsar site is a designation for wetlands that are of international importance.

The Appropriate Assessment concludes that without mitigation the development could have an adverse effect on the integrity of the above protected sites. The following mitigation was therefore proposed, and accepted as appropriate by Natural England:

- A financial contribution to be paid into the Norfolk Green Infrastructure and Recreational Avoidance Mitigation Strategy (GIRAMS)
- Implementation of a Coastal Park Zoning Strategy and subsequent monitoring of this approach to ensure it is effective in reducing recreational disturbance

Other than the GIRAMS payment (£5,206.04), that has already been paid, the other mitigation methods would be suitably conditioned if permission is granted.

The Ecological Appraisal is concerned with site specific ecological issues and considered badgers, bats (also covered by a separate bat risk assessment), great crested newts, birds, reptiles and invasive species.

The Appraisal concluded that no further studies are required, and other than precautionary approaches and best practise (such as vegetation removal outside of the bird breeding season) no further mitigation is required. The following enhancements are proposed:

- Incorporation of bird and bat boxes across the site providing extra potential roosting / nesting resource thus improving biodiversity

- Replanting of a range of ruderal type plants and scrub that will attract pollinators
- Reinstating hedgerows with native species
- New hedgerow to be native
- Planting of an orchard
- Restorative management of the recently planted woodland to extend light to woodland floor creating glades and thus increasing the edge effect.

The best practice mitigation and enhancements would be suitably conditioned if permission is granted.

The Bat Risk Assessment gave specific attention to the buildings and concluded that there was no signs of bats or any potential roosting features in any of the modern barns. It further stated that the *majority of the barns are suboptimal or unsuitable for bats due to construction and the amount of ambient light. The exception being the small brick-built barn with tile roof.* However, bat emergence surveys showed no bats emerging from these buildings.

Therefore, no mitigation is required other than best practice which would be suitably conditioned if permission was granted, this includes use of bitumen type felt and low-level lighting.

Whilst not currently enacted into Planning Law, the Environment Act 2021 is likely to come into force in late 2023 with a probable requirement of a 10% net gain in biodiversity.

The Biodiversity Net Gain Assessment that accompanied the application has concluded that there would be an overall net gain for biodiversity of 28% (27.37% in habitats and 0.69% in hedgerows.)

It should however be noted that this net gain is over both this site and the Mount Pleasant site in combination.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to ecology and biodiversity.

Drainage:

Foul drainage will utilise a new package treatment plant with surface water drainage being distributed to soakaways in appropriate locations.

Full details will be sought by condition if permission is granted.

Crime and Disorder: There are no specific concerns relating to Crime and Disorder, and the Police Architectural Officer has supplied the applicant with advice regarding Designing out Crime.

Other matters requiring consideration prior to the determination of this application:

Trees: No trees are to be removed to enable the proposed development. However, tree planting is proposed adjacent to existing woodland, to the northeast of the proposed car park to create a 'wild' orchard pasture, and to the west of the access to create a woodland pasture setting.

Retail / Event Space: The development would provide some retail offer in both the Visitor Building and Multi-Purpose Building.

Local Plan Policy DM10 covers retail development and seeks to guide retail uses to the main retail centres of King's Lynn, Hunstanton and Downham Market to ensure the retail offer of these centres is not undermined. This policy is primarily concerned with large, edge of centre, retail developments rather than small, rural, ancillary retail proposals such as this and seeks retail impact assessments on retail floorspace that exceeds 2500m². This proposal is well below this figure, and it is not considered that the development would have any undermining impact on the borough's retail centres, primarily Hunstanton given the proximity of the two.

In this regard Local Plan Policy CS10 and the NPPF (paras 84 and 85) acknowledges the need for rural employment as previously discussed.

The Retail Statement that accompanied the application states that a maximum of four commercial units will be provided within these two buildings, one of which would be for bike hire.

It is assumed that this would be in the Multi-Purpose Building that will also accommodate cycle storage, cycle hire and cycle repair facilities. Cycle hire will be for people using the site only, and not for the general public, although those hiring the bikes would be able to use them off-site.

The other units would be in the Visitor Building. The Retail Statement suggests that these would be operated by the estate and third parties to provide goods and services complementary to the overall Wild Ken Hill theme. The idea is to create opportunities for local businesses to offer goods and services to visitors by renting a unit.

The Retail Statement acknowledges that it is important that the retail offer is not a destination in its own right, but an ancillary / complementary offer (e.g., nature based / outdoor pursuits (bird watching, wildlife spotting, foraging, binoculars, camera equipment, books, manuals, maps etc.) To this end the number and types of retail will be strictly conditioned if permission is granted. This is especially important as it will be free to park and enter these shops (as well as the Multi-Purpose Building which houses the café), with payment only being required when one enters the main activities that are behind a 'paywall' and accessed on purchase of a ticket.

The Multi-Purpose Building also houses an event space which the applicant anticipates would be available to host activities and may be hired by local businesses for use as, for example, a studio for yoga, painting, photography, etc. Again, use of this space will need to be carefully conditioned.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to retail development.

Sustainable Design: Paragraphs 129 and 154 of the NPPF relate to the sustainability of buildings and Development Plan Policy CS08 is concerned, in part, with sustainable design and energy efficiency.

The DAS that accompanied the application states that sustainability has been an important consideration for the project from the outset based on the premise that Wild Ken Hill revolves around rewilding, regenerative farming and traditional conservation practices therefore being based on highly sustainable land use methods which enhances carbon sequestration and ecological restoration.

It is suggested that the development includes a significant amount of new tree planting and biodiversity net gain.

The following design principles have been considered during the evolution of the proposal:

- Re-use of existing buildings on the site where feasible from both an operational and structural perspective which minimises embodied carbon related to demolition and construction activities
- Orientation of new buildings and position of new openings in converted buildings to make the most of passive solar gain
- High levels of insulation, airtightness and heat-recovery based ventilation on all buildings both new and converted
- Main energy supply being via renewable energy in the form of solar panels and air source head pumps which would ensure 10% reduction in the predicted CO2 emissions as suggested by Core Strategy Policy CS08
- Materials are durable using recycled elements whilst still ensuring their appearance is appropriate to the local agricultural vernacular

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to sustainable design.

Flooding: The site does not lie in an area at risk of flooding.

Environmental Quality: The requested conditions relating to contamination, smoke emissions and EV charging will be suitably conditioned if permission is granted.

Historic Environment Service: The requested archaeological conditions will be appended to any permission granted.

Fire: The provision of a fire hydrant will be suitably conditioned if permission is granted.

CIL: The development is not CIL liable.

Three further specific issues were raised at the Planning Committee meeting of 3rd April:

1. ***How will Refuse be dealt with?***
2. ***How will foul waste be dealt with?***
3. ***How was the Biodiversity Net Gain of 27% calculated?***

Taking each in turn:

1. **Plans for both sites include clearly demarcated refuse storage areas on site. The applicant will need to explore, before the development is brought into use, refuse collection via either a private service or council services as appropriate.**
2. **As confirmed on the applicant's application forms for both sites, a package treatment plant will be used for foul waste. No consultee has raised concerns regarding this matter and drainage has been suitably conditioned**
3. **The application was accompanied by a detailed 16-page Biodiversity Net Gain calculation document. The 27% biodiversity net gain has been calculated in accordance with the DEFRA Biodiversity Metric 3.1. which is stated to be the government's (only) nationally accepted criteria for calculating biodiversity net gain.**

Other Specific Comments:

In relation to objections received from Heacham Parish Council, the CPRE, KLWNBUG and third-party representations, your officers respond as follows:

- Highway safety and congestion – covered in report

- How will drainage be dealt with? – covered in report, will be suitably conditioned
- A business assessment is required to show the impact on the village businesses – covered in report, additionally there is no requirement for such an assessment
- The Cheney Hill housing developments have not been considered when considering highway impacts – these developments were considered in the Environmental Impact Assessment
- Impact on AONB and natural environment – covered in report
- The development does not accord with the NPPF, development plan policies or neighbourhood plan policies in relation to the location of the development – covered in report
- Lighting – covered in report, will be suitably conditioned
- Concerns with cycle crossing proposed at Snettisham roundabout – the plans are indicative only and would be conditioned if permission were granted. The Local Highway Authority has confirmed that: *at this stage, the submitted drawings are for planning purposes only to agree the principle of the improvements proposed. If approved, the applicant would need to submit detailed engineering drawings which would be the subject of a S278 technical vetting process, including a safety audit, by the Highway Authority before being discharged by yourselves.*
- The proposals are nothing to do with rewilding and conservation and everything to do with making money – covered in report, the development would generate income to enable existing and future objectives to be obtained
- How will Lamsey Lane be crossed? – a suitable pedestrian crossing is conditioned
- Why hasn't a roundabout or traffic lights been proposed at the Lamsey Lane / Lynn Road (A149) junction? – these weren't proposed and given no objection from the Local Highway Authority, are not required
- Will the permissive footpath be surfaced to be appropriate for bikes, buggies, etc? – details are the permissive footpath will be secured by condition if permission is granted

CONCLUSION / PLANNING BALANCE:

Applications for planning permission should be determined in accordance with the Development Plan and any other material considerations. The application falls under the 'banner' of both farm diversification and rural tourism. Both of which are supported at a national and local policy level, subject to other relevant planning policy and guidance, and for the reasons outlined in the report above, it is considered that the development is in general accordance with these enabling policies.

The site lies partly within an AONB and affects its wider setting. However, whilst the land take is substantial, the development itself has limited built form, and the Norfolk Coast Partnership and NCC Protected Landscape Team consider that the development would not have an adverse impact on the AONB or its setting.

It is considered that the scale, mass, design and appearance of the proposed replacement buildings and converted existing buildings are acceptable in the 'farm complex' setting.

However, it is acknowledged that there would be impact from the proposed use especially in terms of visitor numbers and vehicular activity; issues that have raised objections from Heacham Parish Council, the CPRE and a number of third-party representatives.

The proposed development would result in a material increase in vehicular activity. Notwithstanding this, the Local Highway Authority has confirmed this increase in vehicular activity would not significantly affect highway capacity or highway safety. Additionally, the offer of additional land to help facilitate improvements to the Lamsey Lane / Lynn Road Junction, whilst not required by the current applications, is to be

welcomed and will be secured by via S106 Agreement. Therefore, the Local Highway Authority raise no objection on the grounds of highway safety subject to conditions requiring, amongst other things, off-site improvements.

The proposal is considered to come with benefits both to the existing enterprise and also to the wider tourism offer in the Borough, as well as demonstrating a benefit to the local area. These benefits are given weight by officers. That said, it is acknowledged that these benefits are questioned by the Parish Council, CPRE and some third-party representatives, and Members will need to consider the weight to be given to these issues.

It is therefore recommended, that this application be approved subject to the following conditions.

RECOMMENDATION:

A) APPROVE subject to signing of a S.106 legal agreement to secure safeguarded land around the Lamsey Lane junction for potential future highway improvements for a period of 15 years from the date of decision, and the imposition of the conditions set out in the committee report.

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans:

Demolition Plan As_Proposed Drawing Package Rev.B
Site Plan_As Proposed Drawing Package Rev.B
Site Masterplan 1 of 2_As Proposed Drawing Package Rev.B
Site Masterplan 2 of 2_As Proposed Drawing Package Rev.B
Visitor Building Floor Plan_As Proposed Drawing Package Rev.B
Visitor Building Elevations_As Proposed Drawing Package Rev.B
Visitor Building Roof Plan_As Proposed Drawing Package Rev.B
Retail / WCs New Build Floor Plan_As Proposed Drawing Package Rev.B
Retail / WCs New Build Elevations_As Proposed Drawing Package Rev.B
Retail / WCs New Build Roof Plan_As Proposed Drawing Package Rev.B
Muster Point Floor Plan_As Proposed Drawing Package Rev.B
Muster Point Elevations_As Proposed Drawing Package Rev.B
Muster Point Roof Plan_As Proposed Drawing Package Rev.B
Indoor Play Floor Plan_As Proposed Drawing Package Rev.B
Indoor Play Elevations_As Proposed Drawing Package Rev.B
Indoor Play Roof Plan_As Proposed Drawing Package Rev.B
Existing Farm Buildings Floor Plan_As Proposed Drawing Package Rev.B
Existing Farm Buildings Elevations_As Proposed Drawing Package Rev.B
Glamping Site Plan_As Proposed Drawing Package Rev.B
Glamping Site Masterplan_As Proposed Drawing Package Rev.B
Glamping Facilities Plans & Elevations 1 of 2_As Proposed Drawing Package Rev.B
Glamping Facilities Plans & Elevations 2 of 2_As Proposed Drawing Package Rev.B
Biodiversity Enhancement Plan WKH-DIG-00-PL-0003 Rev.P03
Proposed Landscape Masterplan (1 of 2) WKH-DIG-00-PL-0001 (1 of 2) Rev.P10
Proposed Landscape Masterplan (2 of 2) WKH-DIG-00-PL-0001 (2 of 2) Rev.P09

- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: No development shall commence until full details of the foul and surface water drainage arrangements for the site have been submitted to and approved in writing by the Local Planning Authority. The drainage details shall be constructed as approved before any part of the development hereby permitted is brought into use.
- 3 Reason: To ensure that there is a satisfactory means of drainage in accordance with the NPPF. This needs to be a pre-commencement condition as drainage is a fundamental issue that needs to be planned for and agreed at the start of the development.
- 4 Condition: Prior to the commencement of groundworks, an investigation and risk assessment, in addition to any assessment provided with the planning application, must be completed in accordance with a scheme to assess the nature and extent of any contamination on the site, whether or not it originates on the site. The contents of the scheme are subject to the approval in writing of the Local Planning Authority. The investigation and risk assessment must be undertaken by competent persons and a written report of the findings must be produced. The written report is subject to the approval in writing of the Local Planning Authority. The report of the findings must include:
- (i) a survey of the extent, scale and nature of contamination;
 - (ii) an assessment of the potential risks to:
 - * human health,
 - * property (existing or proposed) including buildings, crops, livestock, pets, woodland and service lines and pipes,
 - * adjoining land,
 - * groundwaters and surface waters,
 - * ecological systems,
 - * archaeological sites and ancient monuments;
 - (iii) an appraisal of remedial options, and proposal of the preferred option(s). This must be conducted in accordance with the Environment Agency's Land Contamination Risk Management (LCRM).
- 4 Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors. This needs to be a pre-commencement condition given the need to ensure that contamination is fully dealt with at the outset of development.
- 5 Condition: Prior to the commencement of groundworks, a detailed remediation scheme to bring the site to a condition suitable for the intended use by removing unacceptable risks to human health, buildings and other property and the natural and historical environment must be prepared, and is subject to the approval in writing of the Local Planning Authority. The scheme must include all works to be undertaken, proposed remediation objectives and remediation criteria, timetable of works and site management procedures. The scheme must ensure that the site will not qualify as contaminated land under Part 2A of the Environmental Protection Act 1990 in relation to the intended use of the land after remediation.
- 5 Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and

ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

- 6 Condition: The approved remediation scheme must be carried out in accordance with its terms prior to the commencement of groundworks, other than that required to carry out remediation, unless otherwise agreed in writing by the Local Planning Authority. The Local Planning Authority must be given two weeks written notification of commencement of the remediation scheme works. Following completion of measures identified in the approved remediation scheme, a verification report that demonstrates the effectiveness of the remediation carried out must be produced and is subject to the approval in writing of the Local Planning Authority.

- 6 Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

- 7 Condition: In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with the requirements of condition 4, and where remediation is necessary a remediation scheme must be prepared in accordance with the requirements of condition 5, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures identified in the approved remediation scheme a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority in accordance with condition 6.

- 7 Reason: To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.

- 8 Condition: No demolition/development shall take place until an archaeological written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. The scheme shall include an assessment of significance and research questions; and 1) The programme and methodology of site investigation and recording, 2) The programme for post investigation assessment, 3) Provision to be made for analysis of the site investigation and recording, 4) Provision to be made for publication and dissemination of the analysis and records of the site investigation, 5) Provision to be made for archive deposition of the analysis and records of the site investigation and 6) Nomination of a competent person or persons/organization to undertake the works set out within the written scheme of investigation.

- 8 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF. This needs to be a pre-commencement condition given the potential impact upon archaeological assets during groundworks/construction.

- 9 Condition: No demolition/development shall take place other than in accordance with the written scheme of investigation approved under condition 8 and any addenda to that WSI covering subsequent phases of mitigation.

- 9 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.

- 10 Condition: The development shall not be occupied or put into first use until the site investigation and post investigation assessment has been completed in accordance with

the programme set out in the archaeological written scheme of investigation approved under condition 8 and the provision to be made for analysis, publication and dissemination of results and archive deposition has been secured.

- 10 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.
- 11 Condition: Development shall not commence until a scheme detailing provision for on-site parking for construction workers for the duration of the construction period has been submitted to and approved in writing by the Local Planning Authority. The scheme shall be implemented throughout the construction period.
- 11 Reason: To ensure adequate off-street parking during construction in the interests of highway safety. This needs to be a pre-commencement condition as it deals with the construction period of the development.
- 12 Condition: Prior to the commencement of any works a Construction Traffic Management Plan which shall incorporate adequate provision for addressing any abnormal wear and tear to the highway together with wheel cleaning facilities shall be submitted to and approved in writing by the Local Planning Authority (together with proposals to control and manage construction traffic using the 'Construction Traffic Access Route' and to ensure no other local roads are used by construction traffic).
- 12 Reason: In the interests of maintaining highway efficiency and safety. This needs to be a pre-commencement condition as it deals with safeguards associated with the construction period of the development.
- 13 Condition: For the duration of the construction period all traffic associated with (the construction of) the development will comply with the Construction Traffic Management Plan and use only the 'Construction Traffic Access Route' and no other local roads unless approved in writing with the Local Planning Authority.
- 13 Reason: In the interests of maintaining highway efficiency and safety.
- 14 Condition: Notwithstanding the details indicated on the submitted drawings no works above slab level shall commence on site unless otherwise agreed in writing until detailed drawings for the scheme of off-site highway improvement works have been submitted to and approved in writing by the Local Planning Authority.

The scheme of works shall include:

- Additional signage on Lamsey Lane.
- Provision of a new pedestrian footway connection between the Mount Pleasant site (on site path) and Heacham (existing highway footway) - as indicatively outlined on drawing 2021-F-015-026.
- Provision of a pedestrian crossing of Lamsey Lane between the Mount Pleasant & Heacham Bottom sites.
- Widening (to 3m) of the existing footway provision on the southern arm of the A149 (both sides) and the western side of the B1440 and an improved refuge island crossing at Snettisham Roundabout to safely provide an off-road cycle facility linking the B1440 to the new on-site path - as indicatively outlined on drawing 2021-F-015-030 Rev B.
- Provision of a pedestrian refuge island on the A149 and associated sections of footway to provide a link (and safe crossing) from the development site to the bus stops – as indicatively outlined on drawing 2021-F-015-029 Rev A.

- 14 Reason: To ensure that the highway improvement works are designed to an appropriate standard in the interest of highway safety and to protect the environment of the local highway corridor.
- 15 Condition: Prior to the first occupation/use of the development hereby permitted the off-site highway improvement works (including Public Rights of Way works) referred to in condition 14 of this condition shall be completed to the written satisfaction of the Local Planning Authority.
- 15 Reason: To ensure that the highway network is adequate to cater for the development proposed.
- 16 Condition: Prior to the first use of the development hereby permitted the main vehicular access onto Lamsey Lane shall be constructed (for the first 20 metres) in accordance with a detailed scheme to be agreed in writing with the Local Planning Authority in accordance with the highways specification and thereafter retained at the position shown on the approved plan (2021-F-015-020). Arrangement shall be made for surface water drainage to be intercepted and disposed of separately so that it does not discharge from or onto the highway.
- 16 Reason: To ensure construction of a satisfactory access and to avoid carriage of extraneous material or surface water from or onto the highway in the interests of highway safety.
- 17 Condition: Vehicular access to and egress from the adjoining highway shall be limited to the access from Lamsey Lane only as shown on the approved landscape masterplan. There shall be no access or egress to the development from any other access point. A detailed scheme demonstrating how access to the development from other points, including an existing access from the A149, shall be prevented will be submitted to and approved with the Local Planning Authority prior to the development being brought into use.
- 17 Reason: In the interests of highway safety.
- 18 Condition: The gradient of the vehicular access shall not exceed 1:12 for the first 10 metres into the site as measured from the near channel edge of the adjacent carriageway.
- 18 Reason: In the interests of the safety of persons using the access and users of the highway.
- 19 Condition: Prior to the first use of the development hereby permitted visibility splays shall be provided in full accordance with the details indicated on the approved plan (2021-F-015-020). The splay(s) shall thereafter be maintained at all times free from any obstruction exceeding 0.6 metres above the level of the adjacent highway carriageway.
- 19 Reason: In the interests of highway safety in accordance with the principles of the NPPF.
- 20 Condition: Any access gates/bollard/chain/other means of obstruction shall be hung to open inwards, set back, and thereafter retained a minimum distance of 20 metres from the near channel edge of the adjacent carriageway. Any sidewalls/fences/hedges adjacent to the access shall be splayed at an angle of 45 degrees from each of the outside gateposts to the front boundary of the site.

- 20 Reason: In the interests of highway safety enabling vehicles to safely draw off the highway before the gates/obstruction is opened.
- 21 Condition: Prior to the first use of the development hereby permitted the proposed on-site access, car and cycle parking, on-site pedestrian & cycle paths and accesses, servicing, loading / unloading, turning and waiting areas shall be laid out, demarcated, levelled, surfaced and drained in accordance with the approved master plan(s) and retained thereafter available for that specific use.
- 21 Reason: To ensure the permanent availability of the parking/manoeuvring areas, in the interests of satisfactory development and highway safety.
- 22 Condition: Prior to first occupation/use of the development hereby permitted a scheme for the parking of cycles shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall be fully implemented before the development is first occupied or brought into use and thereafter retained for this purpose.
- 22 Reason: To ensure the provision of adequate cycle parking that meets the needs of occupiers of the proposed development and in the interests of encouraging the use of sustainable modes of transport.
- 23 Condition: Prior to first use of the development hereby permitted, details of a suitable electric vehicle charging scheme shall be submitted to and approved by the LPA. The scheme shall be implemented as approved prior to the first use of the development hereby permitted.
- 23 Reason: To ensure the charging is safe, accessible and convenient in accordance with section 112(a) of the NPPF, AQAP, emerging local policy LP14/18 and the NCC's parking standards (July 2022).
- 24 Condition: The development shall not be brought into use until a scheme for the provision of fire hydrants has been implemented in accordance with a scheme that has previously been submitted to and approved in writing by the Local Planning Authority.
- 24 Reason: In order to ensure that water supplies are available in the event of an emergency in accordance with the NPPF.
- 25 Condition: Notwithstanding the information that accompanied the application, prior to the first occupation of the development hereby approved, a detailed outdoor lighting scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the type of lights, the orientation/angle of the luminaries, the spacing and height of the lighting columns, the extent/levels of illumination over the site and on adjacent land and the measures to contain light within the curtilage of the site. The development shall be implemented in accordance with approved scheme prior to the first occupation of the development hereby permitted and thereafter maintained and retained as agreed.
- 25 Reason: In the interests of minimising light pollution to safeguard the amenities of the locality and minimise the impact on bats in accordance with the NPPF and Development Plan.
- 26 Condition: The development hereby permitted shall be constructed in accordance with the Mitigation measures outlined at paragraphs 5.3 and 5.4 of the Preliminary Ecological Appraisal (PEA) that accompanied the application (Ref: BOM-RSC-21-11, dated July 2022 undertaken by Bombus Ecology), and prior to first use of the development hereby

permitted the Enhancement measures listed at 5.9 of the same report shall be implemented and thereafter be retained and maintained.

- 26 Reason: In the interests of ecology and biodiversity in accordance with the NPPF and Development Plan.
- 27 Condition: The development hereby permitted shall be constructed in accordance with the Mitigation measures outlined at paragraphs 5.3 – 5.6 inclusive of the Bat Risk Assessment (BRA) that accompanied the application (Ref: BOM-RSC-21-11, dated May 2022 undertaken by Bombus Ecology), and prior to the first use of the development hereby permitted the Enhancement measures listed at 5.10 of the same report shall be implemented and thereafter be retained and maintained.
- 27 Reason: To reduce impacts on bats in accordance with the NPPF and Development Plan.
- 28 Condition: The development hereby permitted shall include no more than 10no. yurts. No single yurt shall have an internal floor area of more than 50m² and a height of 4 metres. The 3no. yurts located within the wooded area shall have a base height no higher than 2m.
- 28 Reason: For the avoidance of doubt and in the interests of proper planning.
- 29 Condition: The yurts hereby permitted shall only be occupied as short-stay holiday lets (no more than 28 days per single let), shall only be made available as commercial holiday lets and shall not be occupied as a person's sole or main place of residence.
- 29 Reason: The site lies within in an area in which the Local Planning Authority would not normally permit permanent residential development. This permission is granted because accommodation is to be used for holiday purposes only in accordance with the NPPF.
- 30 Condition: The owners / operators of the development hereby permitted shall maintain an up-to-date register of lettings/occupation of the yurts hereby permitted and shall make this available at all reasonable times to the Local Planning Authority.
- 30 Reason: The site lies within in an area in which the Local Planning Authority would not normally permit permanent residential development. This permission is granted because accommodation is to be used for holiday purposes only in accordance with the NPPF.
- 31 Condition: The yurts hereby permitted shall at all times be held, owned and operated in association with Wild Ken Hill, Heacham and shall not be sold off separately.
- 31 Reason: For the avoidance of doubt and to ensure that the building is not used for unrelated purposes that would be incompatible with the provisions of the NPPF and Policy DM11 of the SADMPP 2016.
- 32 Condition: The development hereby permitted shall be operated in full accordance with the Business Plan that accompanied the application (dated November 2022) and the supplementary information contained within an email from the agent (James Ellis) dated 09 March 2023, 12:24 unless otherwise agreed in writing by the Local Planning Authority.
- 32 Reason: To ensure satisfactory operation of the site in accordance with the NPPF and Development Plan.

- 33 Condition: Construction or development work on site, along with collections and deliveries of waste products, material and equipment, shall only be carried out between the hours of 0800 and 1800 weekdays, and 0900-1300 on Saturdays, with no work allowed on Sundays and Bank/Public Holidays.
- 33 Reason: In the interests of the amenities of the locality in accordance with the principles of the NPPF.
- 34 Condition: No development shall take place on any external surface of the development hereby permitted until samples of all materials to be used for the external surfaces of the new and refurbished buildings, including roof materials, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
- 34 Reason: To ensure a satisfactory external appearance and grouping of materials in accordance with the principles of the NPPF.
- 35 Condition: Notwithstanding the approved plans or additional information that accompanied the application, prior to any works above ground floor finish floor level of the development hereby approved, full details of both hard and soft landscape works shall have been submitted to and approved in writing by the Local Planning Authority. These details shall include the permanent and overflow car park areas (including car park surfaces), access ways, permissive footpaths, outdoor play area (including equipment), service tracks, outdoor seating area, dog walking enclosure, refuse or other storage units, internal signate, other 'street' furniture, structures and minor artefacts and shall include finished levels / contours and materials. Soft landscape works shall include planting plans, written specifications (including cultivation and other operations associated with plant and grass establishment) schedules of plants noting species, plant sizes and proposed numbers and densities where appropriate of all landscape areas and shall include boundary treatments.
- 35 Reason: To ensure that the development is properly landscaped in the interests of the visual amenities of the locality in accordance with the NPPF.
- 36 Condition: Prior to the first use of the development hereby permitted or within the first planting season all hard and soft landscape works shall be carried out in accordance with the details approved under Condition 37 of this permission. The works shall be carried out prior to the occupation or use of any part of the development or in accordance with a programme to be agreed in writing with the Local Planning Authority. Any trees or plants that within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species as those originally planted, unless the Local Planning Authority gives written approval to any variation.
- 36 Reason: To ensure that the work is carried out within a reasonable period in accordance with the NPPF.
- 37 Condition: The proposed bike trails shall be constructed in accordance with the specifications contained in the On Track MTB Trails Design Specification, July 2022 that accompanied the application unless otherwise agreed in writing.
- 37 Reason: In the interests of the amenity of the locality in accordance with the NPPF and Development Plan.

- 38 Condition: The development hereby permitted, other than occupation of the yurts, shall be open to the public between the hours of 09:00 and 18:00 only other than on 20 specific days in any one calendar year when the site can be open for specific events hosted by Wild Ken Hill / Ken Hill Experience and intrinsically linked to the use of the site and surrounding land as a nature diversification project until 22.30.
- 38 Reason: In the interests of the amenity of the locality and occupiers of neighbouring non-associated dwellings in accordance with the NPPF and Development Plan.
- 39 Condition: No deliveries shall be taken at or despatched from the site outside the hours of 0700 and 1900 on weekdays, 0900 and 1700 on Saturdays and 1000 and 1700 on Sundays, Bank or Public Holidays.
- 39 Reason: In order that the Local Planning Authority may retain control over the development in the interests of the amenities of the locality and occupiers of nearby non-associated residential properties in accordance with the NPPF.
- 40 Condition: The development hereby permitted shall comprise of no more than 362m2 GIA of retail space and no more than 4no. individual units. The sale of goods shall be limited to that intrinsically linked to the wider use of the site and surrounding land as a nature diversification project. Products sold could include: optical equipment associated with bird and other wildlife watching or dark skies enjoyment, orienteering, conservation, foraging, camera equipment, books, manuals, maps, small-scale food retail (not exceeding 20% of the total GIA of retail space) to include predominantly food derived from the land holding or surrounding areas, bike equipment, outdoor equipment and accessories associated with the activities available at the site and surrounding land, but not including equipment relating to off-site activities such as water-based activities.
- 40 Reason: To ensure an appropriate retail element in accordance with the NPPF and Development Plan.
- 41 Condition: Prior to the first use of the development hereby permitted, details of information boards, to be located within the application site (locations to be approved as part of this condition) that indicate nearby public rights of way and alternative visitor attractions not in the proximity of designated sites as well as details of nearby designated sites and recreational pressures upon them shall be submitted to and agreed in writing by the local planning authority. The approved information boards shall be erected in the approved locations, and thereafter be retained and maintained in those locations prior to the first use of the development hereby permitted. Additionally, information leaflets, containing the same information as the information boards shall be made available to all visitors of the development.
- 41 Reason: To ensure the development does not have a likely significant impact on protected sites in accordance with the NPPF, Development Plan and Habitats Regulations.

B REFUSE Should the S.106 legal agreement fail to be signed within 4 months of the resolution to approve, on the grounds that it fails to secure the safeguarded land.

Summary of feasibility findings for Heacham A149 Lamsey Lane

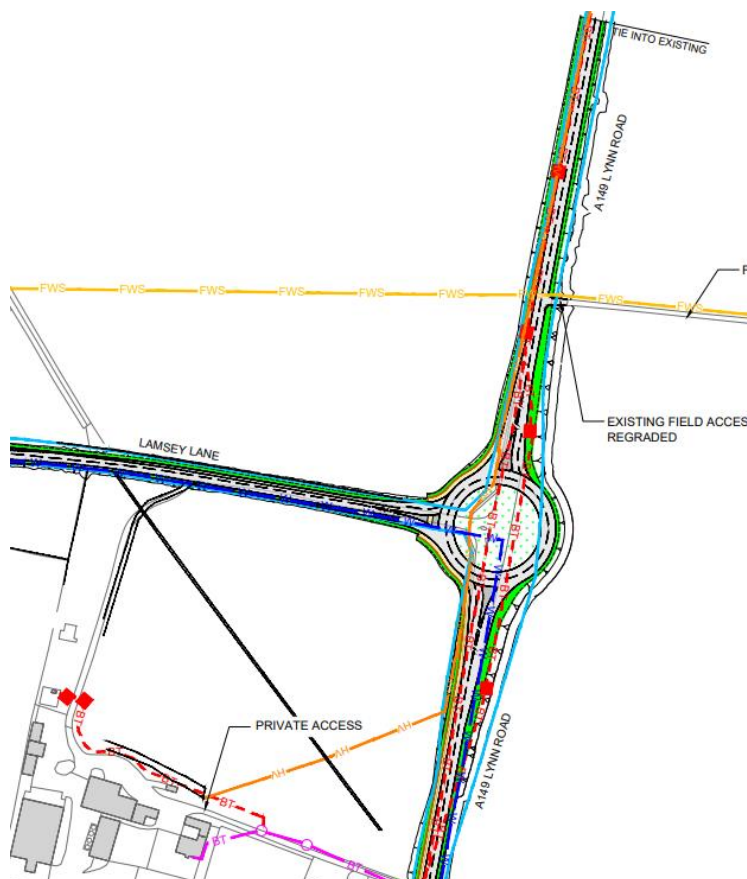
A149 Heacham Lamsey Lane junction

WSP developed options were based on differing arrangements of signalised junction's vs roundabouts with arrangements to give buses priority when exiting Lamsey Lane on a right turn toward Kings Lynn. These options were discounted for costs reasons with estimates ranging between £2.6m & £4.7M

WSP were asked to develop a simple roundabout vs simple traffic signals with no bus priority as a base comparison as either form of junction improvement will produce benefits in bus journey time reliability.

Options:

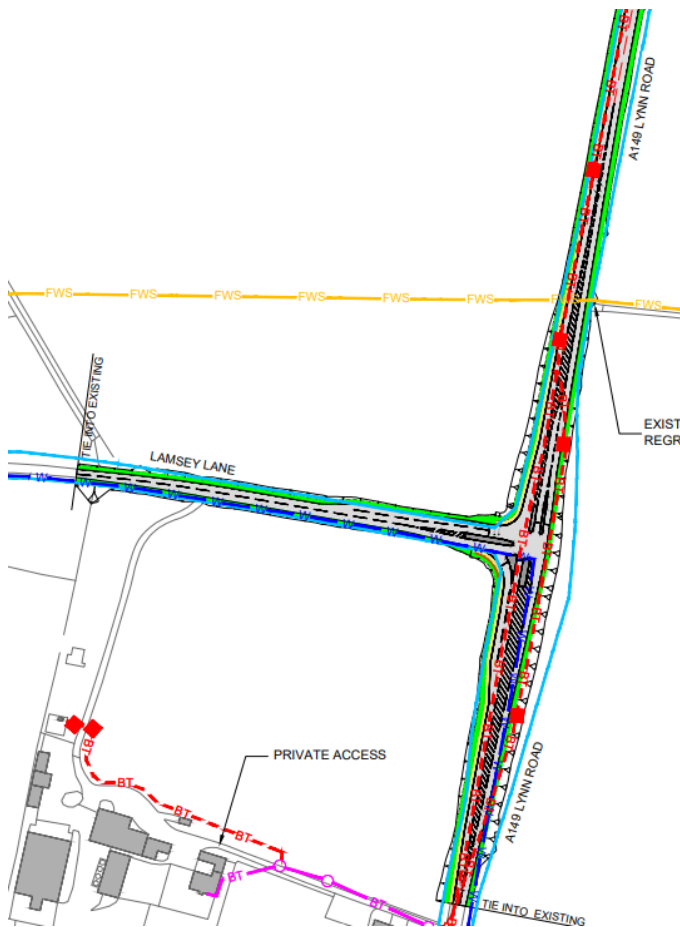
Option 1 - Roundabout



Note – the surrounding land is currently subject to a planning application to deliver a regenerative farm and re wilding site with some accommodation known as Wild Ken Hill. As part of the discussions the land owner has offered land to the west of the site with a condition that highway works commence within 15 yrs, so it is likely that for buildability the roundabout will be built offline to the west.

Advantages	Disadvantages
The reduction of speed limit from 60mph to 50mph would improve the overall safety of the junction where vehicle speeds pose a risk.	Due to the existing vertical alignment, extensive earthworks are anticipated as part of the proposal to ensure sufficient forward visibility is attained.
Provision of informal crossing would provide a safer crossing point for non-motorised users.	Retaining structure or reinforced earthworks may be required along A149 Lynn Road near the existing properties (Heacham Bottom Cottages) if deemed necessary. This will increase the overall project cost.
Existing bus stops present along A149 Lynn Road is to be retained.	Additional land take would be required on the adjacent fields to facilitate implementation of the proposal
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem	Existing statutory utilities within the scheme extents would require diversion.
The existing landscape would be retained.	The existing private accesses within the scheme footprint arms would need to be re-aligned.
	Delays to bus journey times is anticipated.

Option 2 - Traffic Signals



Advantages	Disadvantages
The reduction of speed limit from 60mph to 50mph would improve the overall safety of the junction where vehicle speeds pose a risk.	Due to the existing vertical alignment, extensive earthworks are anticipated as part of the proposal to ensure sufficient forward visibility is attained.
Provision of signalised crossing would provide a safer crossing point for non-motorised users.	Delays to bus journey times is anticipated.
Existing bus stops present along A149 Lynn Road is to be retained.	Retaining structure or reinforced earthworks may be required along A149 Lynn Road near the existing properties (Heacham Bottom Cottages) if deemed necessary. This will increase the overall project cost
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem.	Additional land take would be required on the adjacent fields to facilitate implementation of the proposal.
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem.	Existing statutory utilities within the scheme extents would require diversion.
The existing landscape would be retained	The existing private accesses within the scheme footprint arms would need to be re-aligned
	Street lighting would need to be provided to ensure the overall junction is illuminated during

Costs and Value for Money

Option 1 – Roundabout

Predicted costs for 24/25 year build - £2.489m

BCR (considering accident saving and congestion delays) – **Low 1.3:1**

Option 2 – Traffic Signals

Predicted costs for 24/25 year build - £2.566m

BCR (considering accident saving and congestion delays) – **Poor 0.7:1**
 saving and congestion delays) – **Poor 0.91:1**

Wild Ken Hill
carl TONKS consulting

The Wild Ken Hill Experience

Highways Position Statement

Summary

The Wild Ken Hill proposals were considered by Planning Committee on 3rd April 2023. The committee deferred a decision to seek more information from NCC on a feasibility assessment of the A149 / Lamsey Lane junction. The Local Highway Authority (LHA) had recommended approval of the proposals following detailed review of the Transport Assessment (TA) and further Technical Note 2 TA Addendum from **CTC**.

Planning Committee did not request any additional highways information or analysis from the applicant (only a request from NCC on its strategic work). However, a representative of the Parish Council had queried the highways data collected as part of the TA and on that basis, notwithstanding that the applicant, LHA and LPA are in agreement in relation to this, additional information has been procured by way of Automatic Traffic Count (ATC) in April 2023.

The ATC data has been assessed in relation to the original (TA) and the data presented therein, in addition to the update in Technical Note 2. The comparisons shown below confirm that the results published in the TA/Technical Note 2 and those produced from the 2023 ATC result indicate only minor and insignificant differences. Typically, variance in the traffic flows and capacity analyses are within what one would normally anticipate in daily variation and show a reduction in RFC and queuing from the 2023 ATC figures compared to those published in the TA/Technical Note 2, from the MCC. The only deleterious result in this comparison is an increase of only 0.01 in RFC for one movement. Only minor increases in queuing are seen when the 2023 ATC results are factored in and, again, these are within what one would normally anticipate by way of daily variation. Hence, this validates the original traffic survey data used in the TA. Further sensitivity tests factoring to the high tourist season (August) during the peak hours therein confirm spare capacity and low levels of queueing post development.

On that basis, it is clear that the Transport Assessment which has in any case been approved by the LHA, which therefore has no objection to the proposals provides a robust and sound basis for a positive decision on the application.

Statement

1. **cTc** has produced a substantial quantity of analyses on behalf of Wild Ken Hill and examining key traffic and highway matters in regard to their proposed development, at Heacham Bottom and associated camping facilities at Mount Pleasant. The proposals were considered by Planning Committee on 3rd April, at which the decision was deferred in order to seek an update from NCC on a feasibility assessment of the strategic junction improvements, which the LHA has been seeking for some time. This was despite the Application being subject to no objection from Norfolk County Council, as Local Highway Authority (LHA) who, following detailed technical discussions and submission of additional information by the Applicants, were supportive of the proposal.

2. Since the Committee, correspondence from Heacham Parish Council has further emphasised their concerns in regard to the traffic analyses and identified detail points previously touched upon only in passing. This Position Statement will address the issues raised in that additional correspondence, and will demonstrate that the analyses submitted in the Transport Assessment were and remain fit for purpose and appropriate for the decision-making process. This notwithstanding, additional data collection has been undertaken, in order to provide additional confidence in the conclusions reached. This is also described below.
3. The latest correspondence from Heacham Parish Council refers to discussion within the Committee Meeting, at which it claims that the Highways Officer suggested a “**...data disparity...regarding COVID 19...had been added to the outcome of the dataset.**” This is a misunderstanding, in that what was agreed with Highways Officers was that the data collected was entirely appropriate in its basic form, however and this notwithstanding, further adjustments had been made in order to reflect periods of peak traffic demand and these adjustments had been agreed with Highways Officers as appropriate to permit a decision to be made in regard to the Application. The adjustments resulted in an onerous analysis scenario.
4. In regard to impact of COVID 19; the surveys were undertaken at the following periods;
 - Automatic Traffic Counts (ATCs) at two locations on Lamsey Lane
 - 2 weeks spanning 15th July to 28th July 2021; and
 - Manual Classified Count (MCC) – 24th July 2021.

5. In regard to the lifting of COVID 19 restrictions; on 22nd February 2021, then Prime Minister Boris Johnson published “...***the government’s roadmap to cautiously ease lockdown restrictions in England.***” This set out a four-stage approach to releasing constraints on travel and activity, each stage as broadly summarised below;

- Step 1 – Children and students return to face-to-face education.
March 8th

“Wraparound childcare” and other supervised children’s activities recommence.

Care home residents permitted one regular visitor.

People permitted to leave home for outdoor recreation with their family or support bubble.

Some practical courses at University permitted to restart face-to-face teaching.

From 29th March “Stay at Home” Order ended, although some restrictions remained, including Work From Home where possible.

- Step 2 – Non-essential retail, personal care and public buildings re-open.
April 12th

Most outdoor attractions re-open, although indoor mixing remained prohibited.

Indoor facilities (e.g. gyms, swimming pools) re-open, although mixing of family groups remains controlled.

Hospitality venues open outdoors only.

Self-contained accommodation (e.g. holiday lets) re-open.

Funerals permitted with up to 30 people and weddings/commemorative events with up to 15 people.

- Step 3 – Most social contact rules (outdoors) lifted, but gatherings May 17th to be no more than 30 people.

Outdoor performance venues re-open, although limits remain controlling inter-group contact.

Larger performances and sporting events permitted up to 1,000 people (indoor) and 4,000 people (outdoor).

Up to 10,000 people permitted in the larger outdoor venues.

Up to 30 people permitted to attend weddings.

- Step 4 – All legal limits on social contact removed. June 21st

Nightclubs re-open and no restrictions on large events.

6. In fact, implementation of the final Stage (Stage 4) of the Roadmap described above was delayed from June 21st, as planned, to July 19th. This resulted in the first four days of the pre-school holiday ATC being undertaken under Stage 3 of Lockdown Easing, whilst the remaining survey days were under Stage 4, which saw only minimal constraint. Those remaining constraints most notably included international travel, which was only permitted under specific circumstances or for specific reasons, which did not include family holidays. Consequently, the summer of 2021 saw a significant shift away from British families holidaying abroad and instead, remaining in the UK. This is likely to have resulted in additional demand for holidays in north Norfolk's holiday areas, with consequent impact on traffic flows on the A149, Lynn Road. On this basis, it was agreed with Highways Officers that the traffic surveys undertaken under Stage 4 of the Lockdown easing, as described above, would produce reliable results. Each week of the ATC was analysed independently; hence the second week was entirely within Stage 4 easing and the MCC was also under Stage 4 easing.
7. The reference to traffic flows having been factored to reflect increased demand is discussed in Section 5.2 of the Transport Assessment, which details the factoring of traffic surveyed flows, to reflect "peak of the peak" demand during August. The only data source available at the time of compiling these analyses enabled a comparison of March to August traffic demand and this was adopted in the capacity calculations presented. Notwithstanding this it is evident that the traffic survey undertaken in July and within the early stages of the school holiday period will have identified traffic demand much closer to the August holiday peak than would be the case in March of a "normal year" (ie non-COVID) and consequently, the application of a March to August factor to July surveyed flows clearly adds in a substantial and onerous safety margin to the analyses. This is the factoring to which the Highways Officer referred at Committee and clearly results in a forecast on which one can reasonably rely as over-stating the likely traffic demand.

8. Reference was made in the Parish Council submission to DfT summary figures for the years of 2020 and 2021, however, these include agglomerated traffic flows for periods spanning different Steps of the Lockdown easing, hence offer little, if any, insight into what was happening on the specific dates of the surveys.
9. It is clear, as stated above, that the Authority with technical competence to adequately review and comment on the analyses undertaken (ie. the Local Highway Authority) is in agreement with the data collected and the results of the subsequent calculations. It is agreed that those calculations appropriately reflect the observed levels of junction operation and that;
 - The junction is not currently operating typically in breach of its capacity;
 - The junction models and the conclusions drawn therefrom are appropriate, accurately reflect observations and are fit for purpose in determining the Application; and consequently,
 - There are no valid grounds for refusal of permission on highway or traffic impact.
10. Two key points feed into the above;
 1. Was the data collected at an appropriate time and therefore fit for purpose?
And,
 2. Are the analyses undertaken using those data appropriate, hence adequate to rely on in determining the level of traffic impact of the proposals.

11. The data was collected at a time agreed with officers of the LHA as neutral and unlikely to be subject to any suppression due to the previous COVID lockdowns. As described in some detail above, at the time of survey, Britain was coming out of lockdown and the only remaining relevant constraint was on international travel. Consequently, British holiday-makers who may ordinarily have ventured abroad remained in the UK for their holiday.
12. As made clear to the Planning Committee, the data relied upon by objectors in fact confirms that over the month of the surveys, overall traffic demand was slightly below pre-COVID, however, that the beginning of the month was more severely constrained by COVID lockdown (Step 3), and consequently logic suggests that the latter part of the month (Step 4) actually saw traffic flows at, or above “normal” levels, in order at least in part to offset the impact of the early part of the month in lockdown. Consequently it is agreed with officers of the highway authority that the traffic surveys reflected traffic at, at least the level which could be expected under “normal” demand levels and potentially above.
13. The operating calculations confirmed that the junction operates significantly within its ultimate operating capacity in all forecast scenarios and will continue to do so into the future. The Ratio of Flow to Capacity (RFC) remains comfortably within design parameters for all scenarios examined and queue lengths remain small.
14. The capacity for traffic movements from Lamsey Lane on to Lynn Road is determined by the gaps between passing traffic on Lynn Road, which is predominantly a function of through-traffic volume, not traffic turning into, or out of Lamsey Lane and consequently when forecast generated traffic is added into the surveyed traffic flow, the junction continues to exhibit spare operational capacity and the additional turning traffic has little impact on junction operation.

15. This conclusion of the analyses reflects junction operation as witnessed on-site and is agreed by officers of the County Council, as competent technical authority with remit to control traffic and highway matters. It is clear from the above that the analyses confirm there is no defensible highway or traffic reason for refusal of Planning Permission in this instance.
16. Further to the above points, and these notwithstanding, **cTc** has commissioned further traffic surveys in the form of ATCs on both Lamsey Lane and Lynn Road. These were undertaken by Paul Castle Associates from 24th April 2023 to 3rd May 2023. This was done in response to criticisms from Heacham Parish Council, that the survey data agreed with Officers and used in the previously submitted TA and subsequent TA Addendum reports were not representative of typical traffic characteristics. Notwithstanding the above confirmation of the direct relevance of the data collected and in order to remove any potential criticism of the analyses, these additional surveys were commissioned. The full ATC reports are included as Appendix A.
17. The previous analyses from the MCC identified the following Saturday Peak Hours:
 - AM Peak Hour: 11:00 to 12:00
 - PM Peak Hour: 14:30 to 15:30
18. As the MCC records data in 15 minute time segments, a more precise peak hour is identified. The ATCs record hourly data and therefore identify peak hours to the nearest whole hour. Therefore, to ensure an accurate comparison and analysis, two PM hours from the ATC have been used, either side of the PM Peak Hour from the MCC. These details and comparisons are shown in Table 1, below.

Table 1: MCC and ATC Comparisons

Period	Location	Direction	MCC	ATC	Difference	Relevant Turning Movement	MCC to ATC Factor
11:00 – 12:00	Lamsey Lane	Eastbound	96	90	-6	Lamsey Lane to Lynn Road Left and Right	0.9375
		Westbound	214	196	-18	Not Used	N/A
	Lynn Road	Northbound	737	793	+56	Lynn Road S Left and Ahead	1.0760
		Southbound	581	586	+5	Lynn Road N Right and Ahead	1.0086
14:00 – 15:00	Lamsey Lane	Eastbound	79	92	+13	Lamsey Lane to Lynn Road Left and Right (14:30 – 15:00)	1.1646
		Westbound	223	208	-15	Not Used	N/A
	Lynn Road	Northbound	685	781	+96	Lynn Road S Left and Ahead (14:30 – 15:00)	1.1401
		Southbound	603	639	+36	Lynn Road N Right and Ahead (14:30 – 15:00)	1.0597
15:00 – 16:00	Lamsey Lane	Eastbound	103	85	-18	Lamsey Lane to Lynn Road Left and Right (15:00 – 15:30)	0.8252
		Westbound	210	205	-5	Not Used	N/A
	Lynn Road	Northbound	637	738	+101	Lynn Road S Left and Ahead (15:00 – 15:30)	1.1401
		Southbound	688	670	-18	Lynn Road N Right and Ahead (15:00 – 15:30)	0.9738

19. The above table demonstrates that, typically, traffic flow on Lamsey Lane is slightly lower in the 2023 ATC than the 2021 MCC; and slightly higher on Lynn Road. These are not vastly significant differences, and clearly any two traffic surveys conducted on different days would show a degree of variance. The differences observed are within the bounds of what one would normally expect by means of daily variation, especially once one allows for the passage of almost two years, during which normal traffic growth has occurred. Nonetheless, and in order to establish whether the variance has a bearing on the conclusions drawn previously regarding the Lynn Road / Lamsey Lane junction, further PICADY capacity analyses have been conducted using the factored base flows.

20. The factors stated above show the degree of variance between specific turning movements at the quoted times between the 2021 MCC and 2023 ATC. As such, they also act as temporal growth factors. Hence, once applied to the 2021 MCC base turning movements these are automatically growthed to 2023 turning movements; and are, therefore, directly comparable with the “2023 with Development” capacity analyses quoted in the TA. Figure 1 shows the factored turning movements with proposed development traffic as flow diagrams. Table 2, below, summarises the PICADY results, with the full PICADY report provided as Appendix B.

Table 2: PICADY Results for the Lamsey Lane / Lynn Road junction.

Scenario	Movement	From TA		Factored from 2023 ATC		Difference	
		Max RFC	Max Q	Max RFC	Max Q	Max RFC	Max Q
2023 + Dev AM Peak	Lamsey Lane to Lynn Road North	0.17	0.2	0.16	0.2	-0.01	0.0
	Lamsey Lane to Lynn Road South	0.16	0.2	0.15	0.2	-0.01	0.0
	Lynn Road to Lamsey Lane	0.13	0.1	0.13	0.1	0.00	0.0
2023 + Dev PM Peak	Lamsey Lane to Lynn Road North	0.40	0.6	0.34	0.5	-0.06	-0.1
	Lamsey Lane to Lynn Road South	0.32	0.5	0.27	0.4	-0.05	-0.1
	Lynn Road to Lamsey Lane	0.09	0.1	0.10	0.1	+0.01	0.0

21. The comparisons shown above confirm that the results published in the TA and those produced from the factors obtained from the 2023 ATC result in only minor and insignificant differences. Typically, variance in the capacity analyses show a reduction in RFC and queuing from the 2023 ATC figures compared to those published in the TA from the MCC. Hence, this validates the original traffic survey data used in the.

22. To further ensure a thorough and robust validation of the MCC survey data a seasonal uplift has been applied to the above analyses. As previously noted, the 2023 ATC from which the MCC has been factored was undertaken from 24th April 2023 to 3rd May 2023; as such the data has been further factored to August levels of base traffic flow. Norfolk County Council has provided additional seasonal monthly variation factors for the A149 at Heacham. This data confirmed the following AADF monthly variation factors (from the annual average month):
- April: 0.98
 - May: 1.08
 - April and May Average: 1.03
 - August: 1.37
23. The factor from April/May to August is, therefore:
- $1.37 / 1.03 = \mathbf{1.33}$
24. The summer uplift factor has been applied to the base flows; these are shown as flow diagrams in Figure 2, and with development in Figure 3. PICADY capacity analyses have been undertaken for these scenarios, and this is summarised in Table 3, below, with the full PICADY report presented as Appendix A.

Table 3: PICADY Results for the Lamsey Lane / Lynn Road junction (August 2023).

Scenario	Movement	Max RFC	Max Q
August 2023 No Dev. AM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.35	0.5
	Lynn Road to Lamsey Lane	0.13	0.2
August 2023 With Dev. AM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.40	0.6
	Lynn Road to Lamsey Lane	0.20	0.2
August 2023 No Dev. PM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.29	0.4
	Lynn Road to Lamsey Lane	0.16	0.2
August 2023 With Dev. PM Peak	Lamsey Lane to Lynn Road North	0.61	1.4
	Lamsey Lane to Lynn Road South	0.65	1.4
	Lynn Road to Lamsey Lane	0.16	0.2

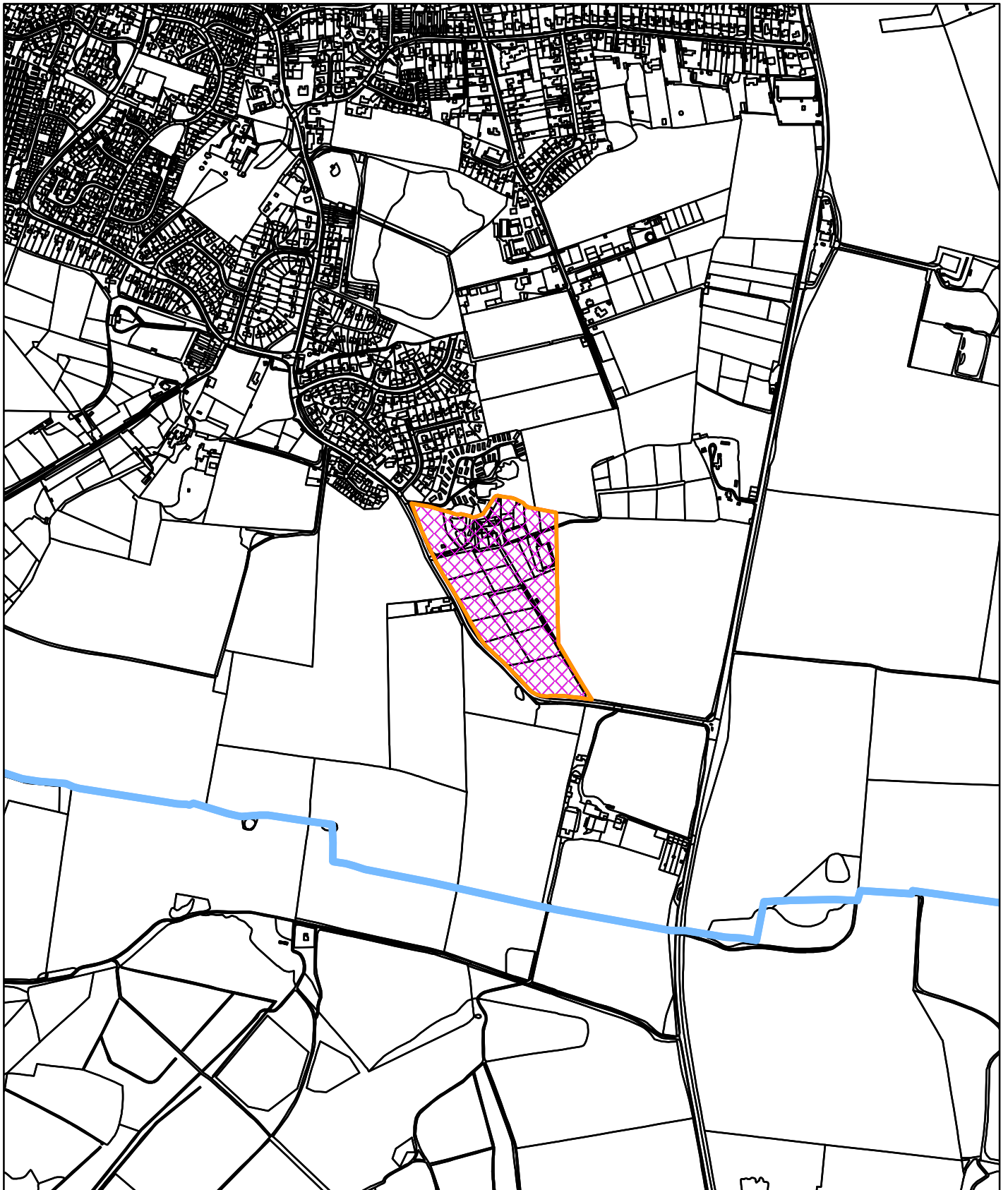
25. The capacity analyses quoted in the above table confirm that the junction operates within capacity, and with minimal queuing, both before and after the proposed development, even under this scenario of the peak hour within the peak month.
26. In conclusion, the 2021 MCC has been validated by the 2023 ATC data; and this supports the view of NCC Officers that accepted the TA and all of its' analyses. Furthermore, capacity analyses under peak hour in high season conditions show spare capacity remains at the Lamsey Lane / Lynn Road junction.

Client:		Wild Ken Hill	
Project Name:		The Wild Ken Experience	
Project Number:		2021-F-015	
Report Title:		Highways Position Statement	
Created by:	Carl Tonks	Date:	May 2023
Proofed by:	Jacqueline Ireland	Date:	May 2023
Approved by:	Carl Tonks carl@tonks-consulting.co.uk	Date:	May 2023
www.tonks-consulting.co.uk		01179 055 155	

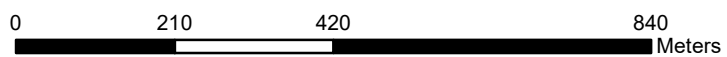
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22/01650/FM

Mount Pleasant Farm 25 Lamsey Lane Heacham PE31 7LE



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Ordnance Survey 100024314



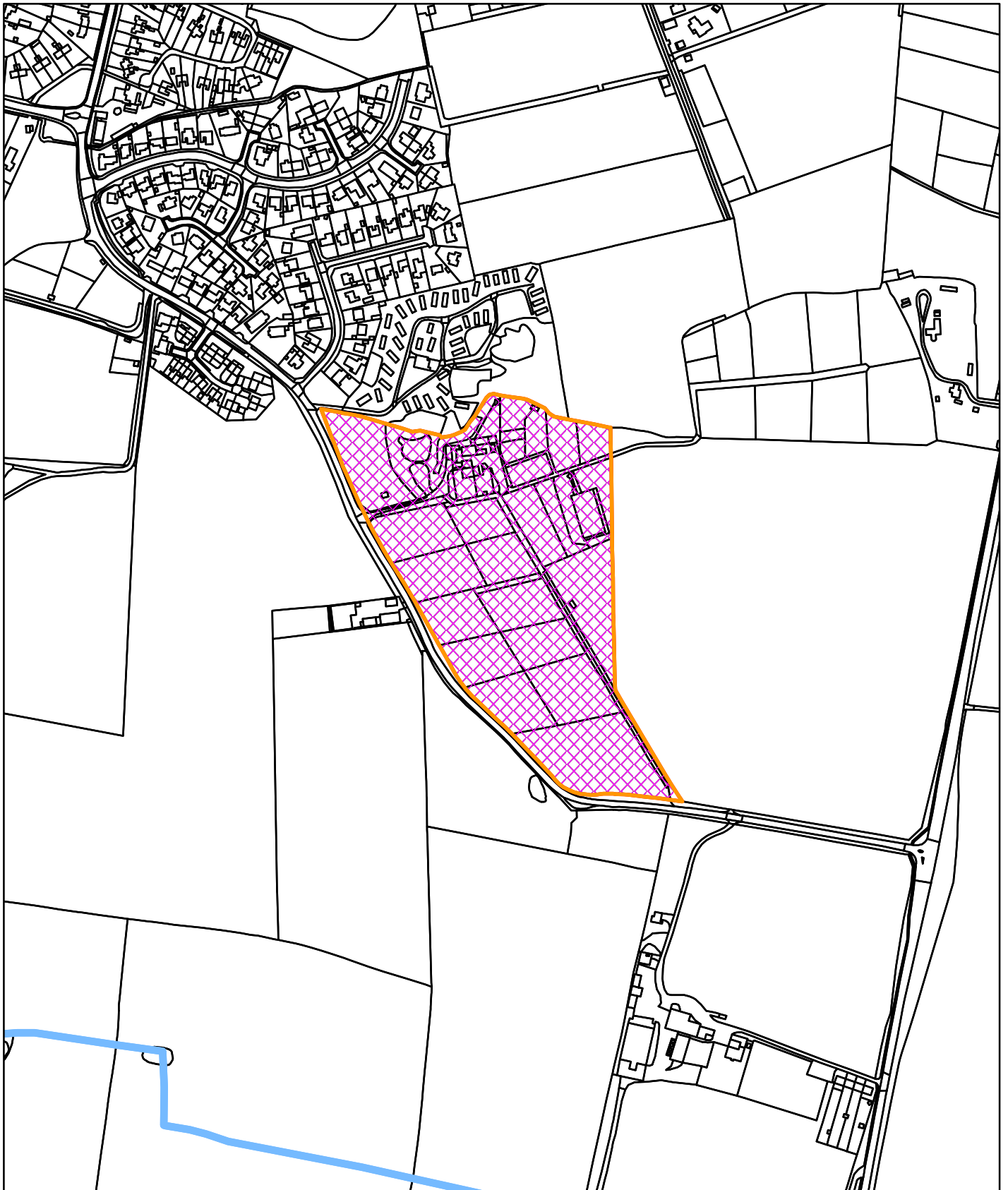
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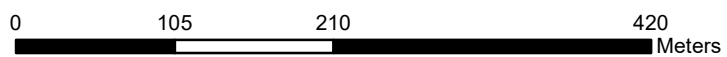


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22/05/2023

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Parish:	Heacham Snettisham	
Proposal:	Change of use of land to provide 20 touring caravan pitches with hard standing; change of use of land to create areas for camping and grass touring caravan pitches; change of use of existing buildings and new building to provide - visitor utility building, reception/retail area and storage area, creation of parking area (temporary parking/drop off) new landscaping and off road path.	
Location:	Mount Pleasant Farm 25 Lamsey Lane Heacham Norfolk	
Applicant:	Wild Ken Hill	
Case No:	22/01650/FM (Full Application - Major Development)	
Case Officer:	Mrs N Osler	Date for Determination: 5 January 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Deferred from April 3 Planning Committee
Called in by Cllr Parish

Neighbourhood Plan: YES

Members Update

Some Members will recall that, although the Local Highway Authority (LHA) concluded that the proposed development would not result in the need to seek significant improvements to Lamsey Lane / Lynn Road junction, this application was deferred from April 3 Committee to enable an update on an ongoing feasibility study by Norfolk County Council for improvements to this junction. Additionally, the Parish Council requested additional traffic information be submitted because they queried the findings of the Automatic Traffic Count (ATC.)

The feasibility study has now been concluded and looked at various traffic light and roundabout options, some with bus priority measures, to improve traffic flow at the junction. The simpler forms of these are lower-cost and require less land take, whereas the roundabout option, which is a typical solution for principal road junctions, would involve higher costs and more land take. A summary of the findings of the feasibility study is attached as Appendix 1 to this report.

Additionally, as agreed via late correspondence on 3rd April Condition 24 has been duly corrected to refer to Condition 23.

The following report has not been updated other than in relation to the latest comments from the Local Highway Authority and minor amendments which have been emboldened for ease.

Case Summary

Full Planning Permission is sought for a holiday site comprising 20 x touring caravan pitches, 40 x grass tent pitches and 7 x bell-tent pitches. The development includes the change of use of an existing brick-built stables and office building to a reception / retail / storage building, the erection of a new building to accommodate a utility building, the creation of a parking area along with landscaping and off-road access tracks and paths.

The site measures c.6.5ha and comprises an existing farmhouse and garden, and various outbuildings such as stables, storage and an office associated with the existing permitted equine use of the site.

The development would involve the demolition of four of these out buildings (357m²) retaining the farmhouse and brick-built stables / office building.

The site lies outside of the development boundary for Heacham in land designated as countryside.

The site falls within Flood Zone 1.

It is suggested that the proposed development would generate 7.75 full time equivalent jobs.

Key Issues

Environmental Impact Assessment

Principle of development

Form and character and impact on the countryside

Highway safety

Impact on neighbour amenity

Ecology / Biodiversity

Drainage

Crime and Disorder

Any other matters requiring consideration prior to determination of the application

Recommendation

A) APPROVE subject to signing of a S.106 legal agreement to secure safeguarded land around the Lamsey Lane junction for potential future highway improvements for a period of 15 years from the date of decision, and the imposition of the conditions set out in the committee report.

B REFUSE Should the S.106 legal agreement fail to be signed within 4 months of the resolution to approve, on the grounds that it fails to secure the safeguarded land.

THE APPLICATION

Full Planning Permission is sought for a holiday site comprising:

- 20 x touring caravan pitches (serviced hardstanding pitches)
- 40 x mixed grass tent pitches and
- 7 x bell-tent pitches (grass pitches)
- Off-site highway improvement works comprising:
- Provision of a new pedestrian footway connection between the Mount Pleasant site (on-site path) and Heacham (existing highway footway)

- Provision of a pedestrian crossing of Lamsey Lane between the Mount Pleasant & Heacham Bottom sites

The development includes the change of use of an existing building to accommodate a reception / retail / storage area, the erection of a new building to accommodate a visitor utility building, the creation of a 20-space car park along with landscaping and off-road paths / access tracks.

The site measures c.6.5ha and comprises an existing farmhouse and garden, and various outbuildings such as stables, storage and an office associated with the existing use of the site as a riding centre and livery yard.

The development would involve the demolition of four of these buildings (357m²) retaining the farmhouse as staff accommodation and brick-built stables and office building for reception / retail and storage. A new utility building would be constructed accommodating kitchen / wash up area, utility, drying room WCs, showers, storage and plant room.

The vast majority of the 6.5ha site would remain undeveloped.

SUPPORTING CASE

Please see Wild Ken Hill's original applicant statement below. We are grateful for the many positive comments on our proposals from Councillors at Planning Committee in April. We appreciate why Councillors sought to defer the applications to seek a full update from Norfolk County Council (NCC) on its feasibility study even if the delay was disappointing. NCC continue to support our applications and their most recent response confirms the proposals will have no material impact on the A149 junction. Since the committee meeting, I have met with Ward and Parish Councillors and those who spoke at committee to further understand everyone's views, to commit to continued collaboration, and to align around the idea that these proposals could act as the catalyst for solutions to existing transport concerns. Both of these interdependent and linked planning applications are absolutely crucial to the future of Wild Ken Hill and following the deferral we hope that Committee will support us to continue our pioneering work.

"Last September, we submitted planning applications for facilities at Wild Ken Hill that will allow us to host, engage, and educate a wider range visitors and residents about the exciting nature restoration work taking place here. Our vision is to allow greater connection with nature and the outdoors at Wild Ken Hill, with sustainable facilities that work for all, including a new off-road path to better link Snettisham and Heacham and new wildflower meadows, as well as a dog walking area with free parking. We truly think the plans will be a great boost for people, wildlife, and climate.

Hopefully everyone at the Council will be aware of the pioneering Wild Ken Hill project which began in 2019 and includes a nationally-unique mix of regenerative farming, rewilding, and traditional conservation practices. We have embarked on these changes as we believe land must be used to benefit wildlife, climate, and people. Many will have seen Wild Ken Hill featured on the BBC's The Watches. Locally, we have also already created 2 new jobs, 15 volunteering opportunities, opportunities for students, a new nature festival, as well as hosting 2-3 open days for local residents, welcoming children on around 200 days per year, and operating 200+ acres of permissive access.

This, however, is just a start – we are extremely constrained by a lack of facilities. In order to fulfil our vision of fighting climate change and restoring biodiversity across the UK, Wild Ken

Hill needs the ability to welcome more visitors for higher quality, longer stays. In addition, several key strands of our land management work – in particular the rewilding project – are only funded until 2028. Developing a sustainable, year-round, nature-based tourism business through these proposals would create certainty for the financial future of this important nature recovery work, and indeed allow us to fund more projects and community engagement work in our local area. Without it, however, the future of the Wild Ken Hill project would be an uncertain one.

As such, the intention behind our proposals at Heacham Bottom Farm and Mount Pleasant is to create a high quality built and natural environment which serves as an exemplar for sustainable, nature-based tourism and education which contribute significantly to the local area. The planning applications are intrinsically linked and interdependent. They have only been made following detailed engagement with Planning, Economic, Highways, and Natural Environment Officers across the Borough Council and other organisations.

We are pleased to note wide ranging support for the proposals. On ecology and landscape, the Norfolk Coast Partnership (NCP) stated “The NCP is supportive of development that serves to help boost the local economy and improve access to and understanding of the AONB whilst protecting and enhancing the special qualities”, and the Norfolk County Council (NCC) Protected Landscapes stated “We believe that the proposals submitted by Wild Ken Hill not only are compatible with the LAC (Limits of Acceptable Change) framework, but could be used as a case study for how the LAC should be applied, and even an exemplar project to showcase externally, including to other developers.” Wild Ken Hill is also pleased to note that Natural England and the RSPB have raised no objections to the proposals.

Regarding tourism, the Borough Council’s Regeneration and Economic Development Team stated: “The Wild Ken Hill proposal demonstrates a focus on sustainable tourism and local environmental issues which has already gained national exposure and interest. The proposal will (therefore) support delivery towards the strategic tourism and economic objectives of the Borough Council.”

Following early-stage and thorough engagement with NCC Highways we are pleased that it offers no objection to the applications. Our proposals include an off-road path which will create the safest and most direct sustainable route between Snettisham and Heacham, new traffic signage, and improvements to the Heacham Bottom bus stops on the A149. In addition, we will be closing two accesses from the farmyard onto the A149 and removing agricultural vehicle movements.

Officers of the Borough Council and County Council have rigorously assessed our applications. There is no objection to the applications from any technical consultee.”

PLANNING HISTORY

08/01074/CU: Application Permitted: 29/05/08 - Change of use of buildings to riding centre and livery yard

RESPONSE TO CONSULTATION

Heacham Parish Council: OBJECT The idea of a touring caravan site and a legal camping site would be good for the tourism of Heacham, but as in the Heacham Bottom application we have the same objections and issues.

The number of vehicles accessing Lamsey Lane off the A149 to both applications, which after 50m is national speed limit (60MPH!) will cause problems at the junction A149/Lamsey Lane. Although visitors may not all turn up at once, you can never predict the holiday traffic. Listen to roads reports on local radio for the A149 during the summer.

Add to this the other application of Heacham Bottom by Ken Hill, with their expected visitors and up to 20 touring caravans plus camping enthusiasts also wishing to traverse this road, it will be a nightmare, not only for visitors to Hunstanton 3 miles north, local residents, who use the road, but the bus service which uses Lamsey Lane to service the whole village via this junction.

We think the traffic will also impact on nature conservation, which Wild Ken Hill has been all about.

Impact on the village. If the queue for the exit and the A149 junction blocks up, drivers will exit right and drive through the village to the controlled junction at the Lavender centre. This is something Ken Hill have said they do not wish to happen.

The current road structure does not support a scheme as presented here.

The transport assessment states no impact as there are bus services and footpaths. Heacham, prior to 1969 had a railway link to Kings Lynn, but as people used cars more, rail traffic declined, and this section of the line was closed. Nothing has changed, people still prefer to go out for the day with their own transport.

The current access road off Lamsey Lane is not a good access point for touring caravans nor vehicles, even though in the past it was an access point for the equestrian business.

One presumes that the vehicles towing the touring caravans will be parked beside their units, the only parking shown is "temporary" parking, so where will vehicles belonging to the pitched tents park? If opposite in the Heacham Bottom car park, then having families trying to get across Lamsey Lane to access the bridleway to the campsite could be dangerous.

Plans are unfortunately considered in isolation, but what also has to be considered here, is that the Plans for the 160 plus dwellings approved for Cheney Hill will also add traffic to A149/Lamsey Lane junction.

The following statement was submitted as late correspondence to 3rd April Planning Committee: This information covers the Transport assessment of both parts of the development Heacham Bottom and Mount Pleasant, as the Transport assortment is for both sites.

Whilst we are objecting to this application as it stands, we don't object to the principle of this development. In fact, we think it will be good for the area; provide awareness of the countryside, promote a healthy lifestyle, be good for tourism and could provide local jobs. Despite this we believe the application as it stands is flawed.

The very busy Village of Heacham really has only two ways in and out. Lamsey Lane and the Norfolk Lavender junction. Due to poorly designed road layouts large vehicles can struggle to enter either of these roads when traffic is queueing to exit.

In its conclusion the applicants Transport Assessment states there is significant spare capacity at the nearby Lamsey Lane / Lynn Road junction, including during high tourist season; If you travel along the A149 from Kings Lynn to Heacham, at weekends and holidays, when this site will be at its busiest, it's common for traffic to be backed up to

Knights Hill roundabout. This road to the coast is like a cul-de-sac, it's really the only way in and out.

Turning right out of Lamsey Lane can be challenging on a quiet day due to the speed of traffic, but on a busy day it's a nightmare. This is a main bus route, and they can often struggle to exit safely. The junction needs significant improvement, and we consider this should be funded out of a section 278 highways agreement, by the developer.

I spent 13 years as senior project lead for Cambridgeshire Highways, so I understand the complexities of The Transport Assessment, which is 306 pages long, a complex technical document. It relies completely on collected traffic data for its assumptions and that is data is very wrong.

Two traffic counts were carried out for the Transport Assessment, in July 2021, the last week of school term and the first week of school holidays. However, the DfT's website on Road Traffic Statistics under summary (<https://roadtraffic.dft.gov.uk/>) states that despite a rise from 2020 levels, car traffic for 2021 remained 15.8% lower than before the pandemic and were lower than 2011 traffic levels.

15.8% is a significant amount and this has been effectively ignored as part of the Traffic Assessment and therefore, makes it unsafe to rely upon for its conclusions. Traffic levels have largely returned to normal and are rising at about 2.2% annually so effectively the data could be wrong by as much 20% and has a knock-on effect throughout the report. It's impossible to see how a safe planning decision can be made on this basis.

In conclusion

We do want this project to go ahead. We believe it offers a worthwhile and beneficial uplift to the area. But only when we understand the road traffic safety implications, and after road safety issues have been properly assessed. Our primary concern is the impact on road safety, something that cannot be assessed with the current information provided and the safety and wellbeing of the community. We would ask that this application is delayed until a Transport Assessment with a corrected dataset is re-evaluated and mitigation measures properly considered.

We have issues with the design of the pedestrian crossing that in the drawing show it at an angle, this will mean pedestrians are on the road longer than needed. We would need to understand that this crossing on a 60 MPH road has been properly considered.

We have a concern over access to Mount Pleasant and specifically what will happen if a driver misses the turn with a caravan in tow. There is little, if any opportunity for drivers to turn round and the village roads narrow and become winding. This could cause safety issues.

Vehicles exiting onto Lamsey Lane are doing so onto a very fast road. We collect traffic data and this demonstrates that there is speeding along this stretch. This is near a bend. What mitigations are being considered to prevent accidents?

Snettisham Parish Council: OBJECT At a council meeting on 28th February 2023 Councillors considered the application and resolved to object to the planning application, they were concerned about the safety of pedestrians and other road users at the Lamsey Lane Junction and along routes to Snettisham. It was considered that this junction was already very busy and considered a danger locally. The increase in traffic brought by the development would also cause significant problems on already overburdened local infrastructure.

Highways Authority: NO OBJECTION Having considered the revised information submitted, I can confirm that there are no outstanding issues from a highway perspective.

The applicant has now submitted revised drawings, which demonstrate the indicative package of highway mitigation measures proposed within the previously submitted position statement and has revised the site layout to provide links to them.

In light of the above, I can confirm that the previously suggested conditions remain valid, updated to reflect the amended plan numbers.

As such, should you be minded to approve the application I would request conditions relating to access details and provision including visibility splays and gradient, parking provision, cycle provision, off-site highways works, closure of other access(s) and access obstruction, are included on any decision notice issued.

PROW: NO OBJECTION We have no objections on Public Rights of Way grounds as although Heacham footpath 15 is in the vicinity, it does not appear to be affected by the proposals.

Natural England: NO OBJECTION SUBJECT TO APPROPRIATE MITIGATION BEING SECURED.

It should be noted that Natural England amended their advice in relation to necessary mitigation when taking this site in isolation via an email received on 10 March. The below outlines the amended advice.

We consider that without appropriate mitigation the application could have an adverse effect on the integrity of:

- The Wash and North Norfolk Special Area of Conservation (SAC)
- The Wash Special Protection Area (SPA)
- The Wash Ramsar
- North Norfolk Coast SPA
- North Norfolk Coast Ramsar
- Damage or destroy the interest features for which the following Sites of Special Scientific Interest (SSSI) have been notified
 - The Wash Site of Special Scientific Interest (SSSI)
 - North Norfolk Coast SSSI

In order to mitigate these adverse effects and make the development acceptable, the following mitigation measures are required to be secured:

- A financial contribution to be paid into the Norfolk Green Infrastructure and Recreational Avoidance Mitigation Strategy (GIRAMS).
- Provision of leaflets to all visitors and provision and maintenance of permanent information boards within the site indicating nearby public rights of way and alternative visitor attractions not in the proximity of designated sites, as well as the details of nearby designated sites and recreational pressures upon them.

We advise that an appropriate planning condition or obligation is attached to any planning permission to secure these measures.

Natural England's further advice on designated sites/landscapes was also given with an acknowledgement that whilst the Appropriate Assessment was not produced by the LPA they

[NE] considered it was acceptable for the LPA to adopt it to fulfil our duty as competent authority.

PROTECTED LANDSCAPES: ...We advise that you consult the relevant AONB Partnership or Conservation Board. Their knowledge of the site and its wider landscape setting, together with the aims and objectives of the AONB's statutory management plan, will be a valuable contribution to the planning decision.

Norfolk Coast Partnership: NO OBJECTION The proposal covers two sites, Heacham Bottom and Mount Pleasant.

The following comments relate specifically to the Mount Pleasant application:

Further camping [to that proposed at the Heacham Bottom site is proposed] to the north in the Mount Pleasant area.

Although much of the development is technically outside of the AONB boundary, it is close enough to it to have a direct impact.

NPPF para 176 states that 'Great weight should be given to conserving and enhancing landscape and scenic beauty in National Parks, the Broads and Areas of Outstanding Natural Beauty which have the highest status of protection in relation to these issues. Development within their setting should be sensitively located and designed to avoid or minimise adverse impacts on the designated areas'.

The Landscape and Visual Appraisal has demonstrated that visual impact is relatively contained due to the site's topography, tree cover and hedgerows which all help to minimise impact.

There will be an increase in movement on site as well as associated works such as car parking, lighting, noise, and other structures associated with the development. This needs to be considered in the wider context.

The AONB has for a while seen a proliferation of small, isolated camping / holiday sites which cumulatively has a big impact on the designation and the social and economic impact does not outweigh the environmental impact to the landscape. Here the focus has been on making a contribution to the enhancement of the landscape and the wider aims of the estate in its sustainable farming practices and rewilding project helping to meet CS12 and para 176 of the NPPF. There is a net gain of 27.37% in terms of habitat creation on site as well as access improvements both on site and by way of creation of a new path that will link Snettisham and Heacham. 24 new jobs will be created, the complex will provide informal and formal recreational areas and there is an environmental educational element in conjunction with the wider work on the estate. The Norfolk Coast Partnership is supportive of development that serves to help boost the local economy and improve access and understanding to and of the AONB whilst protecting and enhancing the special qualities.

Care needs to be taken so as not to increase light pollution on site. which would impact the AONB dark skies, a special feature of the AONB. All external lighting on site should be conditioned.

NCC Protected Landscape Team (PLT): SUPPORT The Protected Landscapes Team at Norfolk County Council works to restore, enhance, protect and sustainably promote designated areas across Norfolk. We believe environmental protection and thriving rural communities and economies can exist side by side; and are supportive of sensitive and sustainable development which can bring net gain to each. We also work to create new

opportunities for people of all abilities and backgrounds to access Norfolk's unique nature and culture, and the associated health and wellbeing benefits.

Additionally, comments relating to a Limits of Acceptable Change (LAC) study were also submitted of which the PLT concludes that *Due to the numerous ways in which the proposals submitted by Wild Ken Hill are consistent with the LAC framework, I believe there is potential to showcase these proposals and the wider Wild Ken Hill project as an exemplar for the LAC, demonstrating to external stakeholders and other developers how it should be interpreted and implemented. I have discussed the above with Wild Ken Hill, who have indicated they would be keen to collaborate on this.*

Furthermore, PLT acknowledges that the proposals are aligned to several local sustainable tourism initiatives and concludes with support for the two planning applications submitted by Wild Ken Hill on the basis that they represent an exemplar for the new LAC framework, are a model for the future of sustainable tourism, and would enhance sustainable all abilities access to the area.

Historic Environment Service: NO OBJECTION Thank you for directly consulting Norfolk County Council Environment Service historic environment strategy and advice team regarding the above-mentioned application and apologies for the long delay in responding.

In broad terms we concur with some of the conclusions of the Heritage Statement and archaeological desk-based assessment. There is potential for previously unidentified heritage assets with archaeological interest (buried archaeological remains) to be present within the current application site and that their significance would be affected by the proposed development.

If planning permission is granted, we therefore ask that this be subject to a programme of archaeological mitigatory work in accordance with National Planning Policy Framework (2021) that should be secured by condition.

RSPB: NO OBJECTION The Royal Society for the Protection of Birds (the RSPB) is a registered charity that takes action for wild birds and the environment. We are the largest wildlife conservation organisation in Europe with a membership of over one million. The principal objective of the RSPB is to save nature.

Information in Support of Habitats Regulations Assessment and Shadow Habitats Regulations Assessment – RSPB comments

In our letter dated 26 October we suggested several lines of enquiry for further investigation. We are happy that these enquiries have been addressed and we have spoken to Wild Ken Hill about habitat enhancement for non-breeding birds such as curlew, a species which they support through habitat management of their own wetland and farmland, but also via the curlew head-starting programme and the range of valuable conservation activities associated with that project. We are also pleased to see further information about mitigation options and a commitment to making a contribution to the Green Infrastructure and Recreational Impact Avoidance and Mitigation Strategy (GIRAMS) in the Shadow HRA.

We understand it is likely that Wild Ken Hill will be required to create a Landscape Management Plan and the RSPB would be pleased to consult with them on any habitat creation and enhancement aspects, including any possible additional mitigation measures, as they finalise a detailed Landscape Masterplan.

As competent authority, it is the Borough Council's responsibility to assess the findings of the assessment and to make its own conclusions regarding the likelihood of significant effects

arising from the proposal, either alone or in-combination. With appropriate mitigation in place, the RSPB is satisfied that the proposals are not likely to result in significant adverse impacts on IIVSSs.

LLFA: NO OBJECTION I can confirm that the County Council as Lead Local Flood Authority (LLFA) has no comments to make.

Internal Drainage Board: NO OBJECTION Having screened the application, the site in question lies outside the Internal Drainage District of the King's Lynn Internal Drainage Board and as per our Planning and Byelaw Strategy the proposed application does not meet our threshold for commenting. Therefore, the Board has no comments to make.

Anglian Water: NO OBJECTION Having reviewed the development, there is no connection to the Anglian Water sewers, we therefore have no comments.

NCC Minerals: NO OBJECTION While the site is partially underlain by a safeguarded mineral resource (carstone), due to the nature of the proposed development it is considered the application would be exempt from the requirements of Policy CS16-safeguarding of the adopted Norfolk Minerals and Waste Core Strategy.

Environmental Health & Housing – Environmental Quality: NO OBJECTION We have no objection in terms of contaminated land subject to the following recommended conditions, but additional information is requested in terms of air quality.

Contaminated Land: In terms of contaminated land the site is partially on an investigated Part 2A site.

The application is for a change of use to provide caravan and camping pitches. Demolition of structures and construction of a utility building, creation of parking area and change of use to provide a reception, retail and storage area.

The applicant has provided a Phase 1 Geo-Environmental Assessment by pwa Geo-Environmental dated July 2022. The report identifies the risk from the former brickworks on site but assess this risk to be negligible. The site has also been subject to a Part 2A investigation by the council as part of its duties under the Environmental protection act 1990. This Investigation was undertaken using documents about the site and a site visit. The brickworks were operated in the late 19th Century. We found no evidence that the clay pits had been landfilled and no visible evidence was found of brickmaking waste in residential gardens. The site in its current use was found to be unlikely to pose a significant risk to human health, property, or controlled waters, and was determined as not contaminated land. This portion of site is not proposed to be developed in the drawing plan provided.

We have reviewed our files and the main area of the site to be developed around mount pleasant farm has been seen developed for the duration of our records. The surrounding fields to house the camping and caravan pitches are not seen developed excluding the brickworks previously discussed. The surrounding landscape is largely agricultural with some residential properties.

The information submitted does not indicate the presence of significant land contamination. However, the former agricultural use and the proximity to a former brickwork means that it's possible that some unexpected contamination could be present. Therefore, I recommend a condition relating to unexpected contamination be appended to any permission granted.

Additionally, due to the age of the property on site there is the potential for asbestos containing materials to be present. With this in mind we recommend a suitable informative be appended to any permission granted.

Air Quality: Further to the development proposals (22/01648/FM and 22/01650/FM) I have now had the opportunity to review the additional information submitted since our initial observations were made. An air quality assessment has been submitted by Dustscan (ref Rev A dated 21/12/22).

As explained the concern in terms of air quality from additional traffic arises when the changes in daily traffic movements (as 24-hr Average Annual Daily Traffic or AADT) are significant and in excess of IAQM EPUK (2017) indicative criteria. The transport assessment had suggested that changes in traffic would be significant during the summer months (March-Aug) and a business case based on car park capacity of 320 spaces with average dwell time based on 4-hrs turn-around.

In the absence of an Air Quality Management Area (AQMA) adjacent to the site, IAQM EPUK (2017) refer to significant traffic as around 500 light duty vehicles per day. The applicant has explained that additional traffic will be around an average of 408 AADT as a result of these developments. Based on the background air quality levels as quoted and absence of an AQMA in this area this is not sufficient to warrant a more detailed assessment for the changes to air pollution occurring in the area. I would therefore have no objection to this part.

We however mentioned that the principles of minimising emissions according to best practice apply to all developments, and especially, the larger major applications as in this case as set out by IAQM. We explained that whilst the development does not fall within a Smoke Control Area there still can be matters that are of material concern especially where they are not controlled elsewhere. Smoke emissions can be controlled via condition.

Finally, we did comment on electric vehicle (EV) charging infrastructure for the additional car-parking spaces including the 320-space car park with average dwell times quoted as around 4hrs. Norfolk CC parking guidelines refers to EV charging in such scenarios to be based on dwell time and travel distances i.e., it is for the applicant to define this. The information that has been provided within the air quality assessment only refers to the minimum will be provided. To ensure there is adequate EV charging and cable routes where necessary to meet the needs of all users and help future proof this development towards ultra-low emission vehicles further information is necessary. This can be suitably conditioned.

Arboricultural Officer: NO OBJECTION No trees are to be removed, and therefore I have no objection.

Fire Safety Carrow Fire Station, Norwich: NO OBJECTION Detailed correspondence submitted relating to Building Regulations.

Norfolk Constabulary: NO OBJECTION Detailed information sent to the applicant in relation to how to meet Secured by Design standards.

REPRESENTATIONS

Thirteen letters of **OBJECTION / CONCERN** have been received from third party representatives. A number of responses related to development proposed on the adjacent site, and are not specific to this application, although cumulative impacts are noted. The reasons for refusal, that are material to the consideration of this application, can be summarised as:

- Highway safety and congestion
- Noise and disturbance. The expansion of the Meadows Caravan Park has already resulted in an increase in people walking past properties, thus reducing privacy. This would be exacerbated by this development
- How will waste water be dealt with?
- Heacham Neighbourhood Plan expressly states that it does not support extensions or intensification of existing holiday sites
- Impact on protected landscapes and landscape character
- The access to Mount Pleasant Farm is at a hazardous section of Lamsey Lane
- Offering alternative holiday accommodation would take away from established related businesses in the village (e.g., hotels, B&Bs, other caravan sites, etc)
- Impacts on wildlife are at the expense of commercial gain
- There are already more caravans than residential properties in Heacham; we do not need more
- Extra pressure on infrastructure
- An alternative access should be proposed
- Following receipt of the traffic survey a further accident occurred at the Lamsey Lane junction on 23 December 2022

The following letter of **SUPPORT** has been received from the EXPERIENCE project: *Wild Ken Hill has actively engaged with our project and has been the first enterprise to submit experiences to the project.*

Wild Ken Hill works on developing a strong connection between people and nature, providing visitors with a unique experience while protecting the environment. Some activities are tailored to be best experienced during the low season, and the focus on cycling is of interest to the project. All this impacts positively the sustainability of tourism in Norfolk and echoes the founding principles of our project.

Through the EXPERIENCE project we look to support businesses develop off-season sustainable experiences. Our aim is to increase the number of visitors in Norfolk, and support a year-round visitor economy, providing lasting benefit for the local economy, our environment and the community.

We believe the application put together by Wild Ken Hill will go towards helping our project achieve those goals and help Norfolk be more sustainable.

We are committed to supporting any tourism business wishing to become more sustainable, and this letter confirms this.

Additionally, a further 7no. letters of **SUPPORT** have been received from third party representatives. A number of responses relate to development. The reasons for support, that are material to the consideration of this application, can be summarised as:

- The development is required to enable Wild Ken Hill to keep delivering its ambitious programme of work and will help to support Wild Ken Hill by providing income that can be used to restore the natural environment
- Job creation
- The camp site will be ideally suited for viewing birds and other wild creatures given its five-acre coverage and will allow the Wild Ken Hill experience to be offered to visitors coming from fu
- rther afield

- This development would have minimal impact on the A149 / Lamsey Lane junction given that it sees an average of over 23,000* vehicles use it each day (*BCKLWN Detailed Traffic Flow Data).

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS10 - The Economy

CS11 – Transport

CS12 - Environmental Assets

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

DM2 – Development Boundaries

DM11 – Touring and Permanent Holiday Sites

DM15 – Environment, Design and Amenity

DM17 - Parking Provision in New Development

DM19 - Green Infrastructure/Habitats Monitoring & Mitigation

NEIGHBOURHOOD PLAN POLICIES

Policy 5: Design Principles

Policy 9: Holiday Accommodation

Policy 11: Green Infrastructure

Policy 12: Provision of Electric Vehicle Charging

Policy 13: Dark Skies

Policy 15: Settlement Breaks

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

Environmental Impact Assessment
Principle of development
Form and character and impact on the countryside
Highway safety
Impact on neighbour amenity
Ecology / Biodiversity
Drainage
Crime and Disorder
Any other matters requiring consideration prior to determination of the application

Environmental Impact Assessment (EIA):

The development falls within Schedule 2 of the Town and Country Planning (Environmental Impact Assessment) Regulations 2017 (as amended) as consisting of tourism and leisure uses exceeding 1ha. Whilst the land take is large (c6.5ha), in this instance the majority of the area will remain devoid of any / extensive operational development and the amount of new / retained buildings is comparable to the existing. Therefore, the development was screened both in isolation and in-combination with the proposed development at Heacham Bottom and the Cheney Hill residential developments.

EIA thresholds suggest further consideration of this type of development is required when pitches reach 200. The 67 proposed under the current application are well below that figure. As such, in terms of EIA, these impacts (ecosystems) do not need further investigation via the EIA route and can be fully considered as part of the planning application.

The in-combination impacts were likewise ruled out as all four developments fall below threshold.

The development was therefore not considered to be EIA development for the purposes of the Regulations.

Principle of Development:

The application is for a holiday site on a predominately redundant farmstead most recently used as an equestrian centre comprising a range of redundant buildings, a rough surfaced yard area and pastoral fields.

Access is off Lamsey Lane via an existing gated agricultural access.

The proposed development site extends to a total area of 6.5 hectares and will comprise:

- A small reception / office / retail area hosted in a repurposed single storey stone (brick and clunch) building under a pantile roof
- A purpose-built facilities block, housing WCs, showers, accessible facilities, kitchen, washup area, utility area and drying area
- An area of hard standing pitches, accessed by a gravelled track with electric and water to each pitch
- An area of grass pitches to host tents and small motorhomes / camper vans with shared services including electric hook up and water taps
- An area allocated for the provision of seasonal glamping in bell tents

- An area of open land provided for visitors to commune and play, including some informal play structures made from natural materials.
- Existing residential property which will be let to provide staff accommodation under multiple occupancy (maximum 4 people)
- Existing tracks and informal paths.

The application falls under the 'banner' of both farm diversification and rural tourism. Both of which are supported at national and local level. Other relevant policy and guidance primarily revolves around protecting the natural environment (AONB and countryside), although other issues such as highway safety and residential amenity are obviously key material considerations too.

The NPPF covers the rural economy at paragraphs 84 and 85:

84. Planning policies and decisions should enable:

- a) the sustainable growth and expansion of all types of business in rural areas, both through conversion of existing buildings and well-designed new buildings*
- b) the development and diversification of agricultural and other land-based rural businesses*
- c) sustainable rural tourism and leisure developments which respect the character of the countryside and*
- d) the retention and development of accessible local services and community facilities, such as local shops, meeting places, sports venues, open space, cultural buildings, public houses and places of worship.*

85. Planning policies and decisions should recognise that sites to meet local business and community needs in rural areas may have to be found adjacent to or beyond existing settlements, and in locations that are not well served by public transport. In these circumstances it will be important to ensure that development is sensitive to its surroundings, does not have an unacceptable impact on local roads and exploits any opportunities to make a location more sustainable (for example by improving the scope for access on foot, by cycling or by public transport). The use of previously developed land, and sites that are physically well-related to existing settlements, should be encouraged where suitable opportunities exist.

Core Strategy Policy CS06 states: *The strategy will be supportive of farm diversification schemes and conversion of existing buildings for business purposes in accordance with Policy CS10 providing any proposal:*

- *meets sustainable development objectives and helps to sustain the agricultural enterprise*
- *is consistent in its scale with its rural location*
- *is beneficial to local economic and social needs*
- *does not adversely affect the building and the surrounding area or detract from residential amenity.*

Core Strategy Policy CS10, where it relates to tourism, states: *The Council will promote opportunities to improve and enhance the visitor economy:*

- *Supporting tourism opportunities throughout the borough*
- *Promoting the expansion of the tourism (including leisure and culture) offer in Hunstanton to create a year-round economy*
- *Smaller scale tourism opportunities will also be supported in rural areas to sustain the local economy, providing these are in sustainable locations and are not detrimental to our valuable natural environment.*

The Council will permit the development of new tourism accommodation in rural areas subject to the following criteria being met:

- *It should be located in or adjacent to our villages and towns*
- *It should be of a high standard of design in line with national guidance*
- *Will not be detrimental to the landscape*
- *Mechanisms will be in place to permanently retain the tourism related use.*

As stated above, the application falls under the 'banner' of both farm diversification and rural tourism. Given that agricultural enterprises are more often than not, not adjacent to the towns and villages there is some conflict between these two policies. However, taking a pragmatic approach as to how these two policies interrelate, it is considered that the development accords with Policy CS10 in so far as tourism as it relates to farm diversification is concerned.

Development Management Policy DM2 allows development within the countryside where it complies with Core Strategy Policies CS06 and CS10.

Development Management Policy DM11 states: *Proposals for new holiday accommodation sites or units or extension or intensification to existing holiday accommodation will not normally be permitted unless:*

- *The proposal is supported by a business plan demonstrating how the site will be managed and how it will support tourism or tourist related uses in the area*
- *The proposal demonstrates a high standard of design in terms of layout, screening and landscaping ensuring minimal adverse impact on visual amenity and the historical and natural environmental qualities of the surrounding landscape and surroundings*
- *The site can be safely accessed*
- *It is in accordance with national policies on flood risk*
- *The site is not within the Coastal Hazard Zone indicated on the Policies Map, or within areas identified as tidal defence breach Hazard Zone in the Borough Council's Strategic Flood Risk Assessment and the Environment Agency's mapping.*

Small scale proposals for holiday accommodation will not normally be permitted within the Norfolk Coast Area of Outstanding Natural Beauty (AONB) unless it can be demonstrated that the proposal will not negatively impact on the landscape setting and scenic beauty of the AONB or on the landscape setting of the AONB if outside the designated area. Proposals for uses adversely affecting Sites of Special Scientific Interest (SSSIs) or European Sites will be refused permission.

Policy DM11 also requires certain restrictive conditions relating to holiday use.

In relation to the points raised above the LPA responds as follows:

1. A suitable business plan accompanied the application
2. The re-use of some buildings and design of new buildings (to reflect the agricultural nature of the locality) along with landscaping that will be conditioned if permission is granted suggests that the development would not have an unacceptable impact on the locality
3. The local highway authority raises no objection in relation to safe access or highway safety in general subject to conditions
- 4 and 5. The site does not lie in an area at risk of flooding or within the Coastal Hazard Zone.

The Business Plan that accompanied the application largely relates to the Heacham Bottom Proposal. However, the addendum received via email covering check-in / check-out times, what is prohibited to bring, curfews and how breaches would be managed and safety measures in terms of fire / medical emergencies etc. is as relevant to this application as the Heacham Bottom one. This would be suitably conditioned if permission were granted.

It is therefore considered that the proposed development accords with Policy DM11 of the SADMP.

Heacham Neighbourhood Plan (HNP) is adopted and therefore forms part of the Development Plan and must be given significant weight in consideration of the application.

Heacham Neighbourhood Plan

Heacham Neighbourhood Plan Policies, relevant to this application, are: Policies: 5 (Design Principles), 9 (Holiday Accommodation), 11 (Green Infrastructure), 13 (Dark Skies) 14 (Community Facilities) and 17 (Settlement Breaks).

Taking each in turn, where relevant to this application:

Policy 5: Design Principles

Development proposals should deliver high quality design. As appropriate to their scale, nature and location development proposals should:

1. Preserves or enhances the village of Heacham, be sensitive to its surroundings, and demonstrate that it minimises adverse impacts on neighbouring residences – considered acceptable and covered later in this report

2. Recognise and reinforce the character of the local area in relation to height, scale, spacing, layout, orientation, design, and materials of neighbouring buildings - The replacement building is considered to be of an appropriate height, scale, mass etc. Materials are considered acceptable and would be suitably conditioned if permission is granted.

5. Incorporate measures which increase energy efficiency and which reduce energy and resource loss, e.g. installation of solar panels, use of grey water, use of alternatives to plastic - solar panels are proposed on the roof of the utility building and an air source heat pump will augment the hot water heating needs of the utility building

6. Provide sufficient external space for:

- refuse and recycling storage
- bicycle parking
- child and disabled facilities where appropriate
- the integration of meter boxes, lighting, flues and ventilation ducts, gutters and pipes, satellite dishes, aerials and telephone lines - Appropriate for the type of development sought

10. There is no unacceptable adverse impact (visual or otherwise) on the area's landscape, and proposals for development will be expected to demonstrate how they have minimised landscape impacts on the open countryside and coastline – considered acceptable and covered later in this report

11. Incorporate adequate landscaping to mitigate the visual impact of the development and to ensure that proposals are in keeping with the existing village context. Where possible, sites are screened through the use of landform, native trees and locally appropriate planting - Would be suitably conditioned if permission is granted

14. Where practicable, make better connections to other areas of the parish, including access to local services and public open spaces – considered acceptable and covered later in this report

15. Retain mature or important trees (NPPF 2019 Section 175 applies) – No trees are to be removed

16. Ensure new boundary treatments reflect the distinct local character and incorporate semi-mature street planting and hedges to boundaries with open countryside – would be suitably conditioned if permission is granted

17. Access to the site is provided/improved to highway authority standards – considered acceptable and covered later in this report

18. Where appropriate, proposals make a positive contribution towards open spaces, whether respecting the amenity, recreational and wider environmental value of existing

spaces or, especially for developments of more than 8 dwellings, provide additional public open space to meet the needs of new residents – considered acceptable and covered later in this report.

Notwithstanding issues that are covered later in this report, it is considered that the proposed development is in general compliance with Policy 5 of the HNP.

Policy 9: Holiday Accommodation

In order to maintain and improve Heacham's attraction as a quiet uncommercialised holiday centre, applications for further holiday accommodation beyond existing defined holiday areas, will only be supported where the proposals:

1 Maintain the distinction between the contrasting holiday centres of Heacham and Hunstanton and do not diminish the physical separation between these centres - The site does not diminish the physical separation between these centres

2 Do not have any unacceptable impact on local infrastructure, including green infrastructure - Heacham is a Key Rural Service Centre with many services and facilities. It is not considered that the scale of the proposed development would have an unacceptable impact on local infrastructure.

3 Minimise any visual and physical impact on the village by including, where appropriate, a landscaping plan incorporating the use of landform, native trees and locally appropriate planting - A detailed landscaping plan would be suitably conditioned if permission is granted

4 Are not directly adjacent to any residential areas – considered acceptable and covered later in this report

5 Do not need to be accessed through the village centre of Heacham - The site does not have to be accessed through the village centre

6 Incorporates high quality accommodation for which adequate parking and servicing arrangements are provided - The proposed units are a scale, mass, design and utilise appropriate materials, and appropriate parking is proposed

7 Can demonstrate a link to wider tourism or land use initiatives that provide demonstrable benefits to the local area – This application forms a key part of the overall proposals. However, Members will need to consider if in isolation (i.e., if the previous application at Heacham Bottom Farm was refused), this development provides a demonstrable benefit to the local area.

In relation to the latter point, it is suggested this development would still provide 7.75 full-time equivalent (FTE) jobs and, in combination with the Mount Pleasant application could generate an additional £4.3 million of visitor spending per year into the local economy (based on Visit Britain figures for average spend per overnight stay of £67 and a day trip of £40(2019 figures) and visitor number projections of 80,000 per annum for Heacham Bottom and 16,500 per annum overnight accommodation.)

Staff	FTE
General Manager	0.25
Accommodation Lead	0.75
F&B Lead	0.25
Admin / bookkeeping	0.5
Sales & Marketing	0.5
Cleaning & Maintenance	1.5
Operational	2
Seasonal	2

Policy 11: Green Infrastructure:

As appropriate to their scale, nature and location, development proposals should protect and where practicable enhance existing green infrastructure and where practicable provide new

green infrastructure facilities. In particular, support will be given to proposals that further enhance:

1. *The quality, accessibility and usage of public open spaces, allotment provision and areas of sport provision*
2. *Existing public rights of way within the parish, and to seek opportunities to create new public rights of way to create linkages to the beaches, and into the wider countryside locally*
3. *The preservation and enhancement of Area of Natural Beauty and local habitats*
4. *Increasing the number of trees in the village and enriching green areas with wildflower planting*
5. *Maintain existing grass verges where possible, e.g., where there is a footpath on the opposite side of the road...*

The development is therefore considered to accord with the overarching aims of this policy where relevant.

Policy 13: Dark Skies

External lighting associated with development proposals should be sensitively designed to safeguard the dark skies environment of the neighbourhood area and minimise the extent of any light pollution...

Lighting would be suitably conditioned if permission is granted to ensure compliance with Policy 13 of the HNP.

The development is therefore considered to accord with the overarching aims of this policy.

Policy 17: Settlement Breaks

Development proposals outside the development boundaries of Heacham (and as shown in Inset G47 of the SADMP) will only be supported where they:

- *do not cause unacceptable harm to the landscape setting and distinct identity of Heacham*
- *do not detract from the visual separation of Heacham from Hunstanton*
- *do not detract from the views or settings of the Norfolk Coast AONB*
- *New development must not result in the coalescence of Heacham with Hunstanton to the north.*

In relation to the policy criteria above the LPA comments as follows:

- Impact on the landscape (and AONB in particular) is covered in more detail later in this report
- The development would not detract from the visual separation between the settlements.

No part of the site falls within the Snettisham Neighbourhood Plan Area, and therefore, whilst impacts can be given due consideration, the NP Policies cannot form the basis of approval or refusal of this application.

Summary – Principle of Development:

In summary it is considered that the principle of development is to be supported and that the development accords with the overarching national and local policy criteria outlined above.

Form and Character and Impact on the Setting of the AONB:

The Design and Access Statement (DAS) that accompanied the application suggests that careful consideration was given to which buildings should be demolished as well as the siting

and orientation of the new utility building which measures 4.8m to ridge, 2.3m to eaves and is 27.6m long and 9.1m wide and is to be constructed from vertical profile cladding under a profiled metal roof.

Details of the buildings being demolished are as follows:

- The existing barn measures 5.1m to ridge, 3.7m to eaves and is 13.6m long x 9.1m wide and is constructed from steel portal frame with concrete block and vertical timber cladding, profiled metal roof.
- The adjoining long shed (to the rear of the cluster) measures 4m to ridge, 3.3m to eaves and is 20.6m long x 7.3m wide and is constructed from steel portal frame with vertical & horizontal timber cladding, profiled metal roof.
- The adjoining stable block (to the front of the cluster) measures 3.3m to ridge, 2.7m to eaves and is 9.7m long x 4m wide and is constructed from timber, with horizontal timber cladding and roofing felt.
- The stable block (freestanding to the front of the cluster) measures 3.2m to ridge, 2.6m to eaves and is 7.3m long x 4.3m wide and is constructed from timber, with horizontal timber cladding and roofing felt.

Your officers agree that the buildings to be demolished are of no particular merit being constructed generally from block and timber under felt or metal profile roof coverings.

The building being retained is an attractive traditional brick building under a pantile roof. Existing openings are to be retained, retaining the character of the building. This building will be repurposed into an office / retail space and reception area.

Additionally, the pitch layouts and associated landscaping are stated to be informed by the landform to minimise any visual impact, with the hard standing pitches being positioned in the area of the site least visible from the road and other viewpoints, and the grass pitch area being well screened by an existing line of mature trees at the highest point of the site.

Existing trees and shrubs are to be retained with further planting to create screening and small 'glades' to enhance the camping experience as well as increase biodiversity.

There are relatively extensive areas of landscaping (relative to the size of the camping area), principally soft but some hard, including the car park, courtyard area, internal tracks and footpaths and a recreation and play area that require further detailed consideration. This would be suitably conditioned if permission were granted.

The indicative biodiversity enhancement plan shows:

- Hedge planting (both infilling / reinforcing of existing hedgerows and new hedge planting)
- Creation of a new scrubby woodland edge habitat
- Enhance existing meadow by sowing additional species-rich seed mix
- Standard tree planting to provide shade to glamping pitches

Other details that will need to be conditioned are lighting and internal signage.

In relation to lighting, a lighting statement accompanied the application confirming that lighting will be kept to a minimum with low-level bollards with downcast lighting being placed, as appropriate, to mark the primary routes. In the vicinity of the utility building, small bulkhead fittings will be placed under the projecting canopy to highlight the entrances to the showers, WCs and kitchen. The DAS also suggests that the courtyard will act to naturally limit any external light leakage from this area into the wider landscape. All lighting will be turned off post curfew except for the utility building bulkhead fittings.

Additionally, all light sources shall be shielded from direct external view or shall exhibit a maximum source intensity below 2,500cd (as suggested by the Institute of Lighting Professional Guidance Note GN01.)

No details of the bell tents have been provided. However, this would be suitably conditioned if permission were granted.

The site does not fall within the AONB, however paragraph 176 of the NPPF states that *development within their setting should be sensitively located and designed to avoid or minimise adverse impacts on the designated areas*. Additionally, impact on the intrinsic character and beauty of the countryside is also a key consideration.

Protection of the countryside and AONB's is reiterated in Heacham Neighbourhood Plan.

As previously stated, it is suggested that pitch location will lessen visual impacts by using the topography of the site and existing mature screening.

The new building is of a scale, mass, design and appearance that is reflective of the site's agricultural setting.

It is therefore considered that the built form associated with the proposed development is acceptable. However, the use of the site will have an impact on the locality, principally by activity associated with the use. There will be greater vehicular activity, general activity and light pollution, although it is noted that the latter would be limited and suitably conditioned if permission was granted.

In relation to the former issue, activity, Members will need to consider whether this would result in an acceptable impact on the setting of the AONB and if the benefits to the existing enterprise and wider economy outweigh the harm these activities may have.

A detailed Landscape and Visual Appraisal (LVA) accompanied the application. The LVA assessed a total of 13 viewpoints comprising transport, recreational, residential and PROW receptors.

The LVA concluded, in the overall summary, that *It is considered that as the proposed development has followed a landscape led approach which adopts rewilding, natural regeneration and traditional conservation approaches in order to successfully integrate new nature-based tourism within the wider agricultural setting. This will facilitate the sensitive integration of the new facilities to support its intended function and has demonstrated a successful approach to accommodating the level of change within the landscape without establishing important levels of harm to the landscape characteristics and those elements which define the site.*

It is considered that the proposed development can be accommodated to form an acceptable day visitor and tourist attraction together with the wider beneficial approach to integrating rewilding characteristics at the heart of the proposals.

The potential for adverse landscape and visual effects which would be considered at most, moderately harmful at day one, can be appropriately mitigated through the implementation of the site-specific landscape proposals.

It is therefore considered that the level of potential adverse landscape and visual effects associated with the proposed development has been reduced to an acceptable low level.

In this regard neither your officers nor NCC Protected Landscape Team consider the impacts to the setting of the AONB or wider countryside are unacceptable.

A key consideration in the determination of this application is the seasonal nature of the camping element of the proposal, which means that the site would be devoid of tents / bell tents and the activity associated with them for four months of the year (November - February.) The touring pitches however are not seasonal and are proposed year-round.

Your officers believe that the development would conserve the setting of the AONB and the wider rural landscape, and that any harm is outweighed by the benefits associated with the proposed development.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to impacts on protected landscapes and visual impact in general.

Highway Safety:

The site will operate on a 'park at pitch' basis, limited to one car per pitch, with additional parking provided adjacent to the reception area (20 spaces) and at the larger car park at the Heacham Bottom site if necessary.

A new permissive footpath is proposed to the northwest of the site where it will be required to connect to the existing highway footway. This would be secured by condition if permission were granted.

Likewise, a new permissive footpath is proposed leading to the south of the site opposite the access with the Heacham Bottom Site.

In relation to transport paragraphs 85, 104, 105, 110 - 113 of the NPPF, whilst acknowledging that sites to meet local business needs, including tourism, may be beyond existing settlements and not be well served by public transport, require developments to be safe, offer alternative modes of transport, enable charging of plug-in and other ultra-low emission vehicles, and concludes that *Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe.*

These requirements are reiterated in Development Plan Policies CS11 and DM12 and Heacham Neighbourhood Plan Policies 5, 12 and 18.

Highway safety, impacts on the road network, and congestion are some of the most contentious issues associated with this development.

Notwithstanding this, the scale of this development does not require a Transport Assessment (TA), and therefore analysis of the traffic associated with this development has not been assessed in the same detail as the Heacham Bottom site. In this regard the TA states that *the Mount Pleasant site will generate a small number of arrivals and departures spread throughout the day. As such it will be negligible in any given hour and likely to exclude travel within highway peak periods. This has therefore been excluded from analysis in regard to peak hour traffic capacity. However, it is borne in mind when assessing the result of the capacity analyses.*

Likewise, the Local Highway Authority would have considered the cumulative impacts of the two developments on the road network including the Lamsey Lane / Lynn Road (A149) junction.

Notwithstanding this, some of the assessments undertaken are relevant to this application e.g., existing highway characteristics and speed analysis to ensure appropriate visibility splays.

Whilst excluding the Mount Pleasant site from peak hour traffic capacity analysis, traffic flow characteristics adjacent to the Mount Pleasant site were undertaken using the same Manual Classified (traffic) Count (MCC) and Automatic Traffic Count (ATC) surveys as that of Heacham Bottom *including the further ATC that took place between 24 April 2023 and 3 May 2023.*

The ATC concluded that school term-time (last week of school (Thursday 15th to Wednesday 21st July)), two-way, weekday movements adjacent to the Mount Pleasant access were 4,046 with Saturdays being 4,807; and 3,991 and 3,561 during the school holidays (first week of summer holidays (Thursday 22nd to Wednesday 28th July.)) In all instances, the vast majority of movements were from light vehicles (e.g., cars). These are unsurprisingly almost identical to the movements adjacent to the Heacham Bottom site.

The TA concluded that current visibility from the Mount Pleasant access with Lamsey Lane, with appropriate hedgerow management, is: 2.4m x 131m to the left and 2.4m x 141m to the right. The TA concludes, when applying the 85th percentile speeds recorded, that the requirements are 2.4m x 129m to the left and 2.4m x 127m to the right. These can clearly be achieved.

In summary, the TA concludes that the development at Mount Pleasant would not have a material impact on the detailed assessment undertaken in relation to the Heacham Bottom proposal, and that the development:

- Would not cause issues on the local highway network, given the spare capacity identified at the Lamsey Lane / Lynn Road (A149) junction, including during high tourist season
- Can achieve safe visibility requirements at the proposed access
- Is located on highway with a good safety record
- Is sustainably located for its rural location with the potential to increase sustainability
- Is compliant with local and national policy
- There is no defensible reason for refusal on the grounds of traffic, transport or highway safety.

The Local Highway Authority has found the findings of the TA to be an acceptable basis on which to make their recommendation [no objection] subject to conditions relating to access, visibility splays, gradient, parking provision, cycle provision, off-site highways works, closure of other access(s) and access obstruction being appended to any permission granted.

In late correspondence at Planning Committee of 3rd April, it was reported that the applicant confirmed that they would offer land around the Lamsey Lane junction, with a condition that highway works commence within 15 years, to help enable junction improvements. This has now been confirmed within an updated Highways Position Statement (V4), and the recommendation on this application has been amended to secure this.

Whilst the ATC findings were not a formal reason for deferral, the applicants undertook a further ATC between 24th April 2023 to 3rd May 2023. This is attached as Appendix 2.

The Local Highway Authority (LHA) has confirmed that the originally submitted ATC was robust and its findings, including projections for future traffic generation, were

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valid with any differences between the projections and the 2023 ATC not considered to be significant and to be expected as daily variances. The LHA then state that, as with the initial analysis, variance factors and seasonal uplifts have been applied to the 2023 data, and the associated junction traffic modelling carried out which identified no material change. The LHA concludes that their professional opinion remains that there will be no material impact at the junction as a result of the proposals. The LHA's comments can be read in full in the 'Representations' section of this report.

The Local Highway Authority has confirmed that they agree that the findings of the latest ATC support the findings of the original ATC with any differences being insignificant.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to highway impacts.

Impact on Neighbour Amenity:

The NPPF requires development to have a high standard of amenity for existing and future users (para 130f.) This is reiterated in Development Plan Policies CS06 and DM15 and Heacham Neighbourhood Plan Policy 5.

The nearest development to the site is another holiday site to the north, Meadows Caravan Park. It is therefore considered that the proposed development is compatible with its nearest non-associated neighbour. Furthermore, operation of the site, in line with the addendum received in relation to the Business Plan that accompanied the application, would be conditioned if permission were granted.

Additionally, if a statutory nuisance did occur then the Local Authority has powers to intervene.

4 Collingwood Close is the closest non-associated residential dwelling and occupiers have objected in relation to privacy as a result of increased pedestrians walking past their property that has already substantially increased following the expansion of Meadows Caravan Park.

Whilst it is acknowledged that footfall may increase, this is not something that could be controlled. Notwithstanding this, and contrary to the third-party representative, the LPA does not consider that the scale of the proposed development would result in material impacts.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to impacts on neighbour amenity.

Ecology / Biodiversity:

The NPPF, at Chapter 15, requires planning policies and decisions to protect and enhance biodiversity. This is reiterated in Development Plan Policies CS01, CS12 and DM19 and Hunstanton Neighbourhood Plan Policy 11.

The application was accompanied by a detailed Shadow Appropriate Assessment, Biodiversity Net Gain Assessment, Preliminary Ecological Appraisal and Bat Risk Assessment.

Natural England confirmed that it would be appropriate for the LPA to adopt the Shadow Appropriate Assessment as its Appropriate Assessment (as the competent authority.)

Appropriate Assessment is the method of assessing whether a development would have a likely significant effect on protected sites. In this instance the sites are:

- The Wash and North Norfolk Special Area of Conservation (SAC)
- The Wash Special Protection Area (SPA)
- The Wash Ramsar
- North Norfolk Coast SPA
- North Norfolk Coast Ramsar.

The Appropriate Assessment concludes that without mitigation the development could have an adverse effect on the integrity of the above protected site. The following mitigation was therefore proposed, and accepted as appropriate by Natural England:

- A financial contribution to be paid into the Norfolk Green Infrastructure and Recreational Avoidance Mitigation Strategy (GIRAMS)
- Provision of leaflets to all visitors and provision and maintenance of permanent information boards within the proposal site and with the Coastal Park indicating nearby public rights of way and alternative visitor attractions not in the proximity of designated site, as well as the details of nearby designated site and recreational pressures upon them.

The GIRAMS payment has been paid (£929.65), but provision of information boards and leaflets would be suitably conditioned if permission is granted.

A Preliminary Ecological Appraisal / Bat Risk Assessment accompanied the application.

The Appraisal / Assessment is concerned with site specific ecological issues and considered badgers, bats, great crested newts, birds, reptiles and invasive species.

The Appraisal concluded that no further studies are required, and other than precautionary approaches and best practise (such as vegetation removal outside of the bird breeding season) no further mitigation is required. The following enhancements are proposed:

- Incorporation of bird and bat boxes across the site providing extra potential roosting / nesting resource thus improving biodiversity
- Replanting of a range of ruderal type plants and scrub that will attract pollinators
- Reinstating hedgerows with native species
- New hedgerow to be native

The best practice mitigation and enhancements would be suitably conditioned if permission is granted.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to ecology and biodiversity.

Drainage: Foul drainage will utilise a new package treatment plant with surface water drainage being distributed to soakaways in appropriate locations.

Full details will be sought by condition if permission is granted.

Crime and Disorder: There are no specific concerns relating to Crime and Disorder, and the Police Architectural Officer has supplied the applicant with advice in regard to Designing out Crime.

Other matters requiring consideration prior to the determination of this application:

Trees: No trees are to be removed to enable the proposed development.

Retail: The modest retail element proposed is to serve the campsite and would offer a small selection of 'camping essentials' such as firewood, firelighters, matches, confectionary, snacks, beverages, toilet roll and a small range of non-perishable food items and toiletries. This would be suitably conditioned if permission were granted.

Sustainable Design: Paragraphs 129 and 154 of the NPPF relate to the sustainability of buildings and Development Plan Policy CS08 is concerned, in part, with sustainable design and energy efficiency.

The following design principles have been considered during the evolution of the proposal:

- Re-use of the most suitable existing building on the site from both an operational and structural perspective which minimises embodied carbon related to demolition and construction activities
- Orientation of the new building to make the most of passive solar gain
- High levels of insulation, airtightness and heat-recovery based ventilation on all buildings both new and converted
- Main energy supply being via renewable energy in the form of solar panels and air source heat pumps
- Materials are durable using recycled elements whilst still ensuring their appearance is appropriate to the local agricultural vernacular.

It is therefore considered that the development accords with the relevant national and local planning policy outlined above in relation to sustainable design.

Flooding: The site does not lie in an area at risk of flooding.

Environmental Quality: The requested conditions relating to contamination, smoke emissions and EV charging will be suitably conditioned if permission is granted.

Historic Environment Service: The requested archaeological conditions will be appended to any permission granted.

CIL: The development is not CIL liable.

Specific comments and issues:

In relation to objections received from the Parish Councils and third-party representatives, your officers respond as follows:

- Highway safety and congestion – covered in report
- Following receipt of the traffic survey a further accident occurred at the Lamsey Lane junction on 23 December 2022 – the LHA are aware of the accident; it does not change their opinion as to the acceptability, on the grounds of highway safety, of the proposed development
- Pedestrian safety if some parking is provided at the Heacham Bottom site – if both applications are approved a pedestrian crossing will have to be provided
- Noise and disturbance – covered in report
- How will drainage be dealt with? – covered in report
- Heacham Neighbourhood Plan expressly states that it does not support extensions or intensification of existing holiday sites – this is not a correct interpretation of the policy; the policy allows some development that is policy compliant. This is covered in the report
- Impact on protected landscapes and landscape character – covered in report

- The access to Mount Pleasant Farm is at a hazardous section of Lamsey Lane – the TA has shown that the site access can provide adequate visibility splays which has been confirmed by the LHA. This is covered in the report
- Offering alternative holiday accommodation would take away from established related businesses in the village (e.g., hotels, B&Bs, other caravan sites, etc.) – it is not the role of the planning system to prevent competition
- Impact on wildlife - covered in report
- An alternative access should be proposed – developments must be assessed as submitted
- Extra pressure on infrastructure – Heacham is a Key Rural Service Centre with the infrastructure and facilities considered appropriate for this scale of development.

CONCLUSION / PLANNING BALANCE:

Applications for planning permission should be determined in accordance with the Development Plan and any other material considerations. The application falls under the 'banner' of both farm diversification and rural tourism, both of which are supported at national and local level, subject to other relevant planning policy and guidance and for the reasons outlined in the report above, it is considered that the development is in general accordance with these enabling policies.

The site lies outside of the AONB but within its setting. However, an LVA has satisfied the NCC Landscape Protection Team and Norfolk Coast Partnership that the development would not adversely affect the setting of this designated landscape or the wider countryside in general.

It is considered that the scale, mass, design and appearance of the proposed replacement building is acceptable in its rural setting. However, it is acknowledged that there would be some impact from the proposed use itself. Notwithstanding this, it is considered that, given the scale proposed, this impact would not be significant and would not result in material harm. Furthermore, the proposal comes with benefits both to the existing agricultural enterprise and to the tourism offer as well as enabling development of the parallel application at Heacham Bottom. These issues are given weight in this decision.

The proposed development, in isolation, would not result in any material highway safety issues. However, in combination with application 22/01648/FM, there would be a material increase in vehicular activity. Notwithstanding this, the Local Highway Authority has confirmed this increase in vehicular activity would not significantly affect highway capacity or highway safety. Additionally, the offer of additional land to help facilitate improvements to the Lamsey Lane / Lynn Road Junction, whilst not required by the current applications, is to be welcomed and will be secured by via S106 Agreement.

It is therefore recommended that this application be approved subject to the following conditions.

RECOMMENDATION:

- A) APPROVE subject to signing of a S.106 legal agreement to secure safeguarded land around the Lamsey Lane junction for potential future highway improvements for a period of 15 years from the date of decision, and the imposition of the conditions set out in the committee report.**

1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.

2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans:

Demolition Plan As_Proposed Drawing Package Rev.B
Site Plan_As Proposed Drawing Package Rev.B
Wider Site Plan_As Proposed Drawing Package Rev.B
Reception Building Floor Plan_As Proposed Drawing Package Rev.B
Reception Building Elevations_As Proposed Drawing Package Rev.B
Utility Building Floor Plan_As Proposed Drawing Package Rev.B
Utility Building Elevations_As Proposed (south and west) Drawing Package Rev.B
Utility Building Elevations_As Proposed (north and east) Drawing Package Rev.B
Utility Building Roof Plan_As Proposed Drawing Package Rev.B
Proposed Landscape Masterplan (1 of 2) WKH-DIG-00-PL-0001 (1 of 2) Rev.P10
Proposed Mount Pleasant Masterplan WKH-DIG-00-PL-0002 Rev.P05
Biodiversity Enhancement Plan WKH-DIG-00-PL-0003 Rev.P03

2 Reason: For the avoidance of doubt and in the interests of proper planning.

3 Condition: No development shall commence until full details of the foul and surface water drainage arrangements for the site have been submitted to and approved in writing by the Local Planning Authority. The drainage details shall be constructed as approved before any part of the development hereby permitted is brought into use.

3 Reason: To ensure that there is a satisfactory means of drainage in accordance with the NPPF. This needs to be a pre-commencement condition as drainage is a fundamental issue that needs to be planned for and agreed at the start of the development.

4 Condition: No demolition/development shall take place until an archaeological written scheme of investigation (WSI) has been submitted to and approved by the local planning authority in writing. The scheme shall include an assessment of significance and research questions; and

- 1) The programme and methodology of site investigation and recording
- 2) The programme for post investigation assessment
- 3) Provision to be made for analysis of the site investigation and recording
- 4) Provision to be made for publication and dissemination of the analysis and records of the site investigation
- 5) Provision to be made for archive deposition of the analysis and records of the site investigation and
- 6) Nomination of a competent person or persons/organization to undertake the works set out within the written scheme of investigation.

4 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF. This needs to be a pre-commencement condition given the potential impact upon archaeological assets during groundworks/construction.

- 5 Condition: No demolition/development shall take place other than in accordance with the written scheme of investigation approved under condition 4 and any addenda to that WSI covering subsequent phases of mitigation.
- 5 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.
- 6 Condition: The development shall not be occupied or put into first use until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the archaeological written scheme of investigation approved under condition 4 and the provision to be made for analysis, publication and dissemination of results and archive deposition has been secured.
- 6 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.
- 7 Condition: Notwithstanding the details indicated on the submitted drawings no works above slab level shall commence on site unless otherwise agreed in writing until detailed drawings for the scheme of off-site highway improvement works have been submitted to and approved in writing by the Local Planning Authority.

The scheme of works shall include:

- Provision of a new pedestrian footway connection between the Mount Pleasant site (on site path) and Heacham (existing highway footway)
- Suitable pedestrian crossing of Lamsey Lane between Heacham Bottom Farm & Mount Pleasant farm.

- 7 Reason: To ensure that the highway improvement works are designed to an appropriate standard in the interest of highway safety and to protect the environment of the local highway corridor.
- 8 Condition: Prior to the first use of the development hereby permitted the off-site highway improvement works (including Public Rights of Way works) referred to in condition 7 shall be completed to the written satisfaction of the Local Planning Authority.
- 8 Reason: To ensure that the highway network is adequate to cater for the development proposed.
- 9 Condition: Prior to the first use of the development hereby permitted the vehicular access onto Lamsey Lane shall be constructed (for the first 10 metres) in accordance with a detailed scheme to be agreed in writing with the Local Planning Authority in accordance with the highways specification and thereafter retained at the position shown on the approved plan (2021-F-015-005 REV A). Arrangement shall be made for surface water drainage to be intercepted and disposed of separately so that it does not discharge from or onto the highway.
- 9 Reason: To ensure construction of a satisfactory access and to avoid carriage of extraneous material or surface water from or onto the highway in the interests of highway safety.
- 10 Condition: Any access gates, bollard, chain or other means of obstruction, shall be hung to open inwards, set back, and thereafter retained a minimum distance of 10 metres from the near channel edge of the adjacent carriageway. Any sidewall, fences or hedges

adjacent to the access shall be splayed at an angle of 45 degrees from each of the outside gateposts to the front boundary of the site.

- 10 Reason: In the interests of highway safety enabling vehicles to safely draw off the highway before the gates / obstruction is opened.
- 11 Condition: The gradient of the vehicular access shall not exceed 1:12 for the first 10 metres into the site as measured from the near channel edge of the adjacent carriageway.
- 11 Reason: In the interests of the safety of persons using the access and users of the highway.
- 12 Condition: Prior to the first use of the development hereby permitted visibility splays measuring 120 metres x 2.4 metres shall be provided to each side of the access where it meets the near edge of the adjacent highway carriageway. The splay(s) shall thereafter be maintained at all times free from any obstruction exceeding 0.6 metres above the level of the adjacent highway carriageway.
- 12 Reason: In the interests of highway safety in accordance with the principles of the NPPF.
- 13 Condition: Prior to the first use of the development hereby permitted the proposed on-site access, car parking, on-site pedestrian paths, servicing, loading, unloading, turning and waiting area(s) shall be laid out, demarcated, levelled, surfaced and drained in accordance with the approved masterplan and retained thereafter available for that specific use.
- 13 Reason: To ensure the permanent availability of the parking/manoeuvring areas, in the interests of satisfactory development and highway safety.
- 14 Condition: Prior to first use of the development hereby permitted a scheme for the parking of cycles shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall be fully implemented before the development is first occupied or brought into use and thereafter retained for this purpose.
- 14 Reason: To ensure the provision of adequate cycle parking that meets the needs of occupiers of the proposed development and in the interests of encouraging the use of sustainable modes of transport.
- 15 Condition: Prior to first use of the development hereby permitted, details of a suitable electric vehicle charging scheme shall be submitted to and approved by the LPA. The scheme shall be implemented as approved prior to the first use of the development hereby permitted.
- 15 Reason: To ensure the charging is safe, accessible and convenient in accordance with section 112(a) of the NPPF, AQAP, emerging local policy LP14/18 and the NCC's parking standards (July 2022).
- 16 Condition: In the event that contamination is found at any time when carrying out the approved development that was not previously identified it must be reported in writing immediately to the Local Planning Authority. An investigation and risk assessment must be undertaken in accordance with current best practice, and where remediation is necessary a remediation scheme must be prepared, which is subject to the approval in writing of the Local Planning Authority. Following completion of measures in the

approved remediation scheme, a verification report must be prepared, which is subject to the approval in writing of the Local Planning Authority.

- 16 **Reason:** To ensure that risks from land contamination to the future users of the land and neighbouring land are minimised, together with those to controlled waters, property and ecological systems, and to ensure that the development can be carried out safely without unacceptable risks to workers, neighbours and other offsite receptors.
- 17 **Condition:** Notwithstanding the information that accompanied the application, prior to the first occupation of the development hereby approved, a detailed outdoor lighting scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the type of lights, the orientation/angle of the luminaries, the spacing and height of the lighting columns, the extent/levels of illumination over the site and on adjacent land and the measures to contain light within the curtilage of the site. The development shall be implemented in accordance with approved scheme prior to the first occupation of the development hereby permitted and thereafter maintained and retained as agreed.
- 17 **Reason:** In the interests of minimising light pollution to safeguard the amenities of the locality and minimise the impact on bats in accordance with the NPPF and Development Plan.
- 18 **Condition:** Prior to the first use of the development hereby permitted, details of information boards, to be located within the application site (locations to be approved as part of this condition) that indicate nearby public rights of way and alternative visitor attractions not in the proximity of designated sites as well as details of nearby designated sites and recreational pressures upon them shall be submitted to and agreed in writing by the local planning authority. The approved information boards shall be erected in the approved locations, and thereafter be retained and maintained in those locations, prior to the first use of the development hereby permitted. Additionally, information leaflets, containing the same information as the information boards shall be made available to all visitors of the development.
- 18 **Reason:** To ensure the development does not have a likely significant impact on protected sites in accordance with the NPPF, Development Plan and Habitats Regulations.
- 19 **Condition:** The development hereby permitted shall be implemented and retained in full accordance with the Conclusions and Recommendations contained within Chapter 5 of the of the Preliminary Ecological Appraisal and Bat Risk Assessment that accompanied the application (Ref: BOM-RSC-21-11, dated July 2022 undertaken by Bombus Ecology.)
- 19 **Reason:** In the interests of ecology and biodiversity in accordance with the NPPF and Development Plan.
- 20 **Condition:** Construction or development work on site, along with collections and deliveries of waste products, material and equipment, shall only be carried out between the hours of 0800 and 1800 weekdays, and 0900-1300 on Saturdays, with no work allowed on Sundays and Bank/Public Holidays.
- 20 **Reason:** In the interests of the amenities of the locality in accordance with the principles of the NPPF.

- 21 Condition: The development hereby permitted shall be operated in full accordance with the Business Plan that accompanied the application (dated November 2022) and the supplementary information contained within an email from the agent (James Ellis) dated 09 March 2023, 12:24 unless otherwise agreed in writing by the Local Planning Authority.
- 21 Reason: To ensure satisfactory operation of the site in accordance with the NPPF and Development Plan.
- 22 Condition: No development shall take place on any external surface of the development hereby permitted until samples of all materials to be used for the external surfaces of the new and refurbished buildings, including roof materials, have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
- 22 Reason: To ensure a satisfactory external appearance and grouping of materials in accordance with the principles of the NPPF.
- 23 Condition: Notwithstanding the approved plans or additional information that accompanied the application, prior to any works above ground floor finish floor level of the development hereby approved, full details of both hard and soft landscape works shall have been submitted to and approved in writing by the Local Planning Authority. These details shall include the car park, courtyard, access ways / service tracks, permissive footpaths, outdoor play equipment / structures, refuse or other storage units, internal signage, other 'street' furniture, structures and minor artefacts and shall include finished levels / contours and materials. Soft landscape works shall include planting plans, written specifications (including cultivation and other operations associated with plant and grass establishment) schedules of plants noting species, plant sizes and proposed numbers and densities where appropriate of all landscape areas and shall include boundary treatments.
- 23 Reason: To ensure that the development is properly landscaped in the interests of the visual amenities of the locality in accordance with the NPPF.
- 24 Condition: Prior to the first use of the development hereby permitted or within the first planting season all hard and soft landscape works shall be carried out in accordance with the details approved under **Condition 23** of this permission. The works shall be carried out prior to the occupation or use of any part of the development or in accordance with a programme to be agreed in writing with the Local Planning Authority. Any trees or plants that within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species as those originally planted, unless the Local Planning Authority gives written approval to any variation.
- 24 Reason: To ensure that the work is carried out within a reasonable period in accordance with the NPPF.
- 25 Condition: The development hereby permitted shall comprise of no more than 20m² GIA of retail space. The sale of goods shall be limited to camping essentials such as firewood, firelighters, matches, confectionary, snacks, beverages, tent pegs, plasters, toilet roll and a small range of non-perishable food items and toiletries.
- 25 Reason: To ensure an appropriate retail element in accordance with the NPPF and Development Plan.

- 26 Condition: No deliveries shall be taken at or despatched from the site outside the hours of 0700 and 1900 on weekdays, 0900 and 1700 on Saturdays and 1000 and 1700 on Sundays, Bank or Public Holidays.
- 26 Reason: In order that the Local Planning Authority may retain control over the development in the interests of the amenities of the locality in accordance with the NPPF and Development Plan.
- 27 Condition: Prior to the erection of the bell tents hereby permitted full details of the bell tents shall be submitted to and agreed in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details.
- 27 Reason: For the avoidance of doubt and in the interests of proper planning.
- 28 Condition: The development hereby permitted shall include no more than:
20 x hard standing touring caravan pitches
40 x mixed grass tent pitches and
7 x bell-tent pitches.
- The pitches shall be located as shown on drawing no: WKH-DIG-00-PL-0002 Rev.P05.
- 28 Reason: For the avoidance of doubt and in the interests of proper planning.
- 29 Condition: Other than the existing dwelling that occupies the site, the development hereby permitted shall only be occupied as short-stay holiday lets (no more than 28 days per single let), shall only be made available as commercial holiday lets and shall not be occupied as a person's sole or main place of residence.
- 29 Reason: The site lies within in an area in which the Local Planning Authority would not normally permit permanent residential development. This permission is granted because accommodation is to be used for holiday purposes only in accordance with the NPPF.
- 30 Condition: The owners / operators of the development hereby permitted shall maintain an up-to-date register of lettings/occupation of the yurts hereby permitted and shall make this available at all reasonable times to the Local Planning Authority.
- 30 Reason: The site lies within in an area in which the Local Planning Authority would not normally permit permanent residential development. This permission is granted because accommodation is to be used for holiday purposes only in accordance with the NPPF.
- 31 Condition: The development hereby permitted shall at all times be held, owned and operated in association with Wild Ken Hill, Heacham and shall not be sold off separately.
- 31 Reason: For the avoidance of doubt and to ensure that the building is not used for unrelated purposes that would be incompatible with the provisions of the NPPF and Policy DM11 of the SADMPP 2016.
- 32 Condition: The use of the land for camping (tents / bell tents) shall be limited to the period between 1st March and 31st October in any calendar year. No tents / bell tents shall be sited on the land outside of this period.
- 32 Reason: For the avoidance of doubt and to reduce the impacts associated with the proposed development in accordance with the NPPF and Development Plan.

B REFUSE Should the S.106 legal agreement fail to be signed within 4 months of the resolution to approve, on the grounds that it fails to secure the safeguarded land.

Summary of feasibility findings for Heacham A149 Lamsey Lane

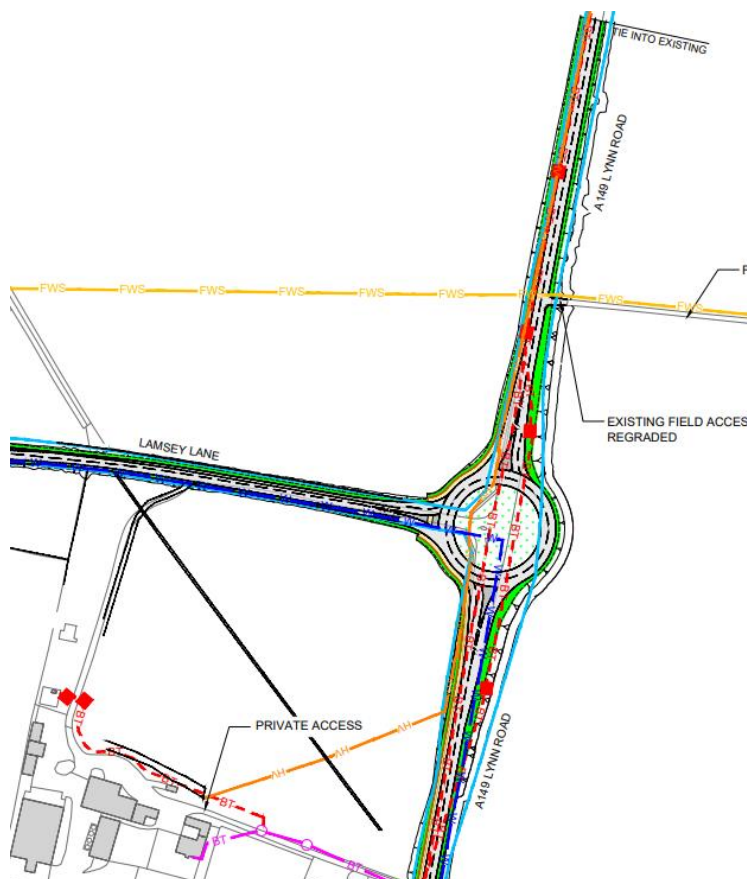
A149 Heacham Lamsey Lane junction

WSP developed options were based on differing arrangements of signalised junction's vs roundabouts with arrangements to give buses priority when exiting Lamsey Lane on a right turn toward Kings Lynn. These options were discounted for costs reasons with estimates ranging between £2.6m & £4.7M

WSP were asked to develop a simple roundabout vs simple traffic signals with no bus priority as a base comparison as either form of junction improvement will produce benefits in bus journey time reliability.

Options:

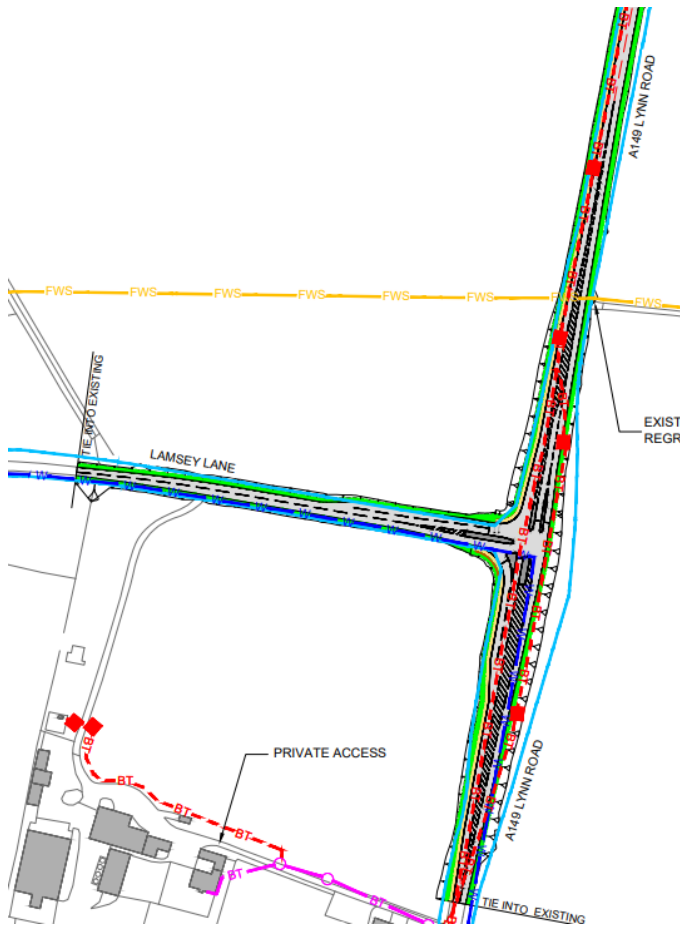
Option 1 - Roundabout



Note – the surrounding land is currently subject to a planning application to deliver a regenerative farm and re wilding site with some accommodation known as Wild Ken Hill. As part of the discussions the land owner has offered land to the west of the site with a condition that highway works commence within 15 yrs, so it is likely that for buildability the roundabout will be built offline to the west.

Advantages	Disadvantages
The reduction of speed limit from 60mph to 50mph would improve the overall safety of the junction where vehicle speeds pose a risk.	Due to the existing vertical alignment, extensive earthworks are anticipated as part of the proposal to ensure sufficient forward visibility is attained.
Provision of informal crossing would provide a safer crossing point for non-motorised users.	Retaining structure or reinforced earthworks may be required along A149 Lynn Road near the existing properties (Heacham Bottom Cottages) if deemed necessary. This will increase the overall project cost.
Existing bus stops present along A149 Lynn Road is to be retained.	Additional land take would be required on the adjacent fields to facilitate implementation of the proposal
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem	Existing statutory utilities within the scheme extents would require diversion.
The existing landscape would be retained.	The existing private accesses within the scheme footprint arms would need to be re-aligned.
	Delays to bus journey times is anticipated.

Option 2 - Traffic Signals



Advantages	Disadvantages
The reduction of speed limit from 60mph to 50mph would improve the overall safety of the junction where vehicle speeds pose a risk.	Due to the existing vertical alignment, extensive earthworks are anticipated as part of the proposal to ensure sufficient forward visibility is attained.
Provision of signalised crossing would provide a safer crossing point for non-motorised users.	Delays to bus journey times is anticipated.
Existing bus stops present along A149 Lynn Road is to be retained.	Retaining structure or reinforced earthworks may be required along A149 Lynn Road near the existing properties (Heacham Bottom Cottages) if deemed necessary. This will increase the overall project cost
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem.	Additional land take would be required on the adjacent fields to facilitate implementation of the proposal.
Tie ins to the existing highway along the major and minor arm is relatively straight forward and should not present a diversion problem.	Existing statutory utilities within the scheme extents would require diversion.
The existing landscape would be retained	The existing private accesses within the scheme footprint arms would need to be re-aligned
	Street lighting would need to be provided to ensure the overall junction is illuminated during

Costs and Value for Money

Option 1 – Roundabout

Predicted costs for 24/25 year build - £2.489m

BCR (considering accident saving and congestion delays) – **Low 1.3:1**

Option 2 – Traffic Signals

Predicted costs for 24/25 year build - £2.566m

BCR (considering accident saving and congestion delays) – **Poor 0.7:1**
 saving and congestion delays) – **Poor 0.91:1**

Wild Ken Hill
carl TONKS consulting

The Wild Ken Hill Experience

Highways Position Statement

Summary

The Wild Ken Hill proposals were considered by Planning Committee on 3rd April 2023. The committee deferred a decision to seek more information from NCC on a feasibility assessment of the A149 / Lamsey Lane junction. The Local Highway Authority (LHA) had recommended approval of the proposals following detailed review of the Transport Assessment (TA) and further Technical Note 2 TA Addendum from **CTC**.

Planning Committee did not request any additional highways information or analysis from the applicant (only a request from NCC on its strategic work). However, a representative of the Parish Council had queried the highways data collected as part of the TA and on that basis, notwithstanding that the applicant, LHA and LPA are in agreement in relation to this, additional information has been procured by way of Automatic Traffic Count (ATC) in April 2023.

The ATC data has been assessed in relation to the original (TA) and the data presented therein, in addition to the update in Technical Note 2. The comparisons shown below confirm that the results published in the TA/Technical Note 2 and those produced from the 2023 ATC result indicate only minor and insignificant differences. Typically, variance in the traffic flows and capacity analyses are within what one would normally anticipate in daily variation and show a reduction in RFC and queuing from the 2023 ATC figures compared to those published in the TA/Technical Note 2, from the MCC. The only deleterious result in this comparison is an increase of only 0.01 in RFC for one movement. Only minor increases in queuing are seen when the 2023 ATC results are factored in and, again, these are within what one would normally anticipate by way of daily variation. Hence, this validates the original traffic survey data used in the TA. Further sensitivity tests factoring to the high tourist season (August) during the peak hours therein confirm spare capacity and low levels of queueing post development.

On that basis, it is clear that the Transport Assessment which has in any case been approved by the LHA, which therefore has no objection to the proposals provides a robust and sound basis for a positive decision on the application.

Statement

1. **CTC** has produced a substantial quantity of analyses on behalf of Wild Ken Hill and examining key traffic and highway matters in regard to their proposed development, at Heacham Bottom and associated camping facilities at Mount Pleasant. The proposals were considered by Planning Committee on 3rd April, at which the decision was deferred in order to seek an update from NCC on a feasibility assessment of the strategic junction improvements, which the LHA has been seeking for some time. This was despite the Application being subject to no objection from Norfolk County Council, as Local Highway Authority (LHA) who, following detailed technical discussions and submission of additional information by the Applicants, were supportive of the proposal.

2. Since the Committee, correspondence from Heacham Parish Council has further emphasised their concerns in regard to the traffic analyses and identified detail points previously touched upon only in passing. This Position Statement will address the issues raised in that additional correspondence, and will demonstrate that the analyses submitted in the Transport Assessment were and remain fit for purpose and appropriate for the decision-making process. This notwithstanding, additional data collection has been undertaken, in order to provide additional confidence in the conclusions reached. This is also described below.

3. The latest correspondence from Heacham Parish Council refers to discussion within the Committee Meeting, at which it claims that the Highways Officer suggested a “**...data disparity...regarding COVID 19...had been added to the outcome of the dataset.**” This is a misunderstanding, in that what was agreed with Highways Officers was that the data collected was entirely appropriate in its basic form, however and this notwithstanding, further adjustments had been made in order to reflect periods of peak traffic demand and these adjustments had been agreed with Highways Officers as appropriate to permit a decision to be made in regard to the Application. The adjustments resulted in an onerous analysis scenario.

4. In regard to impact of COVID 19; the surveys were undertaken at the following periods;
 - Automatic Traffic Counts (ATCs) at two locations on Lamsey Lane
 - 2 weeks spanning 15th July to 28th July 2021; and

 - Manual Classified Count (MCC) – 24th July 2021.

5. In regard to the lifting of COVID 19 restrictions; on 22nd February 2021, then Prime Minister Boris Johnson published “...***the government’s roadmap to cautiously ease lockdown restrictions in England.***” This set out a four-stage approach to releasing constraints on travel and activity, each stage as broadly summarised below;

- Step 1 – Children and students return to face-to-face education.
March 8th

“Wraparound childcare” and other supervised children’s activities recommence.

Care home residents permitted one regular visitor.

People permitted to leave home for outdoor recreation with their family or support bubble.

Some practical courses at University permitted to restart face-to-face teaching.

From 29th March “Stay at Home” Order ended, although some restrictions remained, including Work From Home where possible.

- Step 2 – Non-essential retail, personal care and public buildings re-open.
April 12th

Most outdoor attractions re-open, although indoor mixing remained prohibited.

Indoor facilities (e.g. gyms, swimming pools) re-open, although mixing of family groups remains controlled.

Hospitality venues open outdoors only.

Self-contained accommodation (e.g. holiday lets) re-open.

Funerals permitted with up to 30 people and weddings/commemorative events with up to 15 people.

- Step 3 – Most social contact rules (outdoors) lifted, but gatherings May 17th to be no more than 30 people.

Outdoor performance venues re-open, although limits remain controlling inter-group contact.

Larger performances and sporting events permitted up to 1,000 people (indoor) and 4,000 people (outdoor).

Up to 10,000 people permitted in the larger outdoor venues.

Up to 30 people permitted to attend weddings.

- Step 4 – All legal limits on social contact removed. June 21st

Nightclubs re-open and no restrictions on large events.

6. In fact, implementation of the final Stage (Stage 4) of the Roadmap described above was delayed from June 21st, as planned, to July 19th. This resulted in the first four days of the pre-school holiday ATC being undertaken under Stage 3 of Lockdown Easing, whilst the remaining survey days were under Stage 4, which saw only minimal constraint. Those remaining constraints most notably included international travel, which was only permitted under specific circumstances or for specific reasons, which did not include family holidays. Consequently, the summer of 2021 saw a significant shift away from British families holidaying abroad and instead, remaining in the UK. This is likely to have resulted in additional demand for holidays in north Norfolk's holiday areas, with consequent impact on traffic flows on the A149, Lynn Road. On this basis, it was agreed with Highways Officers that the traffic surveys undertaken under Stage 4 of the Lockdown easing, as described above, would produce reliable results. Each week of the ATC was analysed independently; hence the second week was entirely within Stage 4 easing and the MCC was also under Stage 4 easing.
7. The reference to traffic flows having been factored to reflect increased demand is discussed in Section 5.2 of the Transport Assessment, which details the factoring of traffic surveyed flows, to reflect "peak of the peak" demand during August. The only data source available at the time of compiling these analyses enabled a comparison of March to August traffic demand and this was adopted in the capacity calculations presented. Notwithstanding this it is evident that the traffic survey undertaken in July and within the early stages of the school holiday period will have identified traffic demand much closer to the August holiday peak than would be the case in March of a "normal year" (ie non-COVID) and consequently, the application of a March to August factor to July surveyed flows clearly adds in a substantial and onerous safety margin to the analyses. This is the factoring to which the Highways Officer referred at Committee and clearly results in a forecast on which one can reasonably rely as over-stating the likely traffic demand.

8. Reference was made in the Parish Council submission to DfT summary figures for the years of 2020 and 2021, however, these include agglomerated traffic flows for periods spanning different Steps of the Lockdown easing, hence offer little, if any, insight into what was happening on the specific dates of the surveys.
9. It is clear, as stated above, that the Authority with technical competence to adequately review and comment on the analyses undertaken (ie. the Local Highway Authority) is in agreement with the data collected and the results of the subsequent calculations. It is agreed that those calculations appropriately reflect the observed levels of junction operation and that;
 - The junction is not currently operating typically in breach of its capacity;
 - The junction models and the conclusions drawn therefrom are appropriate, accurately reflect observations and are fit for purpose in determining the Application; and consequently,
 - There are no valid grounds for refusal of permission on highway or traffic impact.
10. Two key points feed into the above;
 1. Was the data collected at an appropriate time and therefore fit for purpose?
And,
 2. Are the analyses undertaken using those data appropriate, hence adequate to rely on in determining the level of traffic impact of the proposals.

11. The data was collected at a time agreed with officers of the LHA as neutral and unlikely to be subject to any suppression due to the previous COVID lockdowns. As described in some detail above, at the time of survey, Britain was coming out of lockdown and the only remaining relevant constraint was on international travel. Consequently, British holiday-makers who may ordinarily have ventured abroad remained in the UK for their holiday.
12. As made clear to the Planning Committee, the data relied upon by objectors in fact confirms that over the month of the surveys, overall traffic demand was slightly below pre-COVID, however, that the beginning of the month was more severely constrained by COVID lockdown (Step 3), and consequently logic suggests that the latter part of the month (Step 4) actually saw traffic flows at, or above “normal” levels, in order at least in part to offset the impact of the early part of the month in lockdown. Consequently it is agreed with officers of the highway authority that the traffic surveys reflected traffic at, at least the level which could be expected under “normal” demand levels and potentially above.
13. The operating calculations confirmed that the junction operates significantly within its ultimate operating capacity in all forecast scenarios and will continue to do so into the future. The Ratio of Flow to Capacity (RFC) remains comfortably within design parameters for all scenarios examined and queue lengths remain small.
14. The capacity for traffic movements from Lamsey Lane on to Lynn Road is determined by the gaps between passing traffic on Lynn Road, which is predominantly a function of through-traffic volume, not traffic turning into, or out of Lamsey Lane and consequently when forecast generated traffic is added into the surveyed traffic flow, the junction continues to exhibit spare operational capacity and the additional turning traffic has little impact on junction operation.

15. This conclusion of the analyses reflects junction operation as witnessed on-site and is agreed by officers of the County Council, as competent technical authority with remit to control traffic and highway matters. It is clear from the above that the analyses confirm there is no defensible highway or traffic reason for refusal of Planning Permission in this instance.
16. Further to the above points, and these notwithstanding, **cTc** has commissioned further traffic surveys in the form of ATCs on both Lamsey Lane and Lynn Road. These were undertaken by Paul Castle Associates from 24th April 2023 to 3rd May 2023. This was done in response to criticisms from Heacham Parish Council, that the survey data agreed with Officers and used in the previously submitted TA and subsequent TA Addendum reports were not representative of typical traffic characteristics. Notwithstanding the above confirmation of the direct relevance of the data collected and in order to remove any potential criticism of the analyses, these additional surveys were commissioned. The full ATC reports are included as Appendix A.
17. The previous analyses from the MCC identified the following Saturday Peak Hours:
 - AM Peak Hour: 11:00 to 12:00
 - PM Peak Hour: 14:30 to 15:30
18. As the MCC records data in 15 minute time segments, a more precise peak hour is identified. The ATCs record hourly data and therefore identify peak hours to the nearest whole hour. Therefore, to ensure an accurate comparison and analysis, two PM hours from the ATC have been used, either side of the PM Peak Hour from the MCC. These details and comparisons are shown in Table 1, below.

Table 1: MCC and ATC Comparisons

Period	Location	Direction	MCC	ATC	Difference	Relevant Turning Movement	MCC to ATC Factor
11:00 – 12:00	Lamsey Lane	Eastbound	96	90	-6	Lamsey Lane to Lynn Road Left and Right	0.9375
		Westbound	214	196	-18	Not Used	N/A
	Lynn Road	Northbound	737	793	+56	Lynn Road S Left and Ahead	1.0760
		Southbound	581	586	+5	Lynn Road N Right and Ahead	1.0086
14:00 – 15:00	Lamsey Lane	Eastbound	79	92	+13	Lamsey Lane to Lynn Road Left and Right (14:30 – 15:00)	1.1646
		Westbound	223	208	-15	Not Used	N/A
	Lynn Road	Northbound	685	781	+96	Lynn Road S Left and Ahead (14:30 – 15:00)	1.1401
		Southbound	603	639	+36	Lynn Road N Right and Ahead (14:30 – 15:00)	1.0597
15:00 – 16:00	Lamsey Lane	Eastbound	103	85	-18	Lamsey Lane to Lynn Road Left and Right (15:00 – 15:30)	0.8252
		Westbound	210	205	-5	Not Used	N/A
	Lynn Road	Northbound	637	738	+101	Lynn Road S Left and Ahead (15:00 – 15:30)	1.1401
		Southbound	688	670	-18	Lynn Road N Right and Ahead (15:00 – 15:30)	0.9738

19. The above table demonstrates that, typically, traffic flow on Lamsey Lane is slightly lower in the 2023 ATC than the 2021 MCC; and slightly higher on Lynn Road. These are not vastly significant differences, and clearly any two traffic surveys conducted on different days would show a degree of variance. The differences observed are within the bounds of what one would normally expect by means of daily variation, especially once one allows for the passage of almost two years, during which normal traffic growth has occurred. Nonetheless, and in order to establish whether the variance has a bearing on the conclusions drawn previously regarding the Lynn Road / Lamsey Lane junction, further PICADY capacity analyses have been conducted using the factored base flows.

20. The factors stated above show the degree of variance between specific turning movements at the quoted times between the 2021 MCC and 2023 ATC. As such, they also act as temporal growth factors. Hence, once applied to the 2021 MCC base turning movements these are automatically growthed to 2023 turning movements; and are, therefore, directly comparable with the “2023 with Development” capacity analyses quoted in the TA. Figure 1 shows the factored turning movements with proposed development traffic as flow diagrams. Table 2, below, summarises the PICADY results, with the full PICADY report provided as Appendix B.

Table 2: PICADY Results for the Lamsey Lane / Lynn Road junction.

Scenario	Movement	From TA		Factored from 2023 ATC		Difference	
		Max RFC	Max Q	Max RFC	Max Q	Max RFC	Max Q
2023 + Dev AM Peak	Lamsey Lane to Lynn Road North	0.17	0.2	0.16	0.2	-0.01	0.0
	Lamsey Lane to Lynn Road South	0.16	0.2	0.15	0.2	-0.01	0.0
	Lynn Road to Lamsey Lane	0.13	0.1	0.13	0.1	0.00	0.0
2023 + Dev PM Peak	Lamsey Lane to Lynn Road North	0.40	0.6	0.34	0.5	-0.06	-0.1
	Lamsey Lane to Lynn Road South	0.32	0.5	0.27	0.4	-0.05	-0.1
	Lynn Road to Lamsey Lane	0.09	0.1	0.10	0.1	+0.01	0.0

21. The comparisons shown above confirm that the results published in the TA and those produced from the factors obtained from the 2023 ATC result in only minor and insignificant differences. Typically, variance in the capacity analyses show a reduction in RFC and queuing from the 2023 ATC figures compared to those published in the TA from the MCC. Hence, this validates the original traffic survey data used in the.

22. To further ensure a thorough and robust validation of the MCC survey data a seasonal uplift has been applied to the above analyses. As previously noted, the 2023 ATC from which the MCC has been factored was undertaken from 24th April 2023 to 3rd May 2023; as such the data has been further factored to August levels of base traffic flow. Norfolk County Council has provided additional seasonal monthly variation factors for the A149 at Heacham. This data confirmed the following AADF monthly variation factors (from the annual average month):
- April: 0.98
 - May: 1.08
 - April and May Average: 1.03
 - August: 1.37
23. The factor from April/May to August is, therefore:
- $1.37 / 1.03 = \mathbf{1.33}$
24. The summer uplift factor has been applied to the base flows; these are shown as flow diagrams in Figure 2, and with development in Figure 3. PICADY capacity analyses have been undertaken for these scenarios, and this is summarised in Table 3, below, with the full PICADY report presented as Appendix A.

Table 3: PICADY Results for the Lamsey Lane / Lynn Road junction (August 2023).

Scenario	Movement	Max RFC	Max Q
August 2023 No Dev. AM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.35	0.5
	Lynn Road to Lamsey Lane	0.13	0.2
August 2023 With Dev. AM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.40	0.6
	Lynn Road to Lamsey Lane	0.20	0.2
August 2023 No Dev. PM Peak	Lamsey Lane to Lynn Road North	0.25	0.3
	Lamsey Lane to Lynn Road South	0.29	0.4
	Lynn Road to Lamsey Lane	0.16	0.2
August 2023 With Dev. PM Peak	Lamsey Lane to Lynn Road North	0.61	1.4
	Lamsey Lane to Lynn Road South	0.65	1.4
	Lynn Road to Lamsey Lane	0.16	0.2

25. The capacity analyses quoted in the above table confirm that the junction operates within capacity, and with minimal queuing, both before and after the proposed development, even under this scenario of the peak hour within the peak month.
26. In conclusion, the 2021 MCC has been validated by the 2023 ATC data; and this supports the view of NCC Officers that accepted the TA and all of its' analyses. Furthermore, capacity analyses under peak hour in high season conditions show spare capacity remains at the Lamsey Lane / Lynn Road junction.

Client:		Wild Ken Hill	
Project Name:		The Wild Ken Experience	
Project Number:		2021-F-015	
Report Title:		Highways Position Statement	
Created by:	Carl Tonks	Date:	May 2023
Proofed by:	Jacqueline Ireland	Date:	May 2023
Approved by:	Carl Tonks carl@tonks-consulting.co.uk	Date:	May 2023
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22/02113/F

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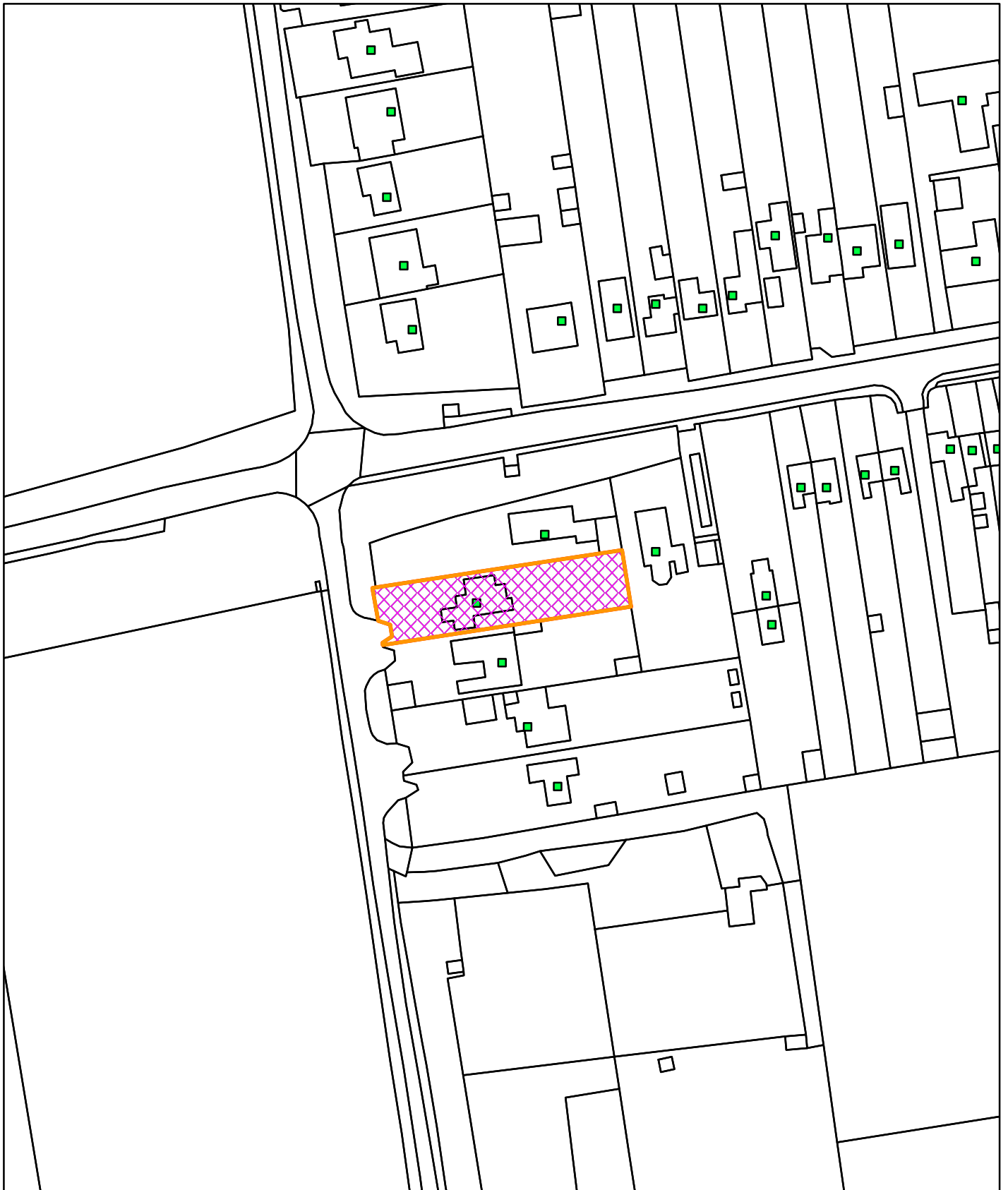
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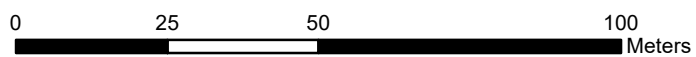


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23/05/2023

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Parish:	Holme next the Sea	
Proposal:	VARIATION OF CONDITION 2 OF PLANNING CONSENT 21/01394/F: Extensions and alterations to dwelling (Retrospective)	
Location:	Terns 49 Peddars Way Holme next The Sea Norfolk	
Applicant:	Mr And Mrs Howitt	
Case No:	22/02113/F (Full Application)	
Case Officer:	Mrs Jade Calton	Date for Determination: 19 January 2023 Extension of Time Expiry Date: 12 June 2023

Reason for Referral to Planning Committee – Called in by Cllr Lawton

Neighbourhood Plan: Yes

Members update

Members may recall that this application was deferred from the 3rd April 2023 Planning Committee in order to investigate irregularities with the plans.

Following a further site visit to measure the extension, it can be confirmed that the development within the red line is correctly depicted on the submitted plans. However, the neighbouring dwelling to the south, No.51, appears to be incorrectly plotted on the plan, which has been extracted from OS Maps.

The extension, as measured on site, is located 1m from the shared boundary fence, which accords with the submitted plans. The neighbouring dwelling, however, is sited approx. 0.85m from the shared boundary, as opposed to 1.6m as shown on the plans.

For ease of reference amendments to the April report (inclusive of previous Late Correspondence) are presented in emboldened text.

Case Summary

The application site comprises a detached bungalow situated on the eastern side of Peddars Way (south), Holme-Next-The-Sea.

Retrospective consent is sought to vary the approved plans condition of planning permission 21/01394/F to include an additional single storey rear-side extension to the dwelling to provide a bedroom.

Holme is classified as a Smaller Village and Hamlet under the Local Development Plan.

Key Issues

- * Principle of Development
- * History
- * Form and Character
- * Neighbour Amenity
- * Other Material Considerations

Recommendation

APPROVE

THE APPLICATION

The application site comprises a detached bungalow situated on the eastern side of Peddars Way (south), Holme-Next-The-Sea.

Terns, No.49 is one of a row of five dwellings fronting the eastern side of Peddars Way. Bungalows are sited directly to the north and south of the site and two storey dwellings further south.

The site is located on the edge of the village and is therefore rural in character with paddock land further to the south and open countryside on the opposite side of the road.

Retrospective consent is sought to vary the approved plans condition of planning permission 21/01394/F to include an additional single storey rear-side extension to the dwelling to provide a third bedroom.

The single storey extension comprises a flat roof, measuring approx. 2.8m in total height. The footprint measures 3.2m x 2.3m, which gives a floor area of 7.36 msq.

The extension has been rendered off-white to match the rest of the dwelling.

Whilst the extension is constructed to the rear of the existing attached garage, it is also located on the side elevation of the dwelling, which does not constitute permitted development within the AONB.

The extension is approx. 1m from the southern boundary of the site and the neighbouring bungalow is a further **0.85m** from the shared boundary.

The site is bounded by a 1.5m close boarded timber fence to the north and south along with some semi-mature garden trees to both boundaries.

Following the submission of the late correspondence, set out below, further investigation has been carried out on site and it can be confirmed that the extension has been measured and plotted on the submitted plans accurately. Thereby meaning that the extension is correctly shown to be sited 1m away from the shared boundary with the neighbouring property to the south.

However, it has become apparent that there is a discrepancy between the plan and the measurements on site in so far as the neighbouring dwelling to the south (No. 51) is concerned. The plan shows the neighbouring dwelling sited 1.6m away from the shared boundary, whereas in reality, it is approx. 0.85m away (as measured on site by the planning officer).

This is a discrepancy with the OS mapping of the neighbouring dwelling and the applicant / agent is not obligated to check off-site measurements and precisely depict any buildings outside of the red line. Providing the property and the proposed development are plotted correctly within the red line on the submitted plans the application is valid and can be properly assessed.

SUPPORTING CASE

A Design and Access Statement accompanies the application and offers the following conclusion: -

'This proposal would provide a small variation to the original scheme approved in December 2021. The small amendment fits well with the original scheme and together is considered to be less than the 40% increase in footprint of the original dwelling thereby complying with the Policy on the matter.

It is not believed there will be any overlooking of the neighbours dwelling or garden and there would be no loss of neighbour amenity.

The nature of the design and use of materials would complement that which was agreed in the original scheme and the plans submitted with this application confirm that. There is no harm to the neighbours or any interests of acknowledged importance'.

PLANNING HISTORY

22/01213/LDP: Not Lawful: 17/10/22 - Extension to rear of existing garage

21/01394/F: Application Permitted (Delegated): 03/12/21 - Extensions and alterations to dwelling

RESPONSE TO CONSULTATION

Parish Council: OBJECT –

- Planning history
- The proposed works have already been carried out.
- No prior opportunity for consultation.
- Unconsented development is close to the neighbouring property to the south.
- Negative impact on neighbour amenity.
- Loss of daylight
- Impact on health and well-being.
- The submitted plans are confused.
- Trying to present the proposal as a minor, inconsequential change.
- Views from Terns over the replaced, lower fence
- Impact on privacy
- Poor design

Planning Committee
5 June 2023

- Enclosed environment
- The changes have introduced a more urban character to this rural village setting.
- Contrary to para. 130 of the NPPF – development should function well and add to the overall quality of the area.
- Para. 130 states that development should promote health and well-being, with a high standard of amenity – the proposal does not satisfy this criteria.
- Contrary to para. 132 of the NPPF – early discussions between applicants, the LPA and community are encouraged.
- Para. 135 states that the LPA should seek to ensure that the quality of approved development is not materially diminished between permission and completion, as a result of changes being made to the permitted scheme – This is not the case.
- Contrary to SADMP Policy DM15 – adds weight to national policy noting that development must protect and enhance the amenity of the wider environment.
- Contrary to SADMP Policy DM15 - proposals will be assessed against their impact on neighbouring uses and their occupants highlighting the importance of considerations.
- DM15 is clear that development that has a significant adverse impact on the amenity of others or which is of a poor design will be refused.
- Contrary to Neighbourhood Plan Policy HNTS11 - ensure that the existing amenity of residents is not adversely affected by new development including extensions, alterations, replacement of existing buildings or redevelopment of sites.

The Parish Council urges the Borough to refuse this application. If despite the strong policy arguments in favour of refusal the BC is minded to recommend approval, we request that a condition is applied to restrict any future, remaining Permitted Development Rights.

The Parish Council submitted late correspondence and were registered to speak at the 3rd April committee. They raised the following additional comments: -

- **Incorrect drawings.**
- **Inaccuracies in the officer report.**
- **Latest set of drawings posted one month after the consultation responses had been made.**
- **Trying to present as minor inconsequential alterations.**
- **Considerable significance to the neighbouring property to the south.**
- **Sets out history of applications on the site.**
- **The gap has been re-measured on site by the neighbour and Parish and is 1.8m not 2.6m as per the plans and report.**
- **Impact on amenity and well-being.**
- **Inaccurate representation of the eaves and guttering.**
- **Does not pass the BRE 25 degree test for light impact.**
- **Why was the separation distance not checked?**
- **How was BRE applied without entering the neighbour's property?**
- **On what basis has a judgement been reached about the impact on outlook and light levels in the Neighbours' rooms if the property hasn't been visited?**
- **How can the Officers Report reasonably reach the conclusion that a snug and a study are not 'main living spaces' in the home of a retired couple?**

REPRESENTATIONS

TWO representations received from third parties **OBJECTING** to the proposal on the following grounds: -

- Neighbouring bungalow is directly adj. to the southern boundary of Terns, No.49 Peddars Way.
- Two north facing windows that look directly onto the southern aspect of the variation.
- Impact on visual aspect
- Deprivation of light
- Already built
- Planning history
- Close proximity
- There is approx. 1m between the extension and boundary fence.
- The height of the new extension is approx. 42cm above the new garage roof.
- Overbearing
- Mass
- Incorrectly stated that the extension cannot be seen from the road.
- Incorrect plans
- Information within the application shows historic planting which is no longer there.
- None of the other 1960's bungalows have been extended by 40%
- The Neighbourhood Plan aims to retain smaller dwellings.
- Neither the side extension nor the garage extension were included in the original application.
- Not just a small variation.
- Accuracy of the GIA calculations regarding the porch.
- The additional GIA is referred to as 7% and at other times as 7sqm.

TWO representations received under late correspondence from Third Parties in response to the officer's report. One of the third parties was registered to speak at the 3rd April meeting. The following OBJECTIONS were raised: -

- **Inaccuracies within the officer report.**
- **No.51 is 0.83m away from the shared boundary and NOT 1.6m.**
- **The correct distance between the extension and neighbouring flank elevation is 1.83m and NOT the reported 2.6m.**
- **The eaves and gutters are approx.. 35cm therefore enclosing the gap further still.**
- **The neighbour's side windows are considered to serve main living areas as they are used all the time, for ready, recreation, crafts and home working.**
- **These activities require good natural light levels.**
- **The original garage had no impact on the outlook from the neighbour's north facing windows as it did not extend as far as those windows.**
- **The smaller original garage (4.65m deep) was knocked down and replaced with the extension.**
- **This was replaced with a smaller garage (just over 3m in depth and 2.1m in height).**
- **A larger extension was built behind the garage (over 5m in depth and 2.8m in height).**
- **Dominating and oppressive impact on neighbour's outlook given its size and closeness to the boundary.**
- **The gable of No.49 is some 3.75m away and therefore has no effect on loss of daylight.**
- **Neighbour's could see the open sky above the old garage and above the roof of the front bedroom at No.49.**
- **The open sky is no longer visible because of the extension.**
- **We understand that the impact on daylight from a development is based on accurate measurements being taken between the neighbouring properties, the centre of the habitable window affected and the height of the extension.**

- We have indicated in earlier correspondence that these measurements have been shown to be inaccurate.
- Therefore, the determination of 'no significant loss of daylight' is incorrect and should be recalculated before such a statement is made.
- The extension has resulted in loss of light.
- Its close proximity has had a dominating and overbearing impact on our two north facing rooms.
- It most definitely has had a significant detrimental impact on our neighbour amenities, contrary to the officer report.
- Multitude of drawings different, confusing and misleading.
- Do not have the benefit of a sophisticated measuring tool to assess the accuracy of the drawings.
- We would have hoped or even expected, the Architects, Agents or Planning Department to have checked these for accuracy before drawing conclusions about the application.
- The final confirmed drawing was submitted after the consultation process was completed.
- The fact that the applicants chose not to incorporate this extension into their original application 21/01394/F even though the plans were apparently created on the same day – 28.07.2021 (some 21 months ago), but decided to build this extension under permitted development, denied us the opportunity of being consulted.
- They were aware that it would have a huge impact on our home.
- The fact that this extension was then judged to be 'unlawful', necessitated this variation to be submitted.
- It is vital, that at the very least, recommendations made to the Planning Committee are factually correct.
- We welcome the conditions the Planning Officer is recommending, regarding any future developments at Terns No 49
- However we are mystified why the obscure glazing condition has been recommended for the rooflight as the new extension obliterates any view of this rooflight.
- We believe this demonstrates how the Planning Officer appears to have failed to appreciate the impact this development has had on our amenity.
- Demolition of the existing garage and construction of the new garage was not included on the original application.
- Therefore no neighbour consultation on this element.
- Suggested alternatives to provide additional accommodation.
- Quotes from the Neighbourhood Plan in regard to neighbour amenity.
- Homeworking is increasing therefore the study should be considered to be a main living space.
- The previous extension offered a feeling of spaciousness and better light levels.
- Overbearing.

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS12 - Environmental Assets

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

DM15 – Environment, Design and Amenity

NEIGHBOURHOOD PLAN POLICIES

Policy HNTS11: Street Scene, Character and Residential Environment

Policy HNTS17: Extensions, Annexes and Outbuildings

Policy HNTS20: AONB Landscape Quality

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations in the determination of this application are as follows: -

- Principle of Development
- History
- Form and Character
- Impact on Neighbour Amenity
- Other Material Considerations

Principle of Development:

The application site comprises a residential property, thereby the principle of an extension to the dwelling is acceptable in accordance with the Development Plan.

This application has been submitted retrospectively, which is not contrary to planning law, and the section 73a process is designed to regularise any such development that has been 'carried out without complying with some condition subject to which planning permission was granted' (namely 21/01394/F).

History:

The original application (21/01394/F) granted consent for single storey extensions and alterations to the dwelling, including a small extension to the existing front porch and a kitchen / diner extension to the rear.

The application also included rendering the external surface of the dwelling and the installation of a small area of Cedar boarding to the apex of the front gable.

The original scheme was negotiated to reduce the scale and mass of the proposed extensions in order to improve the design and to comply with Neighbourhood Plan policies, in particular HNTS 17 (Extensions, Annexes and Outbuildings).

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Following the submission of the amended plans, the scheme was considered to comply with Policy HNTS 17, in terms of the proposed extensions being within the allowed 40% increase in gross internal floor area (GIA) of the original dwelling.

The GIA of the original dwelling measured approx. 116m² and the proposed extensions measured approx. 39m², creating a total GIA of 155m². The approved scheme therefore amounted to an increase in GIA of 33% of the original dwelling.

Subsequently, the applicant pursued utilising the remainder of their 40% allowance to make the standard of their home meet their requirements. A Lawful Development Certificate (22/01213/LDP) was submitted in presumption that the small 7% extension to the rear of the existing attached garage would benefit from Permitted Development (PD) Rights under Part 1, Class A of the General Permitted Development Order 1995 (GPDO).

During the course of the LDP application the small extension was constructed as it was believed to have complied with the relevant criteria under the GPDO.

Whilst the proposed extension extended beyond the rear wall of the existing garage (which is part of the original dwelling), it also extended to the side of the dwelling, thus infilling the area between a side elevation and rear wall. The GPDO clearly states that side extensions are not permitted within Article 2(3) land (AONB).

Tighter restrictions are placed on PD Rights for dwellinghouses sited within the AONB.

The applicant's agent challenged this line of reasoning and after some research it was evident that there has been a number of appeal cases (under other authorities) where the Planning Inspector had considered the issue of proposed development which extends beyond a rear wall and a side wall of a dwellinghouse under Class A.2 of the GPDO.

An Inspector stated that regard must be had to the provisions of the 2015 Order (GPDO) and the ordinary meaning of the language used, and for the Householder PD Technical Guidance which provides assistance in the interpretation of Class A of the 2015 Order. The technical guidance states: -

'Where an extension fills the area between a side elevation and a rear wall, then the restrictions on extensions beyond rear walls and side walls will both apply'.

As such the proposal was not considered to comply with the GPDO and planning permission was required.

Accordingly, the current application was submitted in order to regularise the development.

Form and Character:

The established development along the eastern side of Peddars Way [South] is limited, with only five dwellings in a loose ribbon form, set back from the road with verdant frontages.

Whilst the directly adjoining neighbouring properties to the north and south are also bungalows, they take a different form to the dwelling subject of this application. Furthermore, the last two dwellings in the row are of two storey construction, thereby the street scene has a varied appearance.

The rear-side extension is barely seen from the public domain as it is set behind the existing garage. The only limited visible element is part of the flat roof due to it being approx. 450mm taller than the garage flat roof.

There is an established hedgerow to the site's frontage along with some semi-mature garden trees to the northern and southern front boundaries, which partially screen the site and softens the built form.

Whilst the site lies within the AONB, it is not within a Conservation Area and the development is barely visible from the road, therefore cannot be said to cause any visual harm to the street scene.

Notwithstanding the 40% restriction in GIA under the Neighbourhood Plan (NP) Policy (to which the proposal complies), it is considered that the small extension to the rear will have no impact on the form, character or appearance of the area, which is fundamentally what the NP has set out to achieve. The application site is more than capable of accommodating the minor extension in addition to the previously approved extensions, whilst causing no harm to the wider AONB landscape character.

As such, it is considered that the proposed variation accords with Local Plan Policies CS06, CS08 and DM15; NP Policies HNTS 11, HNTS 17 and HNTS 20; and the general provisions of the NPPF, but specifically sections 12 and 15.

Neighbour Amenity:

The extension subject of this application is located to the southern side elevation of the dwelling and will therefore have an impact on the neighbouring property at No.51. However, the impact will be limited due to its small scale and flat roof design.

The extension is approx. 2.8m in height and is approx. **1.85m** away from the neighbouring flank elevation. The neighbour has raised objections regarding loss of light and overbearing impact on their north facing windows which are directly adjacent to the extension.

The existing garage extension, which formed part of the original dwelling, is already in relatively close proximity of the neighbouring boundary and has a degree of impact on the neighbour's outlook from their north facing windows, albeit not significant. The extension is reasonably modest, measuring approx. 3.2m in depth, and whilst slightly taller than the existing garage by 500mm, it is not excessive in height.

The existing gable of Terns No.49 sits behind the extension, thereby already obstructing some of the open sky visible from the neighbouring side windows. Whilst the extension brings the built form closer to the neighbouring window and will be visible, it is of a low level single storey flat roof construction and the impact will therefore not cause a significant loss of daylight to those habitable rooms. For these reasons, it is also not considered that the small scale extension would cause a material overbearing impact on those residents.

Whilst inaccurate measurements of the neighbouring dwelling were taken from the submitted plans and quoted in the original offer report, the development had been fully assessed based on the existing situation on the site.

As mentioned by the third party, there is a 'rule of thumb' referred to as the 25° test under the BRE (Building Research Establishment) guidance, to assess the impact on daylight and sunlight where a development is opposite a neighbouring window.

The centre of the lowest habitable room window should be used as the reference point and if the whole of the proposed development falls beneath a line drawn at 25° from the horizontal, then there is unlikely to be a substantial effect on daylight and sunlight. This was deemed to be the case when the proposal was assessed on the inaccurate measurements from the site plan.

However, the guidance states that 'if the proposed development goes above the 25° line, it does not automatically follow that daylight and sunlight levels will be below standard'. Nevertheless, it does mean that further checks on daylight and sunlight are required. The further checks can be undertaken using the detailed BRE daylight and sunlight.

Based on the officer's assessment of the correct siting of the neighbouring dwelling, being approx. 0.85m from the shared boundary, the extension goes above the 25 degree line very slightly (the top 300mm). Therefore further assessment is required.

These additional detailed tests are set out in BRE guidelines which provide the principal measures of daylight for assessing the impact on properties neighbouring a site, namely Vertical Sky Component (VSC), Daylight Distribution / No-Sky Line (NSL) and Daylight provision.

In terms of sunlight, BRE advise using the Annual Probable Sunlight Hours (APSH) and Exposure to Sunlight guidance.

Given the scale of the proposed development and the limited breach over the 25 degree line, together with the site assessment, it is considered that it would be unreasonable to insist that the applicant instructs a 'right to light' consultant to prepare a daylight and sunlight study to support the application.

Furthermore, the BRE guidelines are not mandatory and should not be used as an instrument of planning policy. Although local planning authorities and planning inspectors can consider the suitability of a proposed scheme for a site within the context of BRE guidance, consideration will be given to the context within which a scheme is located, and the daylight and sunlight will be one of several planning considerations which the local authority will weigh.

The Local Authority does not have an adopted policy which specifically guides the assessment of daylight and sunlight and any impact on neighbour amenity from development. The officer's professional judgement was used having made two site visits and having assessed the development appropriately.

On the basis of the above, it is considered that the limited reduction in light and minor impact on outlook from the neighbouring windows, do not result in a significant impact on their amenity to warrant refusal of the application.

Any outlook from the window in the extension towards the neighbouring property would be too acute to cause any significant loss of privacy. Furthermore, the 1.5m close board fence adequately screens the windows on the extensions and will therefore cause no material overlooking.

It is also relevant to note that the subject extension would be permitted development if the site was not located within the AONB.

Summary:

Following further site investigations, whilst it has become apparent that the plans taken from the OS map are not entirely accurate in regard to the siting of the neighbouring dwelling, this will not prejudice any decision. The application site and the development has been plotted correctly on the submitted plans and the impact of the development has been assessed on site. As such, it remains the officer's opinion that whilst the neighbouring residents will see the extension from their north facing windows, any impact would not be so significant to warrant refusal of the application.

Furthermore, notwithstanding the above BRE guidelines, it is still often possible to achieve adequate daylight and sunlight (even with larger than 25 degree obstruction angles). The affected windows are north facing and as such already receive limited light as a result of the existing gable obstructing the view of the sky. As the majority of the extension is located in front of the gable-end, the increase in new built form is not significant and as such the affected windows do not have a completely obstructed view of the sky. Whilst there is some limited impact from the development, it is not considered that the diffuse daylight is adversely affected over and above what would have been the case prior to the extension having been built.

It is considered therefore that the development complies with Neighbourhood Plan Policies HNTS 11; Local Plan Policies CS08 and DM15; and the general provisions of the NPPF.

Other Material Considerations:

The Parish Council and third-party comments have been taken into consideration in reaching a recommendation for this application, most of which have been addressed above in the report.

It has been questioned whether the development can be deemed to be a minor material amendment under section 73a of the Planning Act. There is no statutory definition of 'minor material amendment', it is dependant on the context of the overall scheme and at the discretion of the LPA as to whether or not it is considered to amount to a fundamentally different planning permission.

In weighing the planning balance, the LPA does not consider the proposed change results in a development that is substantially different from the one which was approved.

The concern over the plans being confused has been rectified by way of the submission of up-to-date, more accurate plans.

The concerns raised regarding the impact on the neighbouring resident's health and well-being have been considered and as set out above in the report, any resulting impact from the minor extension is not deemed to be so significant to warrant refusal of the planning application.

Based on the findings set out within the report above, there is no reason why the LPA would consider the development to be contrary to Development Plan Policies DM15 and HNTS11; or paragraphs 130, 132 or 135 of the NPPF as suggested by the Parish Council. The additional extension is barely seen from the road and is single storey with a flat roof, therefore cannot be said to be of a poor design that would impact on the character of the area or have an adverse impact on neighbour amenities.

In regard to the third party comments referring to compliance with NP Policy HNTS17, as stated above, the previously approved extensions amounted to an increase in GIA by 33% of the original dwelling, and with the additional extension being 7%, the development accords with the restrictions of the aforementioned Policy.

Finally, the condition suggested by the Parish Council relating to the removal of PD Rights is considered to be acceptable and reasonable in order to meet the requirements of the Neighbourhood Plan. The relevant condition will be recommended to be attached to the decision if approved.

The additional third party concerns have been taken into full consideration and have been addressed above in the report in bold.

In response to the Parish Council's additional queries, some of which have been addressed above in the report, the LPA can only presume that the submitted plans are correct, as the discrepancy was not clear on site during the initial site visit. Planning officers are not obligated to check measurements on site unless it is absolutely necessary to do so. The neighbour's letter stated that their measurement was an estimation, therefore the plans were relied upon to check. Following the issue having been brought clearly to the attention of the officer, the application was deferred from the April committee and thorough checks have taken place on site.

The initial 25 degree BRE test does not involve entering the property which is being assessed, it clearly states that it can be undertaken from the external elevations. Furthermore, it is not common practice to enter a property to assess neighbour amenity, this can be done adequately on site and from the proposed plans, using the officer's professional judgement.

The officer report considered the affected rooms to be habitable but due to their use, being a study and a snug, they were not considered to be 'main living areas' such as a sitting room, bedroom or kitchen. Notwithstanding the use of the rooms, it is considered that the additional loss of light or outlook does not amount to causing a significant detrimental impact on the living conditions of the neighbouring residents in planning terms.

In regard to the 'misrepresentation of the eaves and guttering' on the extension, these elements are considered to be de minimis, and it is not essential to show them on the submitted plans as they would generally cause no material impact on neighbour amenity.

A recommendation has been reached having considered a number of material matters, all of which, as set out above, formed the overall planning balance.

CONCLUSION

The modest flat roof single storey extension to the rear of the existing garage is deemed to be a minor material amendment to the original approval (21//01394/F) under s.73a of the Planning Act.

By virtue of its small scale and location set behind an existing element of the original dwelling, it is not considered to cause any visual harm to the street scene or impact on the wider landscape character of the AONB.

Notwithstanding the inaccuracy of the submitted plans in relation to the siting of the neighbouring dwelling, in weighing the issues and considering the planning balance, it is not considered to result in a significant detrimental impact on neighbour amenities, in terms of loss of light, overshadowing, overbearing or loss of privacy, due to its low-level height, flat roof design, the separation distance between properties and screening from existing boundary treatment.

The plans demonstrate that the extension accords with NP Policy HNTS 17 as the overall development does not result in an increase in GIA of the original dwelling by more than 40%.

In summary, the development accords with the aims and objectives of NP Policies HNTS 11, HNTS 17 and HNTS 20; Local Plan Policies CS06, CS08, CS12 and DM15; and the general provisions of the NPPF.

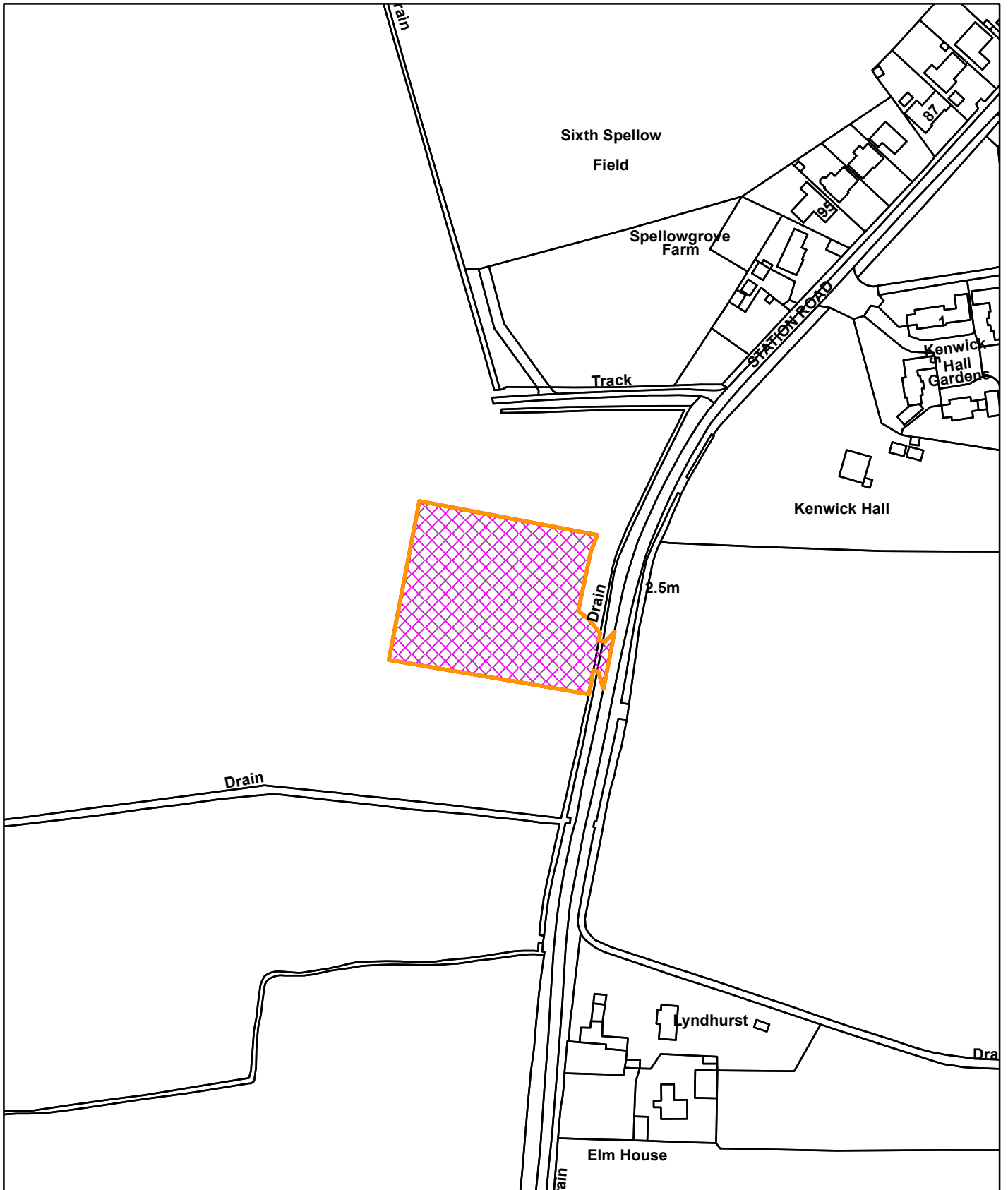
RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development has been determined in accordance with the following approved plans; 571-02 received 18th Jan 2023.
- 1 Reason: For the avoidance of doubt and in the interests of proper planning.
- 2 Condition: Within 6 weeks of the date of this permission, the rooflight to southern roofslope serving the bathroom shall be obscurely glazed and shall be retained as such thereafter.
- 2 Reason: In the interests of protecting neighbour amenity, in accordance with the provisions of the NPPF.
- 3 Condition: Notwithstanding the provisions of Schedule 2, Part 1, Classes A, B, C and D of the Town and Country Planning (General Permitted Development)(England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), the enlargement, improvement or other alteration of a dwelling house, the enlargement of a dwelling house consisting of an addition or alteration to its roof, or the erection or construction of a porch outside any external door of a dwelling house, shall not be allowed without the granting of specific planning permission.
- 3 Reason: In order that the Local Planning Authority may retain control of development which might be detrimental to the amenities of the locality if otherwise allowed by the mentioned Order.
- 4 Condition: Notwithstanding the provisions of Schedule 2, Part 1, Class E of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), the provision within the curtilage of the dwelling house of any building or enclosure, swimming or other pool shall not be allowed without the granting of specific planning permission.
- 4 Reason: In order that the Local Planning Authority may retain control of development which might be detrimental to the amenities of the locality if otherwise allowed by the mentioned Order.

22/02114/F

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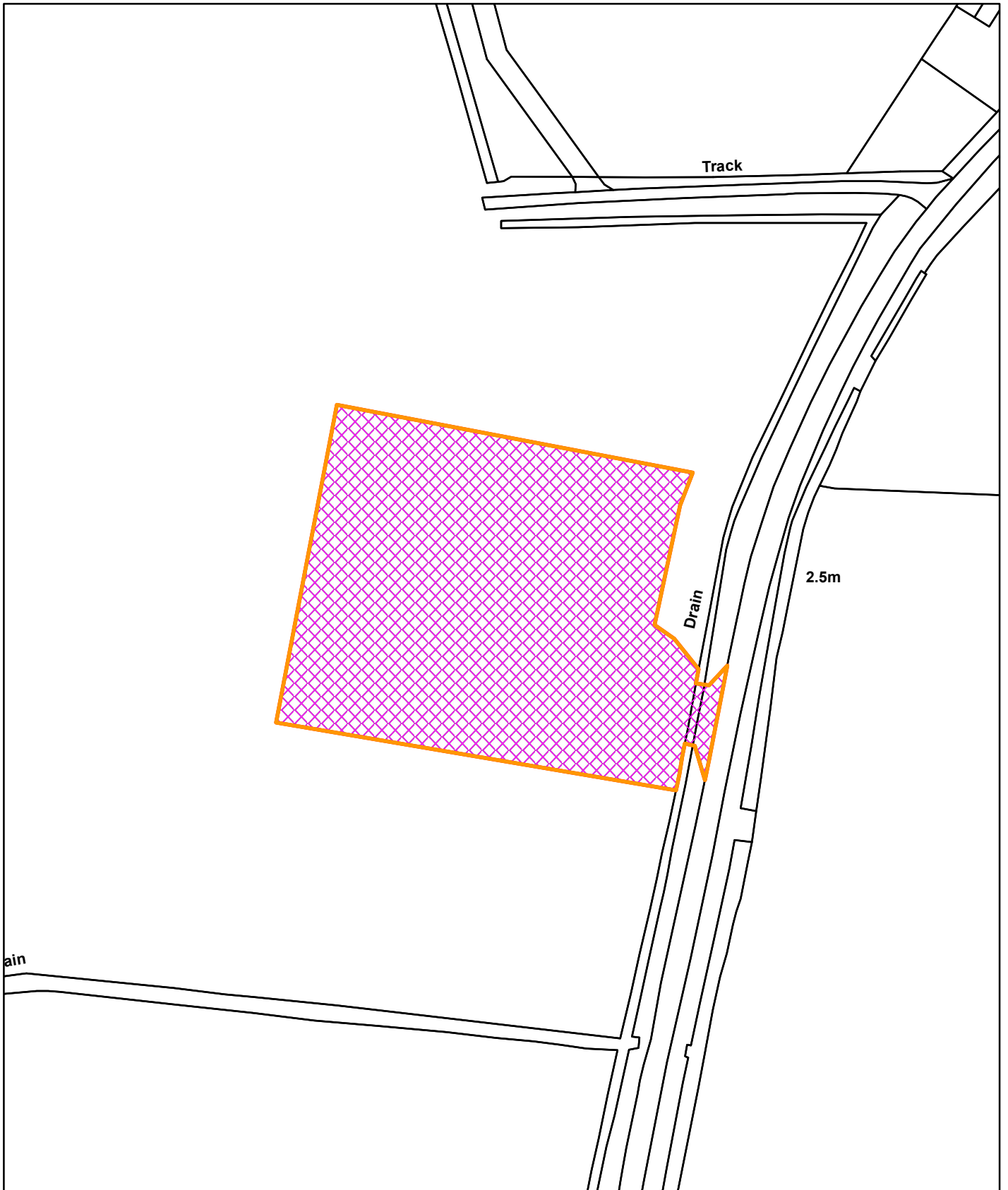
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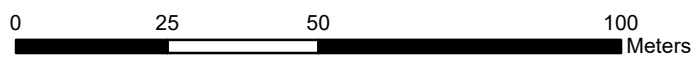


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Parish:	Clenchwarton	
Proposal:	Part retrospective agricultural store	
Location:	Land W of Kenwick Hall And S of Track Station Road Clenchwarton King's Lynn	
Applicant:	Mr Mark Means	
Case No:	22/02214/F (Full Application)	
Case Officer:	Bradley Downes	Date for Determination: 14 March 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – At the request of the Assistant Director

Neighbourhood Plan: No

Case Summary

The proposed development is an agricultural store on a large farm holding. An access point has previously been constructed to the site, under a separate planning permission. The application is part retrospective as foundations have already been constructed, a bund created around the site and hardcore laid down. The site lies in the countryside, on the west side of Station Road, approximately 320m to the north of the junction with the A17.

Key Issues

- Planning history
- Principle of development
- Form and character
- Impact on neighbour amenity
- Highway safety
- Flood risk
- Any other matters requiring consideration prior to determination of the application

Recommendation

APPROVE

THE APPLICATION

The application is for the erection of an agricultural store. The building will be primarily used for the storage of produce including grain and occasionally sugar beet. The application is part retrospective as foundations have already been constructed, a bund created around the site and hardcore laid down. The application arose as a result of an enforcement

Planning Committee
5 June 2023

investigation into construction work being carried out for the foundations that were not in accordance with a previous permission granted under permitted development rights, and it was considered planning permission was therefore required. While an access has also been created over the dyke to the front of the land, this was granted planning permission separately under 21/01725/F.

The site lies in the countryside, on the west side of Station Road, approximately 320m to the north of the junction with the A17. Land immediately to the south currently benefits from prior approval for the erection of a store under Part 6, Class A of Sch 2 of the Town and Country Planning (General Permitted Development) Order 2015 (as amended).

SUPPORTING CASE

This document has been produced to assist the seeking of a Full Planning Approval for an Agricultural Building (Planning Reference 22/02214/F).

The proposed location of the Agricultural Building has been located just off the A17 on my clients Agricultural land to reduce the amount of Heavy Goods Vehicles (HGVs) from travelling through the villages Terrington St Clement and Clenchwarton. The current produce from JS Means is stored at a facility in Wimblington, March which is located approximately 25 miles away. JS Means current clients are located in Stoke Ferry, King's Lynn, Bawsey & Holbeach. Therefore the produce travels from JS Means Farm in Terrington St Clement to Wimblington Stores, and then to various sites.

This would mean that the HGVs would need to travel between 56 - 64 miles (which does not include return trips) to deliver the product locally. With the new location of the Agricultural Building this would reduce this mileage down to between 8 - 18 miles.

The proposed Agricultural Building would also reduce the amount of HGV vehicles going through the village of Terrington St Clement & Clenchwarton by at least 50%. This was the main concern from the neighbouring dwellings that there would be more HGV vehicles travelling down Station Road. The maps and documents which have been submitted with the planning application clearly show that the proposed building will reduce this issue.

Planning has already been approved under a Prior Approval application for a slightly smaller Agricultural Building located further south-west on the same field. My client also has approval to install a new access closer to the A17 junction to allow for safer access due to the current access not having adequate visibility splays.

Overall, my client is attempting to reduce his environmental impact.

PLANNING HISTORY

22/00858/AG: Application Withdrawn: 17/06/22 - Construction of new agricultural building - Land W of Kenwick Hall And S of Track, Station Road, Clenchwarton

21/01732/AG: Consent Not Required: Delegated Decision: 13/12/21 - Agricultural Prior Notification: Steel framed and clad building in modern style of farm buildings - Land Off, Station Road, Clenchwarton

21/01725/F: Application Permitted: Delegated Decision: 08/12/21 - Construction of new agricultural field access - Land Off, Station Road, Clenchwarton

RESPONSE TO CONSULTATION

Parish Council: NO COMMENTS

Highways Authority: NO OBJECTION

We have reviewed the submissions identifying the existing haul routes and storage arrangements plus map showing extent of land within the agricultural unit. Which in summary appears to indicate that the applicant currently hauls crop to their farm (Rhoon Road) where it is then collected by a third party to haul to the 3rd party store in Wimblington. The crop is then said to be distributed from that 3rd party store to customers at locations closer to this application site. We note that a significant percentage of their land farmed is to the south of the A17 at present for which the applicant would naturally use this store to retain crop if approved. We presume that the farmed area to the north of the store could continue in a manner similar to how it does now but would more likely utilise the store proposed given their aim of the development.

It is acknowledged that Station Road is said to form part of the existing haul route to gain access to and from the farm and that is of consideration. However, the applicant could in theory also utilise other routes whereas this store may put more focus on Station Road. In relation to the junction at Station Road with the A17 we note that the junction is said to be utilised currently by the applicant, it offers a right-hand turn facility and there are no reported accidents within the last 3 years. We therefore believe that it would be difficult to substantiate an objection to this proposal in relation to movements at that junction.

With respect to Station Road the carriageway width has been measured at 5.0m between the site and the A17 where the majority of the movements will be concentrated. This width is sufficient to allow an HGV and a car to pass. Two HGVs however should require a width of 5.5m to achieve passing without mounting the verge and on bends may require a small additional strip to accommodate the vehicle swept paths. Having assessed the private point of access that would be utilised by the development I am satisfied that it conforms with the adopted recommended guidance as set out in the Department for Transport Design Manual for Roads and Bridges, and also, Manual for Streets II.

Our experience of stores of this nature is that they tend to be filled during off-peak periods with activity being concentrated over short periods when filled and emptied in association with a crop. However, driving to the site from the A17 on Station Road there is a shallow bend in the road's alignment before the site entrance, over which the ability to see vehicles approaching across the inside of the bend can be affected.

However, what is of material consideration is the level of HGV traffic on Station Road. I observe that the applicant's agent has again reiterated that traffic volumes would not increase. As significant increase in traffic above that which currently exists on Station Road cannot be substantiated given the applicants reassurances and information provided, we would not ultimately recommend an objection to the application on highway safety grounds.

Conditions in relation to the point of access would be the same as those for planning reference 21/01725/F, for which works have started and would therefore be subject to the conditions thereby applied. I also observe that the gate is detailed at a specific position on the plan and would therefore be covered by default of the anticipated plan condition. In relation to the hard standing and servicing area we recommend a condition to ensure the hardstanding is laid prior to use and maintained to ensure permanent availability of parking and turning areas.

CSNN: NO OBJECTION

The application does not include any fixed equipment or machinery but for the avoidance of doubt I am recommending a condition to restrict installation of external plant, such as drying equipment. I am under the impression there would be hardly any grain HGVs going to or from the store using the main part of Station Road and that most HGVs would use the A17. In terms of HGV movements a figure of an average 2-3 HGVs per week is mentioned for a total of approximately 120 per year. Due to the nature of agricultural processes including working irregular hours, it is not reasonable to grant consent with restricted hours of use.

However, it must be acknowledged that there is a need to manage the site responsibly and there are measures that can be taken to ensure that the majority of deliveries, collections and general activities at the site take place during reasonable daytime hours on weekdays. While I have requested a condition to restrict installation of any permanent external plant, this does not cover mobile equipment. Therefore, if mobile equipment is to be used this would also need careful management to ensure the impact on nearby residents is minimised. For these reasons I am also requesting a condition for a noise management plan.

Environment Agency: NO OBJECTION

Strongly recommend that the mitigation measures proposed in the submitted FRA are adhered to.

Internal Drainage Board: NO OBJECTION

Land drainage consent is required.

Environmental Quality: NO OBJECTION

No potential sources of contamination are identified in our records, or in the information provided by the applicant.

REPRESENTATIONS

A total of **29** third party letters have been received. 22 in the first round of consultation including a letter from Cllr Kemp, and 6 in the second round of consultation. Following questions raised in the first round of consultation, matters were clarified such as vehicle numbers, building dimensions and haul routes. To allow the public to view this new information a second consultation was carried out. All of the letters set out **OBJECTIONS** to the application. Salient points raised are presented below.

First consultation:

What is intended to be stored in the building?

Lynn news 10th Nov states vending machine to sell local produce – what local produce will be sold? Will the produce come off the surrounding land or be brought in by farm vehicles? Development will result in increased traffic during construction and when the building is in use.

Increased HGVs would impact neighbours with noise and fumes and cause vibrations.

Building is twice the size of the original application.

First time the barn was rejected because process had not been followed. (referring to AG).

HGVs should not allowed to drive the length of Station Road and should only use the A17 end.

Station road should have a weight limit imposed to stop it being used by HGVs.

Noise that will be created by having a very large agricultural building will be intolerable, especially during summer evenings.

Increased traffic would make it more dangerous for pedestrians as there is no street lighting and people park on the pavement.

The previous application was dealt with inappropriately.

Station Road has potholes and is not suitable for more HGVs or tractors.

Straightness of the road encourages speeding so site access would be unsafe.

Drivers will go through village when A17 is heavy traffic.

HGVs would need to encroach onto wrong side of road when entering and exiting site.

Working hours for construction period should be conditioned.

There was previously discussions about the grain dryer and entrances. It is not stated in this application.

With storage of vehicles there will be a fire risk.

The access point is a sharp blind bend and this might become an accident blackspot.

Grain dryer would create a lot of noise and dust throughout the summer.

Second Consultation:

What are we supposed to read from the routes documents? In one document a route is suggested and highlighted in various colours but there is no key.

Heavy laden vehicles accessing the A17 would be extremely dangerous.

HGVs coming from Ongar Hill will use station road as a short cut to the A17.

Don't have faith in support letters written by paid contractors.

The store will be used on a far larger scale than first thought.

Station Road is not wide enough for the volume and frequency of traffic.

If permission is granted, residents need assurance that this will never be a grain dryer and operating hours should be implemented as HGV's make a lot of noise.

Lyndhurst did not receive a consultation letter.

The size of the building is not as stated in the application form.

Properties south of the site will get hundreds of lorry trips.

Under previous planning application the applicant obtained signatures from local residents to support a grain dryer under false pretences.

Application form states development hasn't started which is untruthful.

Letter from Cllr Kemp dated 4th February:

The application and rationale for it are far from clear. Why was the original permission not built?

Residents of Station Road told me they were canvassed by the applicant in 2021 to approve the facility, without being told it was to be a grain dryer.

The grain dryer was opposed by residents on grounds of noise, dust, safety and risk to the highway.

The applicant also applied for the access on a separate planning application in 2021, without specifying what it was for.

The applicant has said that lorry movements travelling along Station Road will reduce as all lorries would use the A17 for access. How can this be enforced, if at all?

Its possible that in time the building could generate more vehicle movements than the conservative estimate of 2-3 lorries a week.

Lorries travelling along Station Road have caused problems for residents, movement and cracking of their homes. Lorries are particularly egregious in the summer. Residents are fed up with noise and vibrations.

The narrow section of road from the A17 to the bend in Station Road is notoriously narrow with insufficient turning space for cars, let alone HGVs.

Vehicles often encroach on verges and risk entering ditches.

Vehicles exiting the A17 have a propensity to speed and vehicles meeting large lorries turning in and out of the site could be dangerous.

Applicant should consider an entry off the A17 for the lorries and relocate the proposed agricultural store to a safer place.

I oppose this application on grounds of uncertainty and risk to highway safety.

LDF CORE STRATEGY POLICIES

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS10 - The Economy

CS11 - Transport

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

Planning history

Principle of development

Form and character

Impact on neighbour amenity

Highway safety

Flood risk

Any other matters requiring consideration prior to determination of the application

Planning history:

The current part retrospective application for an agricultural store is the latest in a recent string of directly relevant applications. Firstly, a full planning application for the creation of a new access onto Station Road was proposed (21/01725/F). Shortly after, an agricultural prior notification for a grain store was submitted (21/01732/AG). The proposed access was considered on the basis that it would be used as a field access for low levels of traffic. It was approved subject to condition that the access be constructed in accordance with a detailed scheme to be agreed in writing with the LPA. No such scheme ever came forward, however the Local Highway Authority have indicated in their response that the access has been constructed in accordance with adopted standards.

Concern has been raised that the access was approved on the basis of being a field access and that this was underhanded behaviour prior to submitting subsequent applications for buildings on the land. At this time it was not necessary for the applicant to disclose that they intended to build on the land, and both applications were submitted around the same time. The current application for a grain store requires the LPA to consider the increased use of the approved access, and its suitability to cater for the proposed development.

At a similar time to the full application for the access, an agricultural prior notification application was submitted for a grain store on land which was served by the proposed access. This application was made in accordance with the requirements of Schedule 2, Part 6, Class A of the Town and Country Planning (General Permitted Development) Order 2015. Under Part 6, Class A of the GPDO, genuine agricultural enterprises can benefit from permitted development rights to erect buildings on land which are reasonably necessary for the purposes of agriculture.

Part 6, Class A requires the developer to notify the LPA of their intention to develop the land in accordance with the permitted development rights and gives the LPA opportunity to consider whether their prior approval is required as to the siting and design of the building. Concern is raised by third parties about how the agricultural prior notification was dealt with. It is also noted that there was concern at the time of the agricultural prior notification that grain dryers could be used in the building. In such an application, which is essentially permitted development, the LPA has no power to consult with statutory or third party consultees, and cannot consider matters relating to highway safety or residential amenity unless they arise directly as a result of the siting of the building. The siting of the building alone was not considered to give rise any significant impact on residential amenity. While it is acknowledged grain drying equipment could have some impact in terms of noise, no such equipment was proposed to be installed on the building as part of the agricultural prior notification, and the potential for the temporary use of mobile drying equipment is beyond the remit of planning control.

Ultimately it was determined that prior approval was required as to the siting and design of the building, because of the open and undeveloped nature of the existing landscape. This prior approval was subsequently granted under the same application (as it still forms part of the permitted development procedure). It is considered the agricultural prior notification was dealt with appropriately.

Lastly, it was reported that construction work had begun on the site contrary to the approved plan under the prior approval. An enforcement investigation confirmed this to be the case. A second agricultural prior notification application was submitted to regularise the new footprint of the building, but such an application cannot be considered if development has already begun. As the foundations had been commenced, it was not possible to proceed with the second agricultural prior notification and it was promptly withdrawn. Shortly following withdrawal of 22/00858/AG, this full planning application was submitted to regularise the new footprint and grant permission for the construction of a grain store.

Principle of development:

The proposed grain store would be used in association with an established agricultural enterprise for the storage of grown produce and therefore would accord with the requirements of Policy CS06 of the Core Strategy 2011 and DM2 of the SADMPP 2016. While the site lies in the countryside, due to the agricultural use it is considered the principle of development is acceptable.

The prior approval under 21/01732/AG still remains extant and effectively serves as a fallback position. This permission was granted on 13th Dec 2026 and subject to condition that development is complete within 5 years. Therefore, it may be possible for the prior approval to be delivered concurrently with the proposed grain store, however the proposed development also includes landscaping including a bund which would intersect the prior approval. This would effectively restrict the prior approval from being deliverable. As such, it would not be necessary to restrict the implementation of the extant agricultural prior approval, and it would not be reasonable to restrict Part 6, Class A permitted development rights for new buildings on the entire agricultural unit.

Form and Character:

The proposed building will have approximately 882sqm of internal floorspace or 963sqm of footprint when measured from the site plan including the roof overhang. The building approved under the AG had a floorspace of approximately 600sqm. The proposed building is annotated on the submitted plan as being 11.067m high to the ridge with eaves of 7.702m. While this is relatively large, the approved barn under the AG had a height of approximately 9.2m and eaves of 6.4m and therefore the current proposal is not significantly larger than what could already be constructed within the applicant's rights. Furthermore, proposed materials include natural grey colour corrugated sheeting to the walls and roof which is considered appropriate for the location of the building and its intended use.

Proposed landscaping includes a fence and bund around the site, and hedgerow to the front. Full details haven't been provided yet, but these can be secured by condition. Overall, it is considered the proposed building will be in keeping with the appearance of similar agricultural buildings in the countryside, and would not have any significant adverse impact on the character and appearance of the countryside.

Impact on Neighbour Amenity:

The proposed store would be approximately 135m away from the nearest neighbouring property to the north-east, known as Kenwick Hall and approximately 190m away from the nearest neighbouring property to the south-east, known as Lyndhurst. The proposed access point is situated approximately 140m from Kenwick Hall and 160m from Lyndhurst. It is considered the building would be sufficiently far from neighbouring occupiers to avoid any significant overbearing or overshadowing impacts. It is considered the use of the building for storage of produce would not itself generate any significant noise or other disturbance impact such as dust.

One of the main areas of concern with third parties was the potential for noisy grain drying equipment. The applicant has confirmed that the proposed building does not include any permanent grain drying equipment to be installed. The use of any temporary and mobile grain drying equipment cannot be reasonably controlled by planning because it is not development. However, the condition recommended by CSNN for a noise management plan is considered appropriate. A noise management plan will require the applicant to consider the impacts that could arise from use of the building and set out measures to minimise those impacts. Furthermore, matters relating to noise and disturbance can be dealt with by Environmental Health (CSNN) under separate legislation, and therefore any adverse noise could lead to a statutory nuisance investigation.

Part 6, Class A of the GPDO also permits alterations to existing buildings which can include installation of grain drying equipment. To ensure that any grain drying equipment which the applicant may wish to install in the future won't have any significant adverse impact on residents in the surrounding area, it is recommended that any approval of this application is

subject to condition that prior to the installation of any external ventilation or drying equipment, details must be submitted to and agreed in writing with the LPA.

The other main area of concern raised by third parties was in relation to HGV movements to and from the site. Currently, harvested produce is taken by tractor and trailer to a store at Ongar Hill Road, before it is taken by HGVs to the permanent store at Wimblington. From there it is collected by various clients by HGVs. Some of the tractor-trailer traffic currently travels up Station Road to take the grain to the temporary store at Ongar Hill Road. In addition, all of the HGV traffic that delivers produce from the store on Ongar Hill Road to the store at Wimblington currently goes down via Station Road. While it is noted that the HGVs are not forced to take this route and simply do as a matter of convenience, it is considered to still represent an existing impact from which to compare the proposed development.

The proposed building would change the travel arrangements so that instead the produce will be delivered straight from the fields to the new store on Station Road. It is considered this would not significantly affect the number of tractor-trailer movements up and down Station Road. However, HGV movements associated with the operation would appear to reduce significantly. Rather than starting from Ongar Hill Road and driving down through Clenchwarton and Station Road, the HGVs will instead start their journey at the proposed store, avoiding Clenchwarton and the majority of Station Road.

It was considered in discussion with CSNN whether it would be suitable to condition hours of vehicle movements and for vehicles to only turn right when exiting the site. Due to the nature of agriculture and fluctuating demands of the food industry it would not be reasonable to condition hours that vehicles are permitted to use the site. Sometimes the building will be required at late or early hours. The agent has stated a 'worst case scenario' would be 5am to 10pm on certain occasions. It is considered that occasional HGV visits inside these hours (6 a week during peak season), would not have any significant adverse impact on residential amenity. A third party comment has questioned whether construction hours can be conditioned. It is considered due to the distance to neighbouring property and the relatively small scale of the development that it would not be reasonably necessary to condition hours of construction.

Secondly, it is not considered sufficiently necessary or enforceable to condition a 'turn right only' condition. The majority of the HGV traffic will be travelling south and therefore would automatically turn right out of the site. Some other traffic will still need to turn left to access other parts of the farm and the main yard on Ongar Hill Road. Lastly, if the A17 were to shut or the south part of Station Road was otherwise inaccessible, it would be reasonable to expect HGVs to be able to temporarily go north up station road to carry on their business. Some third party responses have also suggested a weight limit or traffic calming measures should be imposed on Station Road. A weight limit or traffic calming would not only impact on the proposed building, but also all other traffic that uses the road. It is considered the proposed development is not going to have a significant enough impact to warrant such mitigation.

It is acknowledged that as a result of the proposed development, more HGV movements will be focussed on the part of Station Road where the application site lies, however it is considered the significant reduction of HGVs travelling up and down the northern half of Station Road would have some benefit in terms of residential amenity. HGVs will still travel past residential properties which are to the south of the site, however as these properties already experience HGV movements from the existing arrangements, it is considered the development would not result in any significant increase in noise or vibrations.

Overall, it is considered the proposed development would not give rise to any significant impacts on residential amenity.

Highway Safety:

Third party comments have raised concern that the condition of Station Road is not suitable for the proposed development, containing potholes, being relatively narrow and being straight which encourages speeding. It is considered these matters are not relevant to the proposed development. It is not the fault of the applicant if other members of the public choose to speed or otherwise drive dangerously. The Local Highway Authority have considered the proposed development and consider the width of the carriageway is sufficient to maintain a satisfactory standard of highway safety. They do not object on the basis that the parking and turning area is maintained in perpetuity.

Another third-party comment has raised concern that HGVs and tractor-trailers would need to cross over to the oncoming side of the road to enter and exit the site. This would still be the case even if the carriageway was widened, and due to the swept path of HGVs and trailed vehicles, is an inevitable outcome. It is considered due to the relatively low number of trips that the more intense use of the existing access would not give rise to any significant highway safety concerns. While road users may rarely need to slow down to enable a vehicle to enter or exit the site, this would not have any significant impact on highway safety.

Lastly, a third party raised concern that the development could increase risk for pedestrians on Station Road. Due to the resulting reduction in HGV movements up and down Station Road to the north of the site, it is considered the proposed development would not have any significant adverse impact on pedestrian safety.

Flood Risk:

The site lies in Flood Zone 3a of the Borough Council SFRA 2018. The proposed development, for an agricultural store, would be classed as 'less vulnerable' development, which is usually considered acceptable. A Flood Risk Assessment has been submitted with the application and the Environment Agency do not object to the application subject to the mitigation set out in the FRA. The only mitigation set out in the FRA is that the building shall be resilient to flooding up to 3.7m (allow water to pass through) without significant damage to the building. As the building would not have any residential occupation it is considered this is sufficient to mitigate the risks of flooding on the site.

It is not considered sufficiently necessary to require surface water drainage details for the proposed development at the planning stage. While the increased built area would result in some surface water run-off, it is considered this can be sufficiently dealt with via building control.

Any other matters requiring consideration prior to determination of the application

Some third party comments have queried what exactly will be stored in the building. The information submitted with this application sets out that grain will comprise the majority of the storage, with some sugar beet and associated machinery at an ancillary level. Another concern was that storage of machinery could give rise to fire risk. It is considered the development would not lead to any significant increase in fire risk, and due to the distance from neighbouring property would not put any other properties at increased risk.

Some questions have been raised about a 'vending machine' that was mentioned in a Lynn News online article from 10th Nov 2022. The article states that the applicant "wants to work even more collaboratively with other farms, with the ambition of having a vending machine selling local produce next to the A17". The applicant and agent have confirmed that this is just an ambition at this stage and acknowledge any such venture would require its own

separate planning application. It is not appropriate to consider this matter under this application.

Third party comments have highlighted an inaccuracy on the application form. Under the question “has the work or change of use already started?” the box “No” is ticked. The applicant has since acknowledged that the foundations (currently buried), constitute a commencement of development and the description has been corrected accordingly. It is not considered necessary to require an amended application form. A third-party comment also questioned whether the dimensions provided in the application form are correct. The site area stated is 6420sqm, and floorspace of the building is stated to be 882sqm. It is considered both of these figures are accurate.

Concern was raised that letters have been sent in by paid contractors in support of the application. These letters have not been treated as supporting comments. Instead, they are considered to constitute part of the information base making up the application submission. Lastly, concern has been raised that Lyndhurst was not sent a consultation letter. It can be confirmed that a letter was posted however it was returned to the council as delivery was attempted on 26th Jan 2023 but the post person could not access the property. Notwithstanding, a site notice was posted adjacent the site which by itself would be sufficient to meet the statutory consultation duty of the Borough Council. The owner of the property is also clearly aware of the planning application.

Cllr Kemp’s letter makes some additional points not raised by other third parties and have not yet been covered in this report. Firstly, it is questioned why the applicant hasn’t implemented their existing AG consent for the smaller barn. This is entirely the applicant’s prerogative; they are under no obligation to implement that permission and are within their rights to make this planning application as an alternative. Concern is also raised that while existing traffic levels might be considered acceptable, what is there to stop traffic from increasing over time? It is considered the scale of the building effectively caps the amount of traffic that would utilise the site. As such it is considered traffic would not be likely to increase significantly or cause further harm in the future.

CONCLUSION

The proposed agricultural barn would be an acceptable form of development in the countryside in principle. The proposed barn would be constructed to appear in keeping with other similar barns in the wider area. It is considered the proposed development would not give rise to any significant increase in traffic along Station Road. While more HGVs would be focussed at the point of access, this is not considered to give rise to any additional material detrimental impacts on residential amenity or highway safety. The concerns of local residents are noted and have been considered above, however it is considered the issues raised are not sufficient to warrant refusal, can be controlled by other legislation, or are otherwise not material planning considerations. Subject to the conditions set out below, it is the recommendation of the Planning Officer that permission is granted for the proposed agricultural store.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans. Dwg nos. PL100A (Location Plan) and PL101 (Proposed Site Plan, Elevations and Floor Plan).

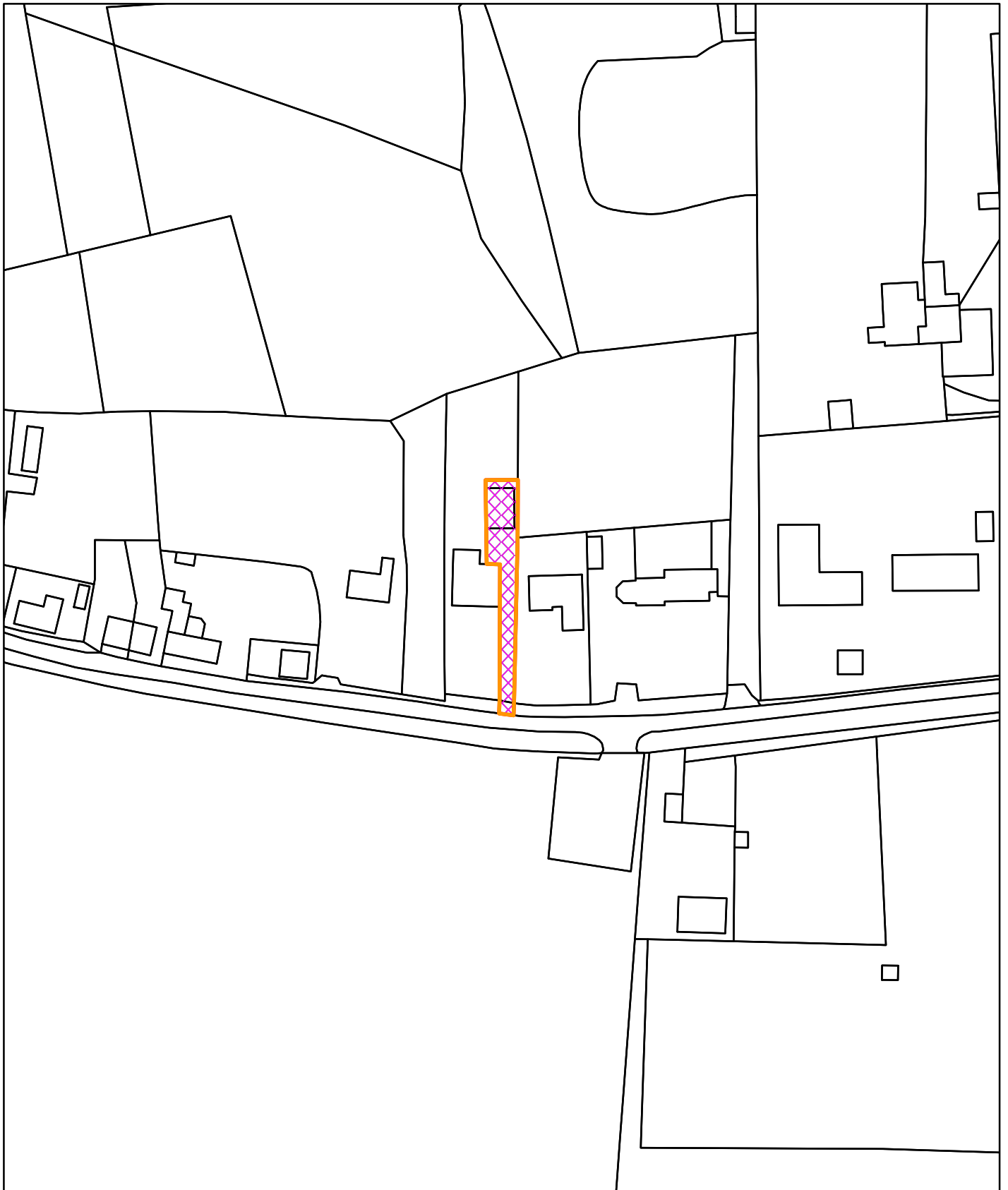
- 1 Reason: For the avoidance of doubt and in the interests of proper planning.
- 2 Condition: Prior to first use of the agricultural building hereby permitted the proposed access / on-site parking / servicing / loading, unloading / turning area shall be laid out, levelled, surfaced and drained in accordance with the approved plan and retained thereafter available for that specific use.
- 2 Reason: To ensure the permanent availability of the parking / manoeuvring area, in the interests of highway safety.
- 3 Condition: Notwithstanding the provisions of Schedule 2, Part 6, Classes A or B of the Town and Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no fans, vents or any other mechanical plant for the purpose of drying, ventilation or extraction shall be allowed to be installed on the building hereby permitted without the granting of specific planning permission.
- 3 Reason: In order that the Local Planning Authority may retain control of development which might be detrimental to the amenities of the locality if otherwise allowed by the mentioned Order.
- 4 Condition: Prior to the first use of the agricultural store hereby permitted, a detailed noise management plan shall be submitted to and approved in writing by the Local Planning Authority. The plan shall be implemented as approved.
- 4 Reason: In the interests of the amenities of the locality in accordance with the principles of the NPPF.
- 5 Condition: The development hereby permitted shall be carried out in accordance with the mitigation measures set out in the submitted Flood Risk Assessment (FRA) by ELLINGHAM CONSULTING LTD, dated January 2023. In particular, the FRA recommends that there shall be flood resilient construction to a level of 3.7m AOD.
- 5 Reason: To ensure the development is resilient to the risks of flooding for its lifetime in accordance with the NPPF and Policy CS08 of the Core Strategy 2011.
- 6 Condition: Prior to the first use of the development hereby approved, full details of both hard and soft landscape works shall have been submitted to and approved in writing by the Local Planning Authority. These details shall include finished levels and contours of the bund, and siting and appearance of the fencing indicated on the approved plan. Soft landscape works shall include planting plans, written specifications (including cultivation and other operations associated with plant and grass establishment) schedules of plants noting species, plant sizes and proposed numbers and densities where appropriate.
- 6 Reason: To ensure that the development is properly landscaped in the interests of the visual amenities of the locality in accordance with the NPPF.
- 7 Condition: All hard and soft landscape works shall be carried out in accordance with the approved details. The works shall be carried out prior to the occupation or use of any part of the development or in accordance with a programme to be agreed in writing with the Local Planning Authority. Any trees or plants that within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species as those originally planted, unless the Local Planning

Authority gives written approval to any variation. The landscaping scheme shall be retained as agreed.

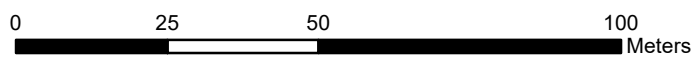
- 7 Reason: To ensure that the work is carried out within a reasonable period in accordance with the NPPF.

23/00271/F

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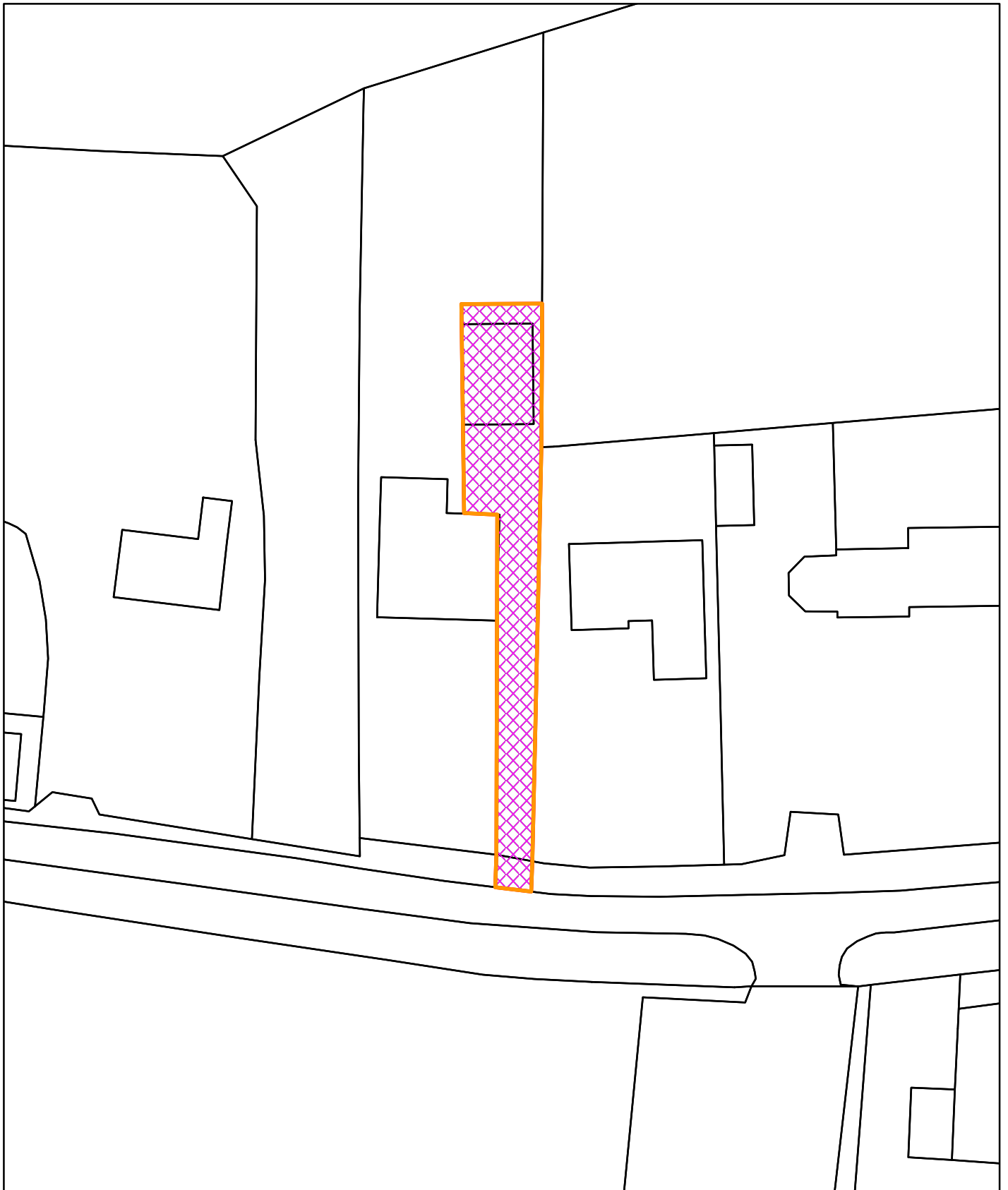
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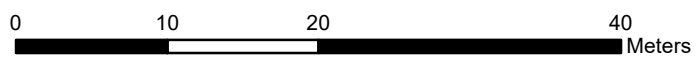


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24/05/2023

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Parish:	Congham	
Proposal:	Change of use from studio to short term holiday let (2 persons)	
Location:	The Lavenders St Andrews Lane Congham KINGS LYNN	
Applicant:	PAUL OLDROYD	
Case No:	23/00271/F (Full Application)	
Case Officer:	Olivia Luckhurst	Date for Determination: 27 April 2023 Extension of Time Expiry Date: 8 June 2023

Reason for Referral to Planning Committee – The recommendation is contrary to that of the Parish Council and the matter was referred to Planning Committee by the Sifting Panel

Neighbourhood Plan: No

Case Summary

The application site is located within Congham which is classified as a Smaller Village and Hamlet Policy CS02 of the Core Strategy 2011. The site is not within a defined settlement boundary and is therefore considered as countryside in policy terms.

The plot is host to one dwelling (The Lavenders) and a garage with a room in the roof which was approved under application 18/00119/RM. The first floor of the garage currently contains a studio. However, the studio space has been used as an annexe containing a kitchen/diner and living room, shower room and one bedroom. The studio was conditioned to be limited to purposes incidental to the needs and personal enjoyment of the occupants of the dwelling.

The proposal seeks permission for the change of use from studio to a short-term holiday let.

Amended plans were submitted showing the neighbouring site's boundary revised, following the receipt of comments from a neighbouring property.

Key Issues

- Principle of Development
- Form and Character
- Impact on Neighbour Amenity
- Highway Safety and Parking
- Other Material Considerations

Recommendation

APPROVE

THE APPLICATION

The application site is located within the rural village of Congham and fronts St Andrews Lane. The site is host to an existing two storey, detached dwelling with a garage with a room above.

The garage at ground floor provides parking spaces and a separate store with a studio/study at first floor. The first floor can be accessed via an external staircase located to the rear of the building (north) and was approved under application 18/00119/RM.

The dwelling and garage have been constructed from red brick and flint with upvc windows and doors. To the front of the plot is a large parking and turning area with the garage accessed to the side of the host dwelling (east).

The site is enclosed by close boarded fencing and has no residential properties located to the rear.

Supporting Statement:

The application site is located within the rural village of Congham and fronts St Andrews Lane. The site is host to an existing two storey, detached dwelling with a garage with a room above.

The garage at ground floor provides parking spaces and a separate store with a studio/study at first floor. The first floor can be accessed via an external staircase located to the rear of the building (north) and was approved under application 18/00119/RM.

The dwelling and garage have been constructed from red brick and flint with upvc windows and doors. To the front of the plot is a large parking and turning area with the garage accessed to the side of the host dwelling (east).

The site is enclosed by close boarded fencing and has no residential properties located to the rear.

PLANNING HISTORY

18/00119/RM - Permitted - Reserved Matters Application: Construction of a dwelling - Delegated Decision

16/02012/O - Permitted - Outline Application: Construction of two dwellings and formation of new access onto St Andrews Lane - Committee Decision

RESPONSE TO CONSULTATION

Parish Council: OBJECT on the grounds that:

The Parish Council object to the application on the grounds that the proposed short-term holiday let would create an additional dwelling therefore generating increased traffic flows on the very narrow single track, St Andrews Lane which is currently over trafficked and the revised location plan red outlined area doesn't have a sufficient turning area to allow a car to leave in forward gear. This will lead to further conflict between pedestrians and vehicles on St Andrews Lane and consequently be detrimental to the safety of residents.

Highways Authority: NO OBJECTION:

Whilst St Andrews Lane is generally considered unsuitable for increased use due to the inadequate widths, for this single letting unit which benefits from an acceptable access, visibility and parking provision, I would find any objection difficult to sustain. I am able to

comment that in relation to highways issues only, as this proposal does not , that Norfolk County Council does not wish to resist the grant of consent.

REPRESENTATIONS

ONE REPRESENTATION received stating that the location plan provided incorrectly showed the extent of their boundary. The representation also objected to the use of the external staircase as this would result in overlooking.

Response:

One representation received stating that the location plan provided incorrectly showed the extent of their boundary. The representation also objected to the use of the external staircase as this would result in overlooking.

LDF CORE STRATEGY POLICIES

CS06 - Development in Rural Areas

CS10 - The Economy

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS08 - Sustainable Development

CS12 - Environmental Assets

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM11 – Touring and Permanent Holiday Sites

DM3 - Development in the Smaller Villages and Hamlets

DM1 – Presumption in Favour of Sustainable Development

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

- Principle of development

- Form and character
- Impact on neighbour amenity
- Highway safety
- Flood risk

Principle of Development:

Planning permission is sought for the change of use from an existing first floor studio to be used as a short term holiday let.

The site lies within the countryside where policies are restrictive, however, the proposal relates to the conversion of an existing building.

Planning Policy supports holiday accommodation in the countryside under Policy DM11 of the SADMPP, in order to support the rural economy, providing such accommodation is situated within sustainable locations, demonstrates high standards of design and would not be detrimental to complies with flood risk policies and would not be detrimental to the countryside, AONB or European designate sites.

It is considered that the proposed holiday use is acceptable in principle on the site as it involves the conversion of an existing ancillary building within the garden curtilage of an existing dwelling, which will provide a public benefit by contributing towards supporting rural tourism and economic growth within this rural area without being detrimental to the landscape.

Holiday let use falls within the same residential Use Class C3 as ancillary accommodation and would be used in a similar manner but not on a permanent basis.

Policy DM11 of the Site Allocations and Development Management Plan 2016 states that proposals for new holiday accommodation sites or units or extension or intensification to existing holiday accommodation will not normally be permitted unless the proposal is supported by a business plan demonstrating how the site will be managed and how it will support tourism or tourist related uses in the area.

The applicant has provided a supporting statement confirming that the site would support the rural economy given its location near the Norfolk Coast, Sandringham and various market towns.

The accommodation would be marketed on various booking sites and is aimed at couples or individuals who are looking for a quiet and relaxing break.

The statement goes onto explain that the site provides a sufficient amount of parking for the host dwelling and guests.

A condition would be added to ensure that the holiday let is tied to the host dwelling so that it cannot be occupied as a separate residential dwelling.

Conditions would also be added to ensure that the accommodation is occupied for holiday purposes only and cannot be occupied for more than 28 days per year. The owners would also be required to maintain an up-to-date register of lettings/occupation and shall make this available at all reasonable times to the Local Planning Authority. Therefore, the proposed development is considered to comply with policy DM11.

The above development is considered to represent sustainable development in accordance with the NPPF.

Overall, the proposed development is considered to comply with policy DM1, DM11 and DM15 of the Site allocations and Development Management Policies Plan 2016 (SADMPP) and policy CS01, CS06, CS08, CS10 and CS12 of the Core Strategy 2011.

Form and Character:

No works or alterations are proposed to the existing building as a result of the change of use and therefore, the character and appearance of the building would remain the same. A small brick wall is proposed in between the main dwelling and the garage to separate the host properties amenity space from the parking and turning area for the proposed holiday let. This addition is considered to be minor and would not be viewable from the street scene. Therefore, the proposed development is considered to comply with Policy DM15 of the SADMPP 2016 and Policy CS06 of the Core Strategy.

Impact on Neighbour Amenity:

The proposed development does not involve the addition or extension of the building and therefore, the proposal would not create any overshadowing or loss of light. The external staircase positioned to the rear of the building is existing and serves a studio/study at first floor. The staircase was included on plans approved under 18/00119/RM and therefore, the principle of the stairs, its location and their use has already been considered acceptable.

Whilst the proposed change of use may slightly intensify the use of the staircase with guests utilising it more often to access their accommodation, this would be limited due to the scale of accommodation provided. The staircase only provides enough space for guests to access the accommodation and does not incorporate a balcony or terrace area. The stairs are positioned approximately 20.9m from the neighbouring properties rear elevation and are located on the centre of the building with rising to the west away from the neighbour. Given the staircase's location, the garage building itself would screen any views into the neighbouring dwelling and the site also offers screening from the existing close boarded, 2m timber fence.

Noise created by users is also considered to be minimal or similar to the existing use due to the scale of accommodation. The one bedroom holiday let would allow for a maximum of two people as only one bedroom is provided. Therefore, noise created from entering and existing the building as well as vehicle movements may be increased slightly however, this not considered to be unacceptable.

Given that the proposed holiday let would only consist of one bedroom, it is not considered to generate an excessive number of movements on the staircase causing a level of overlooking detrimental enough to warrant a refusal.

The access to the holiday let would be positioned adjacent the neighbours access which allows for a separation distance of 2.7m from the fence to the neighbouring dwelling. The west elevation of the neighbouring dwelling does not include any windows and therefore, users of the holiday would not have views into the property.

The site is well screened to the west by mature trees and hedging. The building is positioned 9.8m from the western boundary and therefore, the neighbour to the west would not be impacted by the proposed change of use given the existing screening and separation. Therefore, the proposed development is not considered to have a detrimental impact on residential amenity and complies with DM15 of the SADMPP 2016 and Policy CS06 of the Core Strategy.

Highway Safety:

The proposed holiday let would utilise the existing access off St Andrews Lane and would not impact the existing parking provisions for the main dwelling.

The users of the proposed holiday let would drive down the side (east) of the host property and park in front of the garage building.

The Highway Authority have confirmed that St Andrews Lane is generally considered unsuitable for increased use due to the inadequate widths however, given that the site benefits from an acceptable access, sufficient visibility and parking provisions, they have confirmed they have no objections.

Therefore, the proposal is considered to comply with policy DM15 of the SADMPP 2016 and Policy CS06 of the Core Strategy.

Flood Risk:

The application site is located within Flood Zone 1 and therefore, the site has a low probability of flooding – less than 0.1% annual probability of river or sea flooding

Response to Third Party Comments:

Following the receipt of comments from the neighbouring property, an amended location plan was provided correctly showing the extent of the neighbour's boundary and a reconsultation was issued.

Other matters requiring consideration prior to the determination of this application:

CONCLUSION

In conclusion, the application site is considered to provide and retain a sufficient amount of parking for both the host dwelling and proposed holiday let and would utilise an acceptable access.

No extensions or additions are proposed to the garage and therefore, the proposed use is not considered to create any overshadowing or loss of privacy.

Noise disturbance from vehicle movements and use of the external staircase is considered to be minimal given scale of the accommodation. The site also provides sufficient separation distance from the neighbouring property to the east and screening to the west.

The principle of holiday use is acceptable given that it involves the conversion of an existing building, and the use would contribute towards supporting rural tourism and economic growth within this rural area without being detrimental to the landscape. Therefore, the development is considered to comply with policy DM1, DM11 and DM15 of the Site allocations and Development Management Policies Plan 2016 (SADMPP) and policy CS01, CS06, CS08, CS10 and CS12 of the Core Strategy 2011.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 92 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out using only the following approved plans:

SE-883 21 A FLOOR PLAN PROPOSED Received 27.02.2023
 SE-883 21 A GARAGE LAYOUTS & SECTION Received 27.02.2023
 SE-883 23 BUILDING REGULATION DRAWING SITE & LOCATION PLAN
 Received 26.04.2023
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: The accommodation hereby permitted shall be used as ancillary accommodation or for holiday purposes only, held and operated in connection with The Lavenders, St Andrews Lane, Congham.
- 3 Reason: For the avoidance of doubt to ensure that the building is not used for unrelated purposes that would be incompatible with the provisions of the NPPF 2021.
- 4 Condition: In so far as the holiday let accommodation is concerned, it shall be for short stay accommodation only (no more than 28 days per single let) and shall not be occupied as a person sole or main place of residence.
- 4 Reason: For the avoidance of doubt and to ensure that the development is not used for unrelated purposes that would not be compatible with the NPPF.
- 5 Condition: The owners/operators of the holiday let hereby approved shall maintain an up-to-date register of lettings/occupation and shall make this available at all reasonable times to the Local Planning Authority.
- 5 Reason: In order for the Local Authority to retain control over the development and to ensure that it is not used for unrelated purposes that would not be compatible with the NPPF.

23/00078/F

Land E of Memorial Hall High Street Fincham PE33 9EJ



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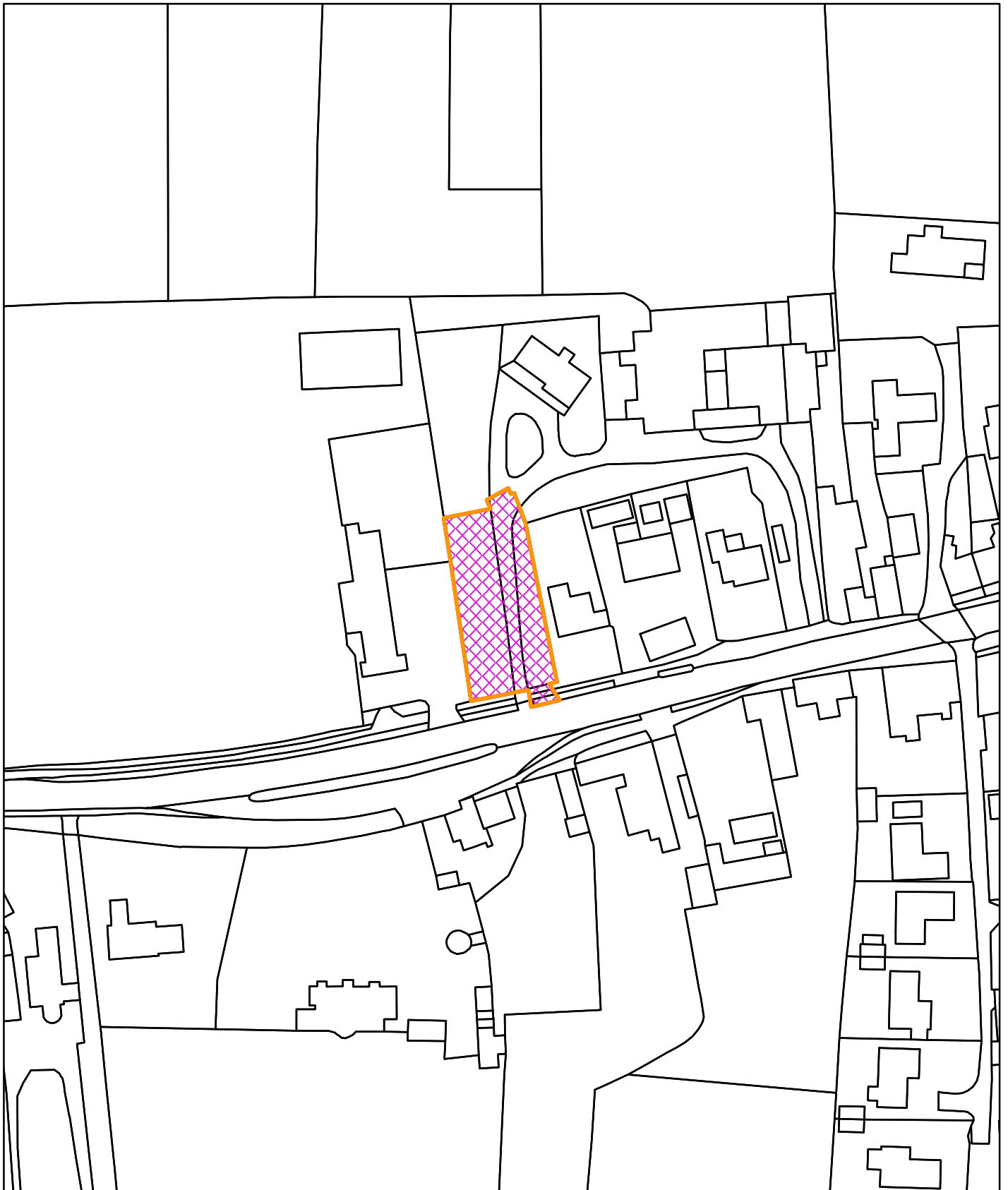
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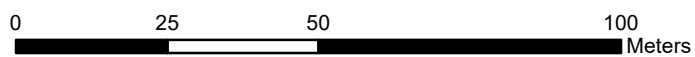


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23/05/2023

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Parish:	Fincham	
Proposal:	Construction of one single storey dwelling	
Location:	Land E of the Memorial Hall High Street Fincham KINGS LYNN	
Applicant:	TB Developments (East Anglia) Ltd TB Developments (East A...	
Case No:	23/00078/F (Full Application)	
Case Officer:	Mrs C Dorgan	Date for Determination: 4 April 2023 Extension of Time Expiry Date: 8 June 2023

Reason for Referral to Planning Committee – Called in by Cllr Howland.

Neighbourhood Plan: No

Case Summary

The application seeks consent for the construction of one single storey dwelling on land east of the Memorial Hall on High Street, Fincham. The site is located north of the High Street (A1122) and adjacent to the car park and access for the Memorial Hall (to the west). Access is via an existing shared access off High Street. The site currently consists of an area of grass with a well-established hedge to the front of the site and low post and rail fencing along the other boundaries.

Fincham is categorised as a Rural Village in the adopted Site Allocations and Development Management Policies Plan. The application site lies within the development boundary for the village (Inset Map G36), and within Fincham Conservation Area.

Key Issues

- Principle of Development
- Form and Character and impact on Heritage Assets
- Neighbour Amenity
- Highways/ Access
- Other Material Considerations

Recommendation:

APPROVE

THE APPLICATION

The application seeks consent for the construction of one single storey dwelling on land east of the Memorial Hall on High Street, Fincham. Access is via an existing shared access off High Street. The site is located north of the High Street (A1122) and adjacent to the car park and access for the Memorial Hall (to the west). It is 0.09ha in size, and currently consists of an area of grass with a well established hedge to the front of the site and low post and rail fencing along the other boundaries.

Fincham is categorised as a Rural Village in the adopted Site Allocations and Development Management Policies Plan. The application site lies within the development boundary for the village (Inset Map G36), and also within Fincham Conservation Area.

The development proposed is a single storey three bedroom dwelling which utilises materials such as local field flints, bricks, pantiles, oak framework etc. The dwelling would be gable end on (north-south axis) to the High Street and would have a ridge height of 6.6m (at the highest point) with an eaves height of 2.7m. The proposed dwelling would be 9.5m wide and 14.5m deep. The dwelling would also have a central chimney.

The dwelling would share an access with recently approved dwellings to the rear, and the site layout provides a front and rear garden for the dwelling with parking to the rear of the garden. The existing hedging along the frontage of the site is to be retained. New hedging is also proposed around the remaining site boundaries as shown on the submitted site plan. In terms of levels, the site falls slightly from north to south by 400mm (between the northern and southern boundaries), and the finished floor level of the proposed dwelling is consistent with the higher level which is acceptable in the street scene.

SUPPORTING CASE

The application site comprises a strip of grazing land and part of the access to Church Farm, the dwelling set well back from the High Street and the Church Farm Barns, recently approved for conversion to two dwellings. To the east of the site is a single storey dwelling, The Kingfisher and to the west is the Village Memorial Hall.

There are several Listed Heritage assets nearby. The houses Shrublands, Australian House and Barsham House are all located on the south side of High Street. It is considered that the proposal, being on the opposite side of the road, located between existing development, will have no adverse effect on the setting of these Listed houses. The houses are already faced by modern bungalows and the garage.

The proposed dwelling, being single storey, will be behind and below the line of vision between the Memorial Hall roof and the Church and it is not considered that the view of the Church from the west will be adversely affected.

The application site is within the Fincham Development Area on both the existing and emerging versions of the Local Plan.

Care has been taken in the design and positioning of the bungalow on the site, to overcome any negative impact on the surrounding area. A new closeboarded screen fence will be provided between the new dwelling and the Memorial Hall car park and play area.

The extended dwelling, Church Farm, to the north, sits at an angle that looks over the play area and playing field, rather than towards the proposed dwelling.

Since the original submission and bearing in mind comments from objectors and the Conservation Officer, the dwelling has been re-designed and set back further in the site, beyond the front line of the Memorial Hall. The Conservation Officer now has no objections to the scheme.

Regarding other comments made, it is suggested that the 'open and rural' aspect in this area really starts with the playing field, to the west of the Memorial Hall and the application site is really only infill.

Surface water drainage will be dealt with at the detail stage. There is no reason why adequate soakaway drainage won't work here.

Regarding noise from the bottle and clothes banks, it is suggested that this pales into insignificance compared with the noise from the aircraft at RAF Marham, which all residents of Fincham accept and get used to.

The design change is to create a 'rural' looking building which will enhance the Conservation Area. The roof pitch has been increased and flint gables added, together with a 'catslide', oak framed, side overhang forming a covered porch to add relief and interest. The bricks have been changed to match those approved for the new cottage, further to the east.

It is suggested that the proposal will now create an interesting and pleasant approach to the development at the rear and will enhance the character and appearance of the area.

PLANNING HISTORY

22/01585/F: Application Permitted (Delegated decision): 19/12/22 - Phased development of - Phase 1 - demolition works to remove 2 pole barns covering former cattle yards and partially collapsed parts of barns and outbuildings and clearance of debris from the site in order to carry out contamination and ecology surveys - Phase 2 - conversion of barns complex to form two new dwellings - Church Farm

22/01584/F: Application Permitted (Delegated decision): 28/11/22 - Extension and alterations to existing dwelling including new roof incorporating first floor accommodation and construction of a garage - Church Farm

21/00849/F: Application Permitted (Delegated decision): 02/11/21 - Conversion of barns complex to form three dwellings - Church Farm Barns

17/00745/F: Application Refused (Delegated decision): 06/10/17 - Proposed construction of two dwellings and pair of courtyard carports/cycle stores following demolition of existing sub standard structures - Church Farm Barns

06/00097/CU: Application Refused (Delegated decision): 03/05/06 - Conversion of barns to 3 dwellings - Church Farm

RESPONSE TO CONSULTATION

Parish Council: NO OBSERVATIONS.

Highways Authority: NO OBJECTION subject to condition.

No objection to the proposed alterations. The proposed access, parking and turning would accord with the adopted standards. The access is also to be widened as part of this application and as a result recommends conditions are attached re the access, and car parking and turning areas.

Internal Drainage Board: NO COMMENTS.

Environmental Health & Housing – Environmental Quality: NO OBJECTION.

The applicant has provided a site plan illustration the proposed development and a screening assessment assisting no known contamination. We have reviewed our files and the site is describes ad grazing land and is not seen developed for the duration of our records. The surrounding landscape is largely residential and agricultural. No potential sources of contamination are identified in our records, or in the information provided by the applicant. We have no objection regarding contaminated land.

Historic Environment Service (NCC): NO OBJECTION subject to conditions.

The proposed development lies within the historic core of the village of Fincham, close to the medieval parish church to the east and to the site of the medieval Baynard Hall to the west. Another medieval church was located a short distance to the southwest, but this was demolished in the 18th century. Within the village there is evidence of medieval and Roman settlement, while on the western edge of the village, a significant Iron Age settlement has been discovered and partially excavated. Consequently, there is potential that heritage assets with archaeological interest (buried archaeological remains) may be present at the site and that their significance will be affected by the proposed development.

If planning permission is granted, we therefore ask that this be subject to a programme of archaeological mitigatory work in accordance with National Planning Policy Framework. Ministry of Housing, Communities and Local Government (2021). para. 205, and controlled via condition.

Community Safety and Neighbour Nuisance (CSNN): NO OBJECTION.

I have looked at this application and am concerned that it is likely that future residents of this property will experience noise issues from the village hall e.g. from music, cars, people's voices, and the bottle banks. The proposed fence may reduce this noise but may not necessarily bring it down to a level where the occupant of the house does not feel affected by it. It is difficult to set conditions that could be sure to sufficiently protect the residents from noise, or to protect the village hall committee from receiving valid complaints. A different fence (with additional lining) may address bass music levels – an acoustician could advise the applicant on the best options. Enhanced glazing on the property could also help, but in the summer time it would be reasonable for the occupants to want their windows open, or to sit out in the garden. Reconfiguration of the property might be possible, so that the bedrooms and living room are on the far side of the property, but again could not be fully sure that this would avoid noise complaints.

There have been no noise complaints about the village hall. When we get complaints about noise coming from village halls the main control mechanisms are management measures from the village hall committees etc. or things such as noise limiters, rather than external improvements around the neighbouring properties.

Conservation Officer: NO OBJECTION.

We have reviewed the plans and can confirm that the amendments have moved the scheme backwards and altered the hipped roof which have made it less dominant within the street scene. We therefore have no objections to the proposed scheme. Please condition materials including a sample panel, vents ducts and flues and joinery details as well as hard and soft landscaping.

Natural England: NO OBJECTION.

It has been identified that this development falls within the 'Zone of Influence' (Zol) for one or more of the European designated sites scoped into the Norfolk Green Infrastructure and Recreational disturbance Avoidance and Mitigation Strategy ('GIRAMS'). It is anticipated that certain types of new development (including new tourist accommodation) in this area is 'likely to have a significant effect' on the sensitive interest features of these European designated sites, through increased recreational pressure when considered either alone or 'in combination' with other plans and projects.

The GIRAMS has been put in place to ensure that this additional recreational pressure does not lead to an adverse effect on European designated sites in Norfolk. The strategy allows effective mitigation to be implemented at a strategic level, so that the relevant councils, Natural England and other stakeholders are able to work together to provide the best outcomes for the designated sites. It also has the benefit of streamlining the process, so reducing the amount of time taken to process individual planning applications for the councils and Natural England.

Natural England advise that a suitable contribution to the Norfolk GIRAMS should be sought from this development to ensure that the delivery of the GIRAMS remains viable.

Natural England's advice is that this proposed development, and the application of these measures to avoid or reduce the likely harmful effects from it, will need to be formally checked and confirmed by your Authority, as the competent authority, via an appropriate assessment in view of the European Site's conservation objectives and in accordance with the Conservation of Habitats & Species Regulations 2017 (as amended).

Natural England advises that it is a matter for your Authority to decide whether an appropriate assessment of this proposal is necessary in light of this ruling. In accordance with the Conservation of Habitats & Species Regulations 2017 (as amended), Natural England must be consulted on any appropriate assessment your Authority may decide to make or the decision recorded as per an agreed approach.

REPRESENTATIONS:

THREE letters of **OBJECTION** have been received and are summarised below -

- Query the demolition of the barns to the rear of the application site.
- The proposed bungalow would sit several metres in front of the existing built line and much closer to the High Street than neighbouring buildings. This would impact views within the conservation area from the High Street, the Memorial Hall, Barsham House, and from the Cottages opposite.
- The proposal would detract from the Conservation Area. It would damage the rural and open aspect and character of the village and be urbanising one of the only road-facing pasture sites in the centre of the village - as well as eroding the amenity, privacy, and character of the area.
- The positive benefit to the street scene of the green gap between the Memorial Hall and Kingfisher Bungalow should be considered against planning policy DM3.

- The proposal may further adversely affect the setting of the listed houses on the south side of the High Street, as well as views of the Church from the West, from the Memorial Hall car park, the playing fields, and the play area, disrupting the historic/visual links between these community facilities.
- The repositioning to the east of a 6-metre-wide access roadway will need careful reconsideration in terms of impact on drainage, highways, and on the amenity of neighbouring dwellings. There is no specification shown regarding the surface material or drainage proposals. The High Street is already prone to surface water flooding in times of heavy rainfall. Any repositioning of the potentially busy driveway would impact on bedroom windows opposite and privacy of adjacent dwellings. More of the rural character of the conservation area would be lost.
- S.72 of Planning (Listed Buildings and Conservation Areas) Act 1990 requires that Local Planning Authorities in making their decision on an application for development in a conservation area pay special attention to the desirability of preserving or enhancing the character or appearance of that area. The dwelling does not represent traditional dwellings seen in the conservation area and do not reflect the local character.
- The location of the proposed bungalow, with bedrooms literally a few feet from the bottle recycling bin and clothes bank is potentially problematic. The Memorial Hall itself is very well used and is often very noisy, even late into the evening, especially when live music, discos, or even exercise classes take place. Similarly, the children's play area would sit adjacent to the rear of the proposed bungalow and its garden, with implications both ways in terms of the public/private interface.
- It would also have an adverse effect on the residential amenity (overlooking and loss of privacy) of near neighbours.

LDF CORE STRATEGY POLICIES

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS11 – Transport

CS12 - Environmental Assets

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

DM17 - Parking Provision in New Development

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

Principle of Development
Form and Character and impact on Heritage Assets
Impact on Neighbour Amenity
Highways/ Access
Other Material Considerations

Principle of Development:

The application site lies centrally within the village of Fincham as designated on Inset Map G36 of the Site Allocations and Development Management Policies Plan. Fincham is categorised as a Rural Village in the adopted Local Plan. As such development is permitted under DM2 of the Site Allocations and Development Management Policies Plan subject to this being in accordance with the other policies in the adopted Local Plan.

Therefore, the principle of development in this location is acceptable and in accordance with the NPPF, and policies CS06 of the Core Strategy and DM2 of the Site Allocations and Development Management Policies Plan.

Form and Character and impact on Heritage Assets:

The application site is located to the north of the High Street at Fincham. It is positioned between the Memorial Hall and car park to the west, and residential dwellings to the east and south. To the north of the site is countryside. The character of Fincham is generally ribbon development fronting onto the High Street, with a range of dwelling types/ designs. There has been a recent residential consent to the east of the site, immediately east of High Field, which is a similar plot and footprint to that proposed.

The site is within Fincham Conservation Area. The Conservation Area Character Statement identifies Fincham as an 'agricultural village with many buildings of the 18th and 19th centuries. Most of the buildings are cottages or small houses, many with outbuildings behind (some of which have been converted to houses) and access lanes to these areas. Many face the road and most of the older buildings are built right up to the street edge. Some are gable end on to the road and face a small courtyard or open area with agricultural buildings and outhouses. The majority of buildings are two storey and there is no impression of great height. Views are hard to come by because of the linear nature of the village although slight bends in the road do afford some views. The main view available is of St Martin's Church, on an incline, which dominates the eastern part of the village.'

To the north east of the application site is a collection of farm buildings, in flint patched with bricks, pantile roofs and timber doors. These are called Church Farm and have recently been given consent for conversion to dwellings, and this work is underway. To the east of the site is a modern bungalow and then the 1930s garage, with clear views of the church tower beyond. To the south east of the site is a substantial Grade II Listed building 'Barsham House' with a gault brick façade and a portico porch. The house has railings at the front and is well protected to the west by a full height wall in the same material.

The proposed development was amended during the consultation process to push the dwelling further back into the plot. It is now approximately 11m from the public highway. This brought the dwelling in line with the hall to the west and existing dwelling to the east. The relocation of the dwelling and the amended roofline has made it less dominant within the street scene and preserves the sense of openness between the dwellings. It also protects the views of the church tower as you travel east along the High Street.

Amendments to the scheme also included the materials proposed. The dwelling is proposed to be constructed of random field flints on the front elevations with brick quoin detailing and

timber framework features with an oak framed porch. Aside from flint, the dwelling would also be constructed using Vandersanden Flemish Antique bricks and the roof tiles would be clay pantiles. The Character Statement reinforces the use of flint within the village, and pantiles as the most common roof covering within the conservation area. Whilst timber framing is rare in Fincham the dwelling has been designed to also relate to Church Farm to the northeast. The materials proposed are considered acceptable subject to conditions including provision of a sample panel, vents ducts and flues and joinery details as well as hard and soft landscaping.

Objections to the scheme, from a neighbouring resident, raised concerns about the fact that the proposal would detract from the conservation area, damaging the rural and open aspect and character of the village. Also, that the proposal would adversely affect the setting of the Listed Buildings on the south side of the High Street, and views of the Church from the west, disrupting the historic/visual links between these community facilities. Finally, that S.72 of Planning (Listed Buildings and Conservation Areas) Act 1990 requires that Local Planning Authorities in making their decision on an application for development in a conservation area pay special attention to the desirability of preserving or enhancing the character or appearance of that area. Their view is that the dwelling does not represent traditional dwellings seen in the conservation area and do not reflect the local character.

However, the revisions to the scheme in terms of positioning, dwelling design and materials has addressed many of these concerns and the Conservation Officer raises no objections to the revised scheme. The proposal does utilise local building materials and is of a low scale in line with existing development. It is suggested that the proposal does not detract from the character or appearance of the area. The site is also sufficiently detached from the Listed Buildings to the south (including Barsham House) due to the distance and spacing between, the linear nature of the village and the busy High Street running between the two. The Conservation Officer has no objection to the development in terms of its impact on Listed Buildings.

An objection to the scheme highlights the positive benefit to the street scene of this green gap between the Memorial Hall and Kingfisher bungalow. They argue that this should be considered against planning policy DM3. However, DM3 relates purely to Smaller Villages and Hamlets, of which Fincham is not. While this was an area of green space, this is a limited size and you have clear views of the existing dwelling and play equipment to the rear in the context of the hall and existing dwellings along the frontage. There is also a large area of green open space to the west of the Memorial Hall. Again, the Conservation Officer does not consider this represents a gap of importance to the conservation area. Furthermore, while this gap would be lost the existing well established hedge along the frontage is to be retained and with the dwelling set back this would have a limited impact.

Overall, the proposed development would be in accordance with the NPPF, Policies CS06 and CS12 of the Core Strategy and DM15 of the Site Allocations and Development Management Policies Plan.

Impact on Neighbour Amenity:

Objections to the scheme raise concerns that the proposed driveway would impact upon bedroom windows opposite, by a loss of privacy/ overlooking. In terms of overlooking/ loss of privacy for neighbours as a result of the development; the dwelling proposed is single storey and as such does not propose any windows that would result in overlooking to neighbouring dwellings. The neighbour raises concerns that the use of this access would impact on bedroom windows opposite. However, this is an existing shared access on the other side of a well used public highway, and so the use of this access by an additional dwelling would not give rise to an unacceptable impact as a result.

The existing dwelling to the north east of the site known as Church Farm is currently single storey so would have an acceptable relationship to the scheme. This site has planning consent for the redevelopment of the site for a two storey dwelling, however the first floor windows are orientated so that they would face southwest and in excess of 42m in distance away from the proposed rear garden, and as a result the relationship between the approved dwelling and that proposed is also acceptable.

Objections received do raise concerns regarding the relationship with the Memorial Hall to the west. It states that the Hall itself does generate noise in the evening when there is live music/ discos and exercise classes, and recycling bottle banks are utilised. There is also children's play equipment adjacent to the rear garden of the dwelling proposed.

Whilst the proposed dwelling would be closer to the Hall than existing dwellings there are a number of existing dwellings within close proximity to the facility. The Hall is licensed to hold events 12pm to 12am Monday to Saturday and 12pm to 11pm on Sunday for live music/ recorded music/ performance of a play/ dance/ Wrestling or boxing match/ exhibition of a film and indoor sporting events. There have been no complaints received to date regarding the operation and use of the Hall.

With the sale of alcohol Monday to Sunday until 11pm. CSNN has considered the application and are of the view that they cannot object to the development. That said they do raise concerns about potential noise and disturbance from the Hall to the future residents of the proposed dwelling, but state that they cannot apply conditions to address this. Whilst an acoustic fence may lessen the bass levels it would not eliminate potential noise complaints. The CSNN officer advises that if noise complaints are received for a village hall they would work with the managers to address these.

Whilst it could be argued that the development would be buyer beware, the NPPF in para 187 states that 'Existing businesses and facilities should not have unreasonable restrictions placed on them as a result of development permitted after they were established. Where the operation of an existing business or community facility could have a significant adverse effect on new development ... the applicant (or 'agent of change') should be required to provide suitable mitigation before the development has been completed.' Given the CSNN officer has not objected to the scheme, on balance the potential impact of the hall on the proposed residents is considered acceptable. If the use/ management of the hall changes which resulted in neighbour amenity issues then any impact would have to be assessed and dealt with by our CSNN department as a statutory nuisance issue. However it is notable that there have been no noise complaints from nearby residents.

In summary, the development proposed would not result in unacceptable impacts on the amenity of residential neighbours. Whilst the Hall may result in some noise and disturbance to the occupier of the proposed dwelling, CSNN has not objected to the scheme and on balance is considered to be in accordance with the NPPF, policy CS08 (of the Core Strategy) and policy DM15 of the Site Allocations and Development Management Policies Plan.

Highways/ Access:

The application includes the use of an existing access but this will be widened from 3.5m to 6m. The Local Highway Authority has no objections to the scheme subject to conditions to secure the wider access, and the parking and turning areas as shown on the submitted plans. The development is in accordance with the NPPF, Core Strategy policy CS11 and Site Allocations and Development Management Policies Plan policies DM15 and DM17.

Other Material Considerations:

Drainage / Flood risk – The application site lies within flood zone 1 and as such is at the lowest risk of flooding. In terms of drainage, details have been provided on the submitted plans however given this is for a single dwelling drainage will be addressed by Building Control.

Archaeology – The Historic Environment Service has stated that there is potential that heritage assets with archaeological interest (buried archaeological remains) may be present at the site and that their significance will be affected by the proposed development. Therefore any consent should be subject to a programme of archaeological mitigatory work in accordance with National Planning Policy Framework, and conditions are attached accordingly.

Ecology – The application site falls within The Wash, Brecklands, Norfolk Valley Fens and North Coast Zones of Influence (Zoi). The GiRAMS Fee is required to be paid in order to mitigate against potential impacts from recreational pressure as a result of the site's location in the Zone of Influence for protected sites. However, the site is some distance away from these sensitive sites and is of a nature and scale that there are no significant additional implications. An appropriate assessment has taken place separately to assess the suitability of this mitigation measure and it is considered that development can be granted subject to the GIRAMS fee (£185.93) which has already been paid in full by the applicant.

CONCLUSION

The application seeks consent for the construction of a single detached dwelling on land within the built settlement of Fincham. The principle of development is acceptable in this location.

The site also lies within Fincham Conservation Area. In terms of the form and character of the development proposed, the dwelling is sited and of a scale appropriate to the locality. The site layout, materials proposed and boundary treatments are also in keeping with the street scene and the wider conservation area.

The proposed dwelling would not, as a result of the development, give rise to unacceptable impacts on neighbour amenity, by virtue of a loss of privacy/ overlooking. The amenity of the potential residents has also been considered given the proximity of the development to the adjacent village hall. However, the CSNN officer has not objected to the scheme and notwithstanding the 'agent of change' principle in the NPPF, on balance it is suggested that the proposed development is acceptable.

There are no statutory objections to the scheme, and subject to the conditions proposed, the application is considered to be in accordance with the NPPF, and policies CS06, CS08, CS11 and CS12 of the Core Strategy and DM2, DM15 and DM17 of the Site Allocations and Development Management Policies Plan. Members are duly recommended to approve the application for the reasons given above.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

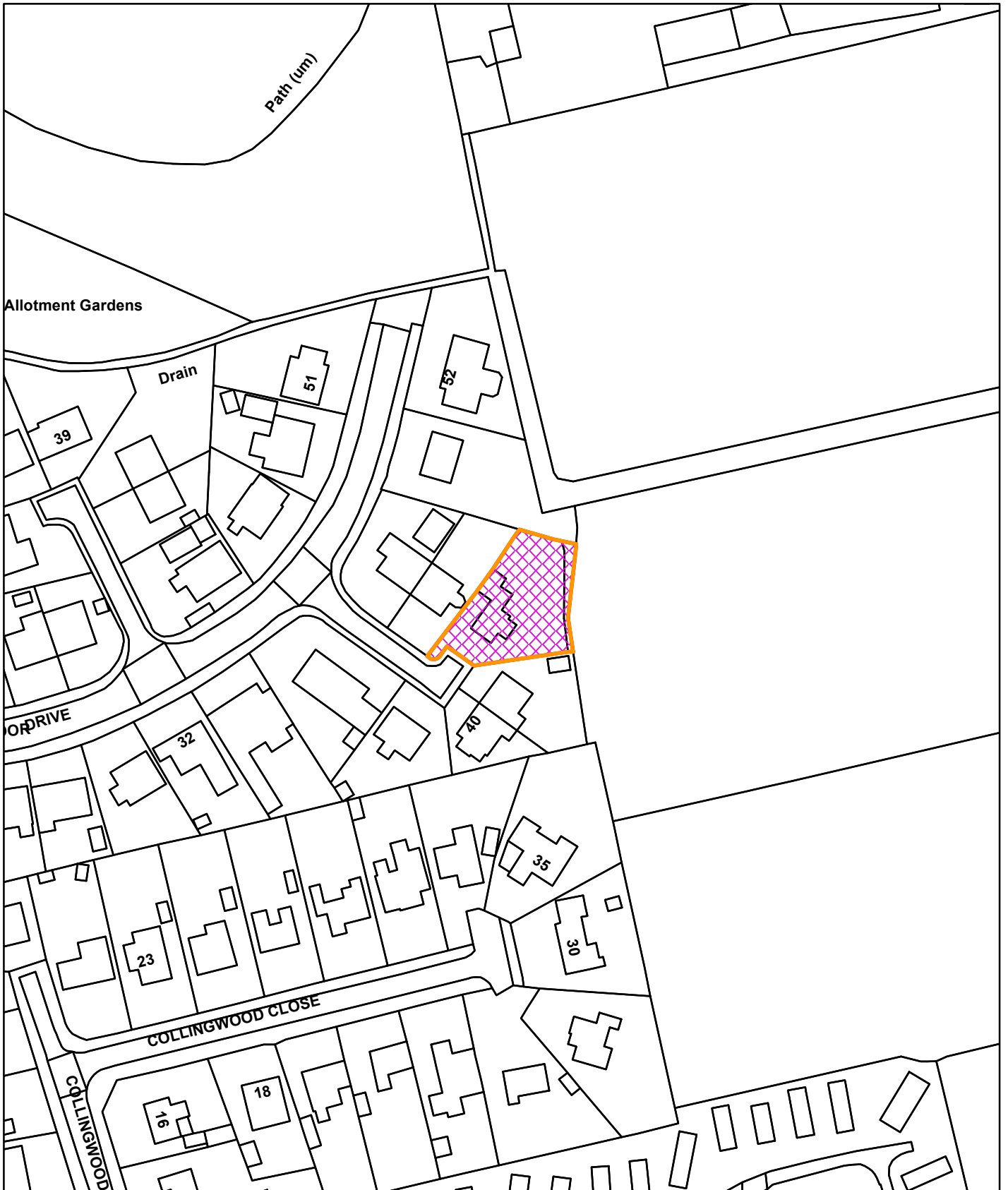
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans Drawing Nos 14463A Proposed Site and Location Plan and 14464A Proposed Layout (received 23 Mar 23).
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: Prior to the commencement of the use hereby permitted the vehicular access indicated for improvement on Drawing No. 14463A shall be widened to a minimum width of 6 metres in accordance with the Norfolk County Council residential access construction specification TRAD3. Arrangement shall be made for surface water drainage to be intercepted and disposed of separately so that it does not discharge from or onto the highway carriageway.
- 3 Reason: To ensure construction of a satisfactory access and to avoid carriage of extraneous material or surface water from or onto the highway in the interests of highway safety and traffic movement.
- 4 Condition: Prior to the first occupation of the development hereby permitted the proposed access / on-site car parking / turning areas shall be laid out, levelled, surfaced and drained in accordance with the approved plan and retained thereafter available for that specific use.
- 4 Reason: To ensure the permanent availability of the parking/manoeuvring areas, in the interests of satisfactory development and highway safety.
- 5 Condition: No development shall take place until an archaeological written scheme of investigation has been submitted to and approved by the local planning authority in writing. The scheme shall include an assessment of significance and research questions; and 1) The programme and methodology of site investigation and recording, 2) The programme for post investigation assessment, 3) Provision to be made for analysis of the site investigation and recording, 4) Provision to be made for publication and dissemination of the analysis and records of the site investigation, 5) Provision to be made for archive deposition of the analysis and records of the site investigation and 6) Nomination of a competent person or persons/organization to undertake the works set out within the written scheme of investigation.
- 5 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF. This needs to be a pre-commencement condition given the potential impact upon archaeological assets during groundworks/construction.
- 6 Condition: No development shall take place other than in accordance with the written scheme of investigation approved under condition 5.
- 6 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.
- 7 Condition: The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the archaeological written scheme of investigation approved under condition 5 and the provision to be made for analysis, publication and dissemination of results and archive deposition has been secured.

- 7 Reason: To safeguard archaeological interests in accordance with the principles of the NPPF.
- 8 Condition: All hard and soft landscape works shall be carried out in accordance with the approved details. The works shall be carried out prior to the occupation or use of any part of the development or in accordance with a programme to be agreed in writing with the Local Planning Authority. Any trees or plants that within a period of 5 years from the completion of the development die, are removed or become seriously damaged or diseased, shall be replaced in the next planting season with others of similar size and species as those originally planted, unless the Local Planning Authority gives written approval to any variation.
- 8 Reason: To ensure that the work is carried out within a reasonable period in accordance with the NPPF.
- 9 Condition: The boundary treatments shall be completed before the dwelling is occupied or in accordance with a timetable to be approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and retained as such in perpetuity.
- 9 Reason: To ensure that the development is compatible with the amenities of the locality in accordance with the NPPF.
- 10 Condition: The existing hedge along the southern boundary of the site, shown as being retained on Drawing No 14463 A, shall not be felled, uprooted, willfully damaged or destroyed, cut back in any way or removed without the prior written approval of the Local Planning Authority. If the hedge is removed without such approval or dies or becomes severely damaged or seriously diseased within 5 years from the completion of the development hereby permitted it shall be replaced with hedge plants of a similar size and species in the next available planting season, unless the Local Planning Authority gives written approval to any variation.
- 10 Reason: To ensure that the development is compatible with the amenities of the locality in accordance with the NPPF.
- 11 Condition: The brick/stone to be used for the external surfaces of the building hereby approved shall be constructed in accordance with a sample panel, prepared on site for the inspection and written approval of the Local Planning Authority. The sample panel shall measure at least 1 metre x 1 metre using the proposed materials, mortar type, bond, and pointing technique to be used in the approved scheme. The development shall be implemented in accordance with the details agreed in writing by the Local Planning Authority.
- 11 Reason: To ensure that the materials are appropriate in the Conservation Area in accordance with the principles of the NPPF.
- 12 Condition: Full details of all extractor vents, heater flues and meter boxes including their design and location shall be submitted to and approved in writing by the Local Planning Authority prior to installation. Installation shall be carried out in accordance with the approved details.
- 12 Reason: To ensure that the materials are appropriate in the Conservation Area in accordance with the principles of the NPPF.

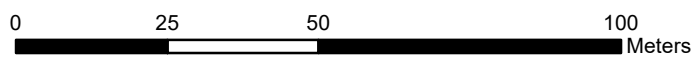
- 13 Condition: No development shall take place above foundation level until 1:20 drawings of all new windows shall be submitted to and approved in writing by the Local Planning Authority. The plans shall provide for the use of timber single glazed windows, puttied and not beaded and shall include joinery details, cross-sections and the opening arrangements. The development shall be implemented in accordance with the approved details.
- 13 Reason: To ensure that the materials are appropriate in the Conservation Area in accordance with the principles of the NPPF.

23/00273/F

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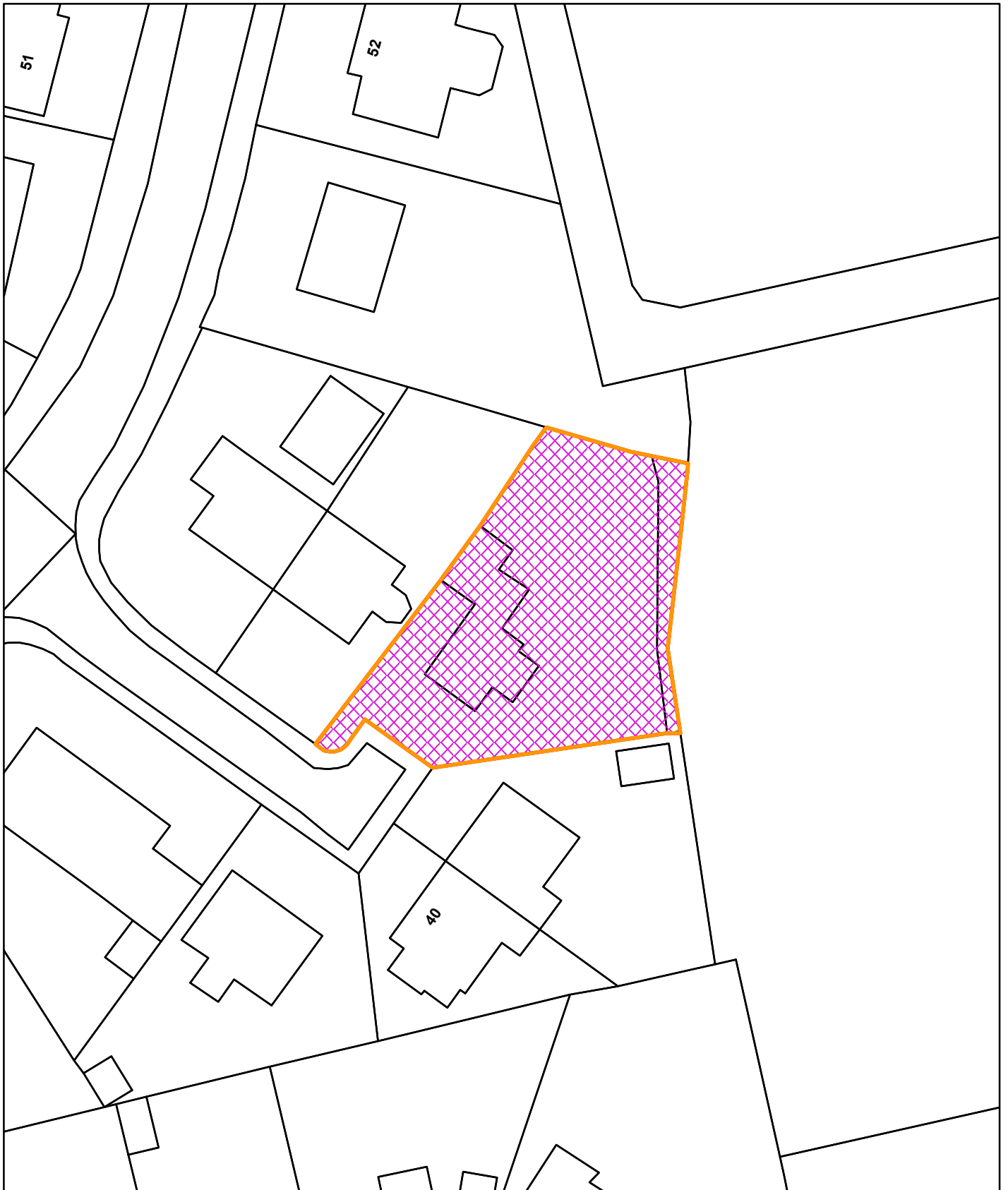
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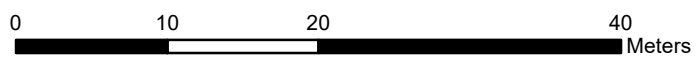


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Parish:	Heacham	
Proposal:	Rear extension and garage conversion	
Location:	44 South Moor Drive Heacham Norfolk PE31 7BW	
Applicant:	Harry Leak	
Case No:	23/00273/F (Full Application)	
Case Officer:	Mrs Rebecca Bush	Date for Determination: 10 April 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Called in by Cllr Parish

Neighbourhood Plan: No

Case Summary

A rear single storey extension and garage conversion to create a new master bedroom and ensuite with additional alterations to the northeast of the application site.

Key Issues

Principle of Development
Impact on Neighbour Amenity
Parking Issues
Any other material considerations

Recommendation

APPROVE

THE APPLICATION

This is a single storey dwelling situated within the development boundary of Heacham. The application is for a rear extension and conversion of the garage to incorporate a proposed master bedroom with ensuite and additional alterations to dwelling.

SUPPORTING CASE

- Refer to the new drawing no LEAK PA 1.1 (proposed block plan) received 15.05.23.

Planning Committee
5 June 2023

- Correct arrangement would make use of the existing dropped kerbs to allow for all 3 vehicles to move on and off the drive freely without the need to move another vehicle.
- Adequate space so the cars do not encroach onto the pavement once parked and passengers can exit the vehicles with ease.
- No 42 has a similar driveway directly adjacent so the proposal will be in keeping with the existing street scene.

PLANNING HISTORY

No planning history for this site.

RESPONSE TO CONSULTATION

Parish Council:

Heacham Parish Council have no observations for this application.

Highways Authority:

The proposal does not affect the current traffic patterns or the free flow of traffic, that they do not wish to resist the grant of consent.

REPRESENTATIONS

TWO letters of **OBJECTION** reading the following:

- Proximity to my neighbour fence line is excessive.
- Issues with the size of development.
- Could cause excess drainage.
- Conversion would overlook my garden and affect light to my sunlounge and make kitchen area dark.
- Parking/car issues.
- To be used as a holiday let.
- Toilet windows facing into garden area (No 48)
- Encroachment of garden area.

LDF CORE STRATEGY POLICIES

CS08 - Sustainable Development

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM15 – Environment, Design and Amenity

DM2 – Development Boundaries

NEIGHBOURHOOD PLAN POLICIES

Policy 1: Small Scale *(windfall and infill) development

Policy 3: Residential Extensions

Policy 5: Design Principles

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

The main considerations are:

- Principle of development
- Design
- Impact on neighbour amenity
- Highway safety
- Any other matters requiring considerations

Principle of Development:

The proposal is located within the development boundary of Heacham, a Key Rural Service Centre as defined by Policy CS02 of the Core Strategy 2011.

The application is for a proposed single storey extension and a garage conversion to create a master bedroom with ensuite and additional alterations to the northeast of the property, all within the plot of an existing residential unit. As such, the principle of development is acceptable subject to compliance with neighbourhood, local and national policy.

Design and Impact:

The existing dwelling is a single storey bungalow situated at the bottom of a cul-de-sac within the village of Heacham. The dwelling currently has two bedrooms. It has a pitched roof constructed in brick, render, hung tiles and double glazed windows, with a flat roof to the side of the dwelling. The dwelling has a small garden with garage and drive to the front and a large garden to the rear.

The garage conversion to the northwest would incorporate a proposed master bedroom and ensuite. The new extension to the east would comprise of a living/dining area and would measure 4m deep by 6.4m wide. Both the garage conversion and extension would be 2.8m high and would match the eaves of the original dwelling. A proposed car port would be located to the northwest of the property in front of the proposed master bedroom which would be open sided.

The extension would be constructed using render and would have a flat roof. The windows would be double glazed to match the existing.

The modest design and materials of the dwelling would be in keeping with the appearance of the existing dwelling and the character of South Moor Drive. The application is under the 50% limit for extensions set out in Policy 3 of the Heacham Neighbourhood Plan and would not appear cramped given the spacing around the property. Therefore, due to the subservient and sympathetic scale and design, the proposed extension would have no detrimental impact on the locality.

Given the position of the extension to the rear of the existing dwelling, and the conversion of the garage, the impact on the street scene would be limited. The comments of neighbours cannot therefore be supported. As a result, the application accords with Policy DM15 of the SADMPP, CS08 of the Core Strategy and Policy 3 and 5 of the Heacham Neighbourhood Plan.

Impact on Neighbour Amenity:

The garage conversion would be 2.8m (max) high and 2.3m to the eaves. It would incorporate a slight extension to the height and would increase by 0.7m for a length of 3m from the rear elevation of the dwelling and then step down to its original height of 2.1m. The proposed carport would be 5.2m deep x 3m wide and 2.3m in height and would protrude to the south east of the garage but still lie behind the original building line of the property.

In terms of the impact on No 46, to the north west, there are no proposed windows to the north west elevation of the garage conversion and the car port would be open sided. Further, there is an existing 1.8 metre fence and planting separating the two dwellings creating some screening and as a result, there would be limited impact in terms of overlooking or loss of privacy. In terms of overshadowing and being overbearing, the garage conversion, even at its extended height is low rise, with a flat roof and given its orientation, would not result in conditions detrimental to neighbour amenity.

In terms of the living, dining and kitchen extension, it would have a flat roof at 2.8m height and 4m depth and would wrap around the rear and side elevations (north east and south east). The extension is screened to the south by mature hedging and trees, with the side of the proposed extension a distance of 4.5m from the boundary treatment to No 42. Given the screening, combined with modest scale and height, the extension would have a limited impact on No. 42.

Contrary to the opinion of neighbours, for reasons outlined above, it is considered there are no adverse impacts on the neighbouring properties. The proposal would be acceptable and would comply with Policy CS08 of the Core Strategy, Policy DM15 of the SADMPP and Policy 5 of the Heacham Neighbourhood Plan.

Highway Issues :

As the dwelling would gain a bedroom, parking issues have been raised as a concern. The current dwelling is a 3-bedroom property with the addition of a 4th bedroom thus requiring 3 parking spaces. Although, located towards the end of a cul de sac and losing a garage, there would be two spaces available on the driveway plus a car port and the amended plan has demonstrated that parking for 3 cars is achievable.

No objection from Norfolk County Council has been raised given the small-scale domestic nature of the property.

As a result, the proposal complies with Policy 6 of the Heacham Neighbourhood Plan and Policy CS11 and DM15 of the Local Plan.

Any other material considerations:

This application is only for a conversion and extension and not as a holiday let (which the same use class as residential dwelling).

With only one additional ensuite the drainage should not be excessive and would link in to the existing system.

CONCLUSION

The proposed garage conversion and extension, by virtue of its balanced appearance, appropriate choice of materials and subservient nature would present an in keeping and in scale addition to the dwelling. Further, it would not give rise to any unacceptable neighbour amenity issues.

As a result, the proposal therefore complies with the NPPF, Policies CS06, CS08 and CS11 of the Core Strategy 2011, Policy DM15 of the SADMPP 2016 and Policies 3, 5 and 6 of the Heacham Neighbourhood Plan.

RECOMMENDATION:

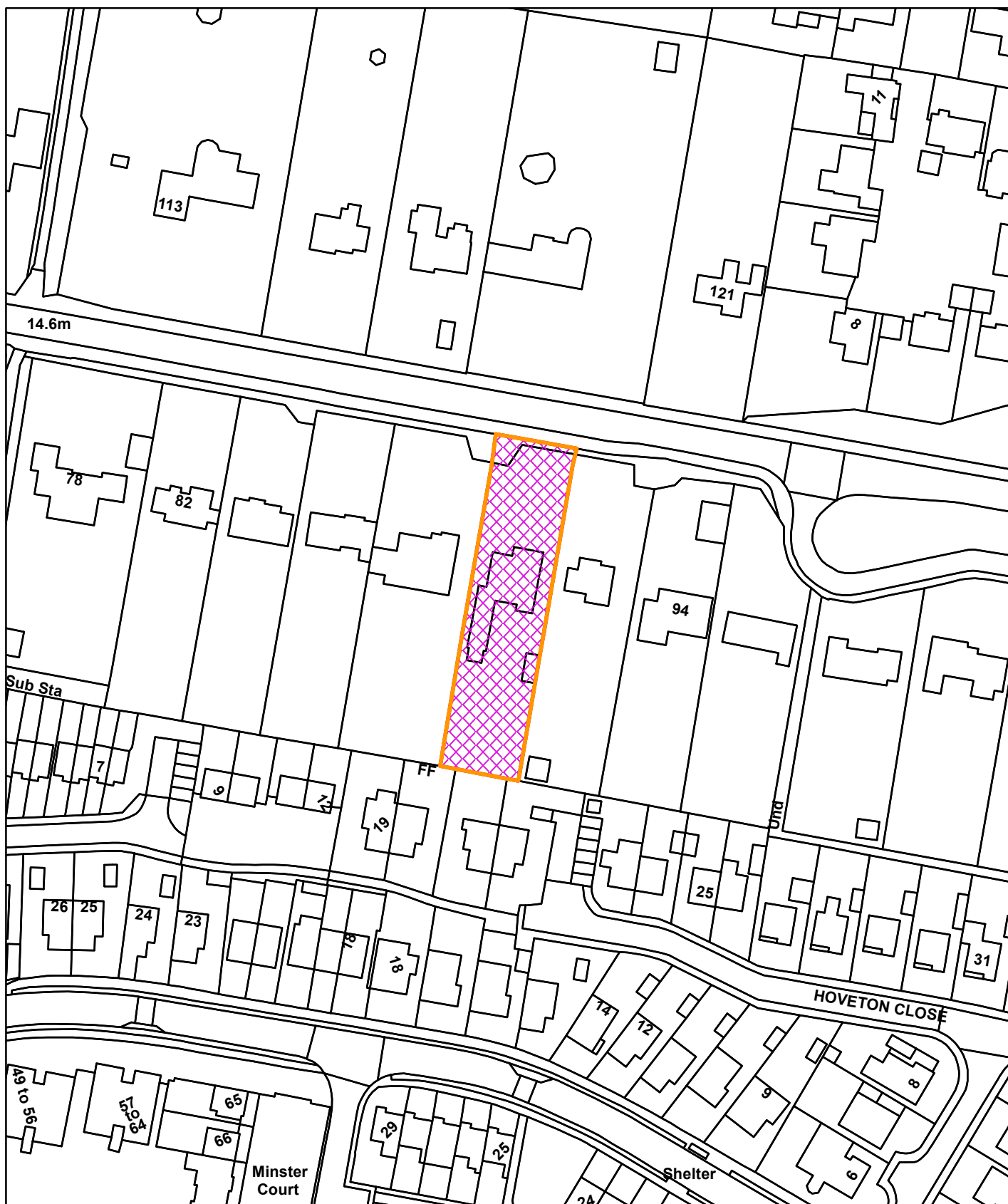
APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans:

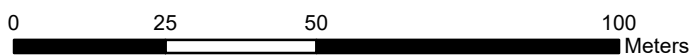
Location Plan - D01 Rev 02
Existing Floor Plan - D02 Rev 02
Proposed Floor Plan - D03 Rev 02
Existing First Floor Plan - D04 Rev 02
Proposed First Floor Plan - D05 Rev 02
Existing Roof Plan - D06 Rev 02
Proposed Roof Plan - D07 Rev 02
Existing Elevations - D08 Rev 02
Proposed elevations - D09 Rev 02
Specification & Section Detail Drawings - D10 Rev 02
Section Detail Drawings - D11 Rev 02
Section Detail Drawings - D12 Rev 02
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: The materials to be used in the construction of the external surfaces of the extension hereby permitted shall match, as closely as possible, the type, colour and texture those used for the construction of the existing building.
- 3 Reason: To ensure a satisfactory external appearance and grouping of materials in accordance with the principles of the NPPF.

22/00282/F

90 Gayton Road Kings Lynn PE30 4ER



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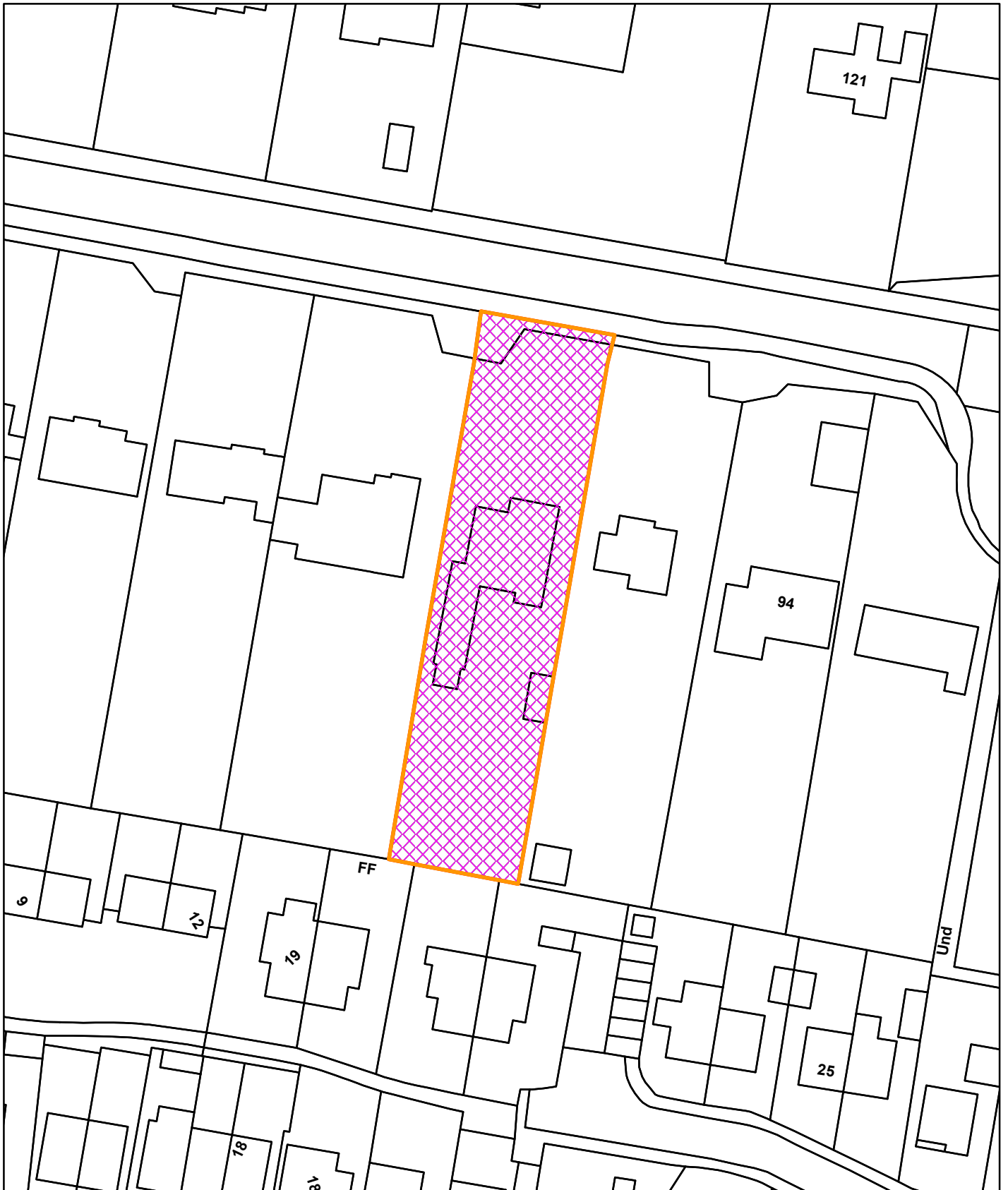
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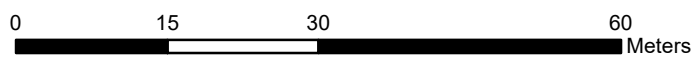


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Parish:	King's Lynn	
Proposal:	Change of use of a dwelling house to a 8 room (8 household) HMO	
Location:	90 Gayton Road King's Lynn Norfolk PE30 4ER	
Applicant:	Mr John Levine	
Case No:	22/00282/F (Change of Use Application)	
Case Officer:	Connor Smalls	Date for Determination: 24 March 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Called in by Councillor Rust

Neighbourhood Plan: No

Case Summary

The application site consists of an existing detached house within King’s Lynn, close to Queen Elizabeth Hospital and on a major route into the core of the town (Gayton Road) consisting of a mostly residential area.

The application seeks consent for a House of Multiple Occupation (HMO) providing 8 bedrooms.

Key Issues

- *Principle of development
- *Form and character
- *Impact on neighbour amenity
- *Highway safety
- *Assessment under DM4

Recommendation

APPROVE

THE APPLICATION

The application site is located on Gayton Road, a major route into the core of the town and in close proximity to the Queen Elizabeth Hospital. The site consists of a large, detached house set within a large and deep plot, and the local area is of a mostly residential character

with neighbouring dwellings to the east, south and west. The dwelling is currently used as a smaller HMO (up to 6 unrelated individuals), not requiring full planning permission.

The application itself seeks consent for a House of Multiple Occupation (HMO) providing a total of 8 bedrooms with a new shower room and the creation of two bedrooms from an existing room. No external works to the site or dwelling itself are proposed other than the provision of a bin store to appropriate standards and the addition of bike storage in sheds around the site.

SUPPORTING CASE

The Applicant seeks permission to change the use of the property to a 8 room (8 household) HMO, the property is currently used a small HMO that does not require planning permission.

The application has been called in by Councillors, however, the planning reasons for the call in are not published on the Councils website.

The application is supported by a location plan, floor plans, cycle storage areas and a bin storage plan, as requested by Officers.

In terms of the proposed use Planning Policy DM4 of the SADMPP (2016) states that the conversion of existing dwellings/ new development for HMOs may be permitted where:

- there is no adverse impact on the amenity of existing and new residents and the historic and natural environment
- the development and associated facilities can be provided without significant detriment to the occupiers of adjoining or neighbouring properties; and
- the site is within reasonable distances to facilities, public open space, supporting services and local employment.

The site is located in a sustainable residential area, extremely close vicinity to the Queen Elizabeth Hospital and within walking distance to the amenities of Kings Lynn.

The visual appearance of the building is proposed to remain as existing, as no external changes are proposed to accommodate the additional internal rooms. Therefore, the proposal will not result in impacts upon the character of the area and or neighbour amenity in relation of loss of privacy, or overbearing impacts.

The building is proposed to be used as an HMO due to the demand of staff from the hospital needing accommodation close to their place of work. The property will be used similar to a dwelling house compatible to the existing residential uses and will not give rise to undue noise and disturbance. However, if Councillors consider it necessary, a management and maintenance plan can be prepared, and this could be imposed by a condition in the event of Councillors following officer recommendation of approval.

PLANNING HISTORY

2/00/0223/F: Application Permitted: 18/04/00 - Extension to provide self-contained accommodation for elderly relative and construction of detached double garage.

RESPONSE TO CONSULTATION

Parish Council: N/A

Highways Authority: NO OBJECTION

To accord with the adopted parking standards the site should be provided with 4 car parking spaces and 8 cycle spaces. It is evident that the existing external car parking area could accommodate the required number but cycle parking is currently not shown. As the development has the land to provide enclosed cycle parking to accord with standard a condition is recommended.

Cycle storage has now been shown on plans so a condition is attached to ensure this is implemented prior to occupation.

Housing Standards: NO OBJECTION

In response to the consultation referred to above we would comment as follows:

Space Standards

The conversion of the reception room, into the two double bedrooms (bedroom 5 and 6) are in satisfactory condition. The sizes of the bedrooms 5 and 6 are good to be called a double bedroom's sizes and can be occupied by a couple, married or co-habiting. (2 people each room), satisfactory.

The 8-bedroom property is sufficient enough to allocate 8 occupiers. The facilities in this property have been upgraded with additional shower room, WC and hand basin to keep with amenity standard. All satisfactory with Housing Standards requirements.

Informative

The proposed development will require an application for a HMO licence prior to occupation of 5 or more persons from two or more separate households.

The above response is informed by and made on the basis of plans submitted as part of the above planning application only. It is assumed that fire precautionary and other works are to be undertaken in accordance with current building regulations, and, in this instance, subject to oversight by the Fire and Rescue Services.

Community Safety and Neighbourhood Nuisance (CSNN): NO OBJECTION

I have assessed the above application and confirm that I have no objections/requests for conditions.

Waste and Recycling Manager: NO OBJECTION

Following receipt of amended plans detailing appropriate bin storage to the front of the property, no objection is raised.

REPRESENTATIONS

None received at time of writing.

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS03 - King's Lynn Area

CS08 - Sustainable Development

CS09 - Housing Distribution

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

DM2 – Development Boundaries

DM4 - Houses in Multiple Occupation

DM15 – Environment, Design and Amenity

DM17 - Parking Provision in New Development

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

- *Principle of development
- *Form and character
- *Impact on neighbour amenity
- *Highway safety
- *Assessment under DM4

Principle of Development:

The application site is within the development boundary of King's Lynn as defined within the Site Allocations and Development Management Policies Plan 2016 and the proposed residential use is within an established residential area. Policy DM4 of the Site Allocations and Development Management Policies Plan 2016 - Houses in Multiple Occupation, establishes the principle that conversion of existing dwellings to, and new development of properties for, multiple occupation, may be permitted subject to certain considerations and limitations. This is as follows:

DM4 - Houses in Multiple Occupation

The conversion of existing dwellings to and new development of properties for multiple occupation may be permitted where:

- There is no adverse impact on the amenity of existing and new residents and the historic and natural environment; and
- The development and associated facilities, including bin storage, car and cycle parking, can be provided without significant detriment to the occupiers of adjoining or neighbouring properties; and
- The site is within reasonable distances to facilities, public open space, supporting services and local employment.

These issues are considered in more detail below.

Form and Character:

There are no external changes proposed or carried out on the dwelling as part of this development so the detached house would not be impacted externally and there would be no impact on the street scene or visual amenity of the wider area. Bins will be stored to the front of the property to an agreed standard with the Waste and Recycling Manager. This is as with many properties along this part of Gayton Road, which have a large area at the front of properties which are set back. The bins would also be somewhat screened by front boundary planting. The cycle parking is to be within the site, and there is ample space to provide it.

Impact on Neighbour Amenity:

It is not considered that there would be any adverse or unacceptable impact on the amenity of neighbouring dwellings. There have been no neighbouring objections to this scheme and CSNN raise no objection to the proposal. The use remains residential and whilst intensified compared to the large mostly single-family houses nearby, the use is considered to be compatible and acceptable given the nature of the site, which is a detached property set in relatively large grounds.

Highway Safety:

Norfolk County Council Highways raise no objection to the scheme but requested that a condition is attached to any decision detailing the provision of cycle storage on site; enough information was considered present in relation to parking for cars, which would be on the large area at the front of the site. There is enough space for at least four cars which meets the required standard from Norfolk County Council Highways.

This information has now been provided so a condition will instead be attached ensuring that the cycle provision is made available prior to occupation. No other parking or highway safety matters are outstanding.

Assessment Under DM4:

Amenity has been discussed in the above section of the report. The historic and natural environment are not affected as there are no external changes to the dwelling and the rear garden area remains. Bin storage alongside car and cycle parking has been provided with no consultee, public or neighbouring objections. The site is within easy access to greenspace either by walking, cycling or public transport. The site is also within an approximately 20-minute walk to the local centre of Gaywood which is within easy cycling

distance and has direct public transport links from the site. Cycle and public transport links also extend into the centre of King's Lynn which can be accessed from close proximity to the site. The site is also very close to the hospital, one of the towns major employers.

CONCLUSION

Overall, the principle of development is supported by virtue of the site being located within the development boundary for King's Lynn. The principle of development converting existing dwellings into properties for multiple occupation is also supported by policy DM4 of the Site Allocations and Development Management Policies Plan 2016 subject to details. The form and character of the dwelling will be unaltered as no external alterations are proposed, cycle parking and the addition of suitable bin storage will not have an adverse visual impact on the locality.

The impact of the development on neighbour amenity is considered to be acceptable with no adverse impact resulting from the proposed development. NCC Highways raise no objection, and the Waste and Recycling Manager considers the proposed bin store to the front of the property to be acceptable. The proposal is considered to fully accord with Policy DM4 of the Site Allocations and Development Management Policies Plan 2016 and the development is therefore recommend for approval.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans: Location Plan, received: 26/01/23, 90 Gayton Road, outside measurements/bin location, received: 19/05/23, EXISTING/PROPOSED FIRST FLOOR, received: 27/01/23 and PROPOSED GROUND FLOOR PLAN - YELLOW MARK, received: 27/01/23.
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: Prior to the first occupation of the development hereby permitted, cycle parking shall be fully implemented as detailed on plan: 90 Gayton Road, outside measurements/bin location, received: 19/05/23 and thereafter retained for this purpose in perpetuity.
- 3 Reason: To ensure the provision of adequate cycle parking that meets the needs of occupiers of the proposed development and in the interests of encouraging the use of sustainable modes of transport.
- 4 Condition: Prior to the first occupation of the development hereby permitted, bin storage shall be fully implemented as detailed on plan: 90 Gayton Road, outside measurements/bin location, received: 19/05/23 and thereafter retained for this purpose in perpetuity.

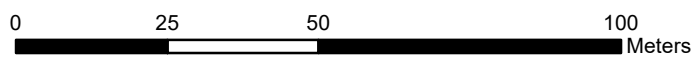
- 4 Reason: To ensure the provision of adequate bin storage that meets the needs of occupiers of the proposed development and in the interests of the amenities of the locality in accordance with the principles of the NPPF.

23/00470/CU

20 Woodside Kings Lynn PE30 4SD



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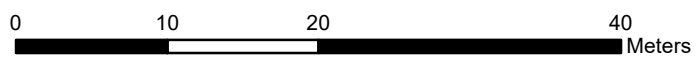


23/00470/CU

20 Woodside Kings Lynn PE30 4SD



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23/05/2023

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Parish:	King's Lynn	
Proposal:	Change of use of open space land to garden land (Retrospective)	
Location:	20 Woodside King's Lynn Norfolk PE30 4SD	
Applicant:	Mr Darren Liddy	
Case No:	23/00470/CU (Change of Use Application)	
Case Officer:	Mrs Jade Calton	Date for Determination: 1 June 2023

Reason for Referral to Planning Committee – Called-in by Cllr Everett

Neighbourhood Plan: No

Case Summary

The application site comprises a rectangular parcel of land measuring approximately 55.8 square metres and is located to the north-eastern side of No. 20 Woodside, Fairstead, King's Lynn.

The land previously formed a grass verge set aside as part of the landscaping scheme for the original development of the estate, and has been enclosed by a 1.8m close boarded timber fence and changed to garden land in association with No. 20.

This application seeks to retrospectively change the use of the land to garden.

The site is located within the Sub-Regional Centre of the Borough, as defined by the Core Strategy.

Key Issues

- * Principle of Development
- * Form and Character
- * Other Material Considerations

Recommendation

APPROVE

THE APPLICATION

The application site comprises a rectangular parcel of land measuring approximately 55.8 square metres and is located to the north-eastern side of No. 20 Woodside, Fairstead, King's Lynn.

The land previously formed a grass verge set aside as part of the landscaping scheme for the original development of the estate, and has been enclosed by a 1.8m close boarded timber fence and changed to garden land in association with No. 20.

The site is owned by the Local Authority and the application seeks to retrospectively change the use of the land to garden.

SUPPORTING CASE

Due to the nature and scale of the application, it is not accompanied by a Planning Statement.

PLANNING HISTORY

No recent history.

RESPONSE TO CONSULTATION

Parish Council: N/A

Highways Authority: **NO OBJECTION**

Property Services: **NO OBJECTION** to the grant of planning permission for a change of use to garden land but the owner would need to agree terms with the Council as landowner to occupy the land and this may or may not be granted.

REPRESENTATIONS

No Third Party comments received.

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS03 - King's Lynn Area

CS08 - Sustainable Development

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

DM22 – Protection of Local Open Space

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The key considerations in the determination of this application are as follows: -

- Principle of Development
- Form and Character
- Other Material Considerations

Principle of Development:

The site lies within King's Lynn's Sub-Regional Centre where the principle of development is acceptable in accordance with the Development Plan.

The area is residential in character and the use of the land as garden land in association with the directly adjoining dwelling could be considered acceptable subject to other material considerations.

Form and Character:

The locality is characterised by areas of public open space, which softens the 1960s housing estate development. The application site comprises a narrow-grassed section of land which formed part of the wider landscaping of the original estate adjacent to the footpath network. This small section of land offers limited wider amenity value to the character of the area.

The land has been enclosed with a 1.8m close boarded timber fence directly adjacent to the existing garden of No.20, to allow for a larger private amenity space. The change of use does not incorporate the entire strip of amenity area to the north-east of No.20, there is a grassed area to the front and rear of the application site, along with other much larger areas of public open space within the immediate vicinity.

The site is located in a residential area where the change of use of the land would not appear at odds with the street scene. There appears to have been other similar changes of use of land to private garden space within the locality. Whilst the LPA accepts the importance of the landscape buffers and their contribution to the quality of the area, each case is considered on its own merits, taking into consideration the size and location of the parcel of land and how much weight should be afforded to its individual contribution to the local environment.

In this case, it is considered that the loss of the small section of the grass verge would not cause any detrimental impact on the character and appearance of the area.

The LPA would not consider the land in question to be an area of 'local open space' as defined in Policy DM22 of the Site Allocations and Development Management Policies Plan (2016). Therefore its value is limited and its loss would not adversely impact on public access, visual amenity, local distinctiveness, recreational value or biodiversity, as set out within the abovementioned Policy.

As such, it is considered that the application accords with Development Plan Policies CS03, CS08, DM15 and DM22; and the general provisions of the NPPF.

Other Material Considerations:

Land Ownership:

The parcel of land is under ownership of the Local Authority. The Property Services department are aware of the application for the change of use and raises no objection in principle. They are in the process of issuing a license for the lease of the land.

Crime and Disorder

Section 17 of the Crime and Disorder Act 1998 requires Local Authorities to consider the implications for crime and disorder in the carrying out of their duties. The application before the Committee will not have a material impact upon crime and disorder.

CONCLUSION

It is your officer's opinion that the retrospective change of use of a small section of estate landscaping to garden land, in association with No. 20 Woodside, causes no detrimental impact on the amenities of the locality or wider character and appearance of the area.

It is therefore considered that the proposal accords with Development Plan Policies CS08, DM15 and DM22; and the general provisions of the NPPF.

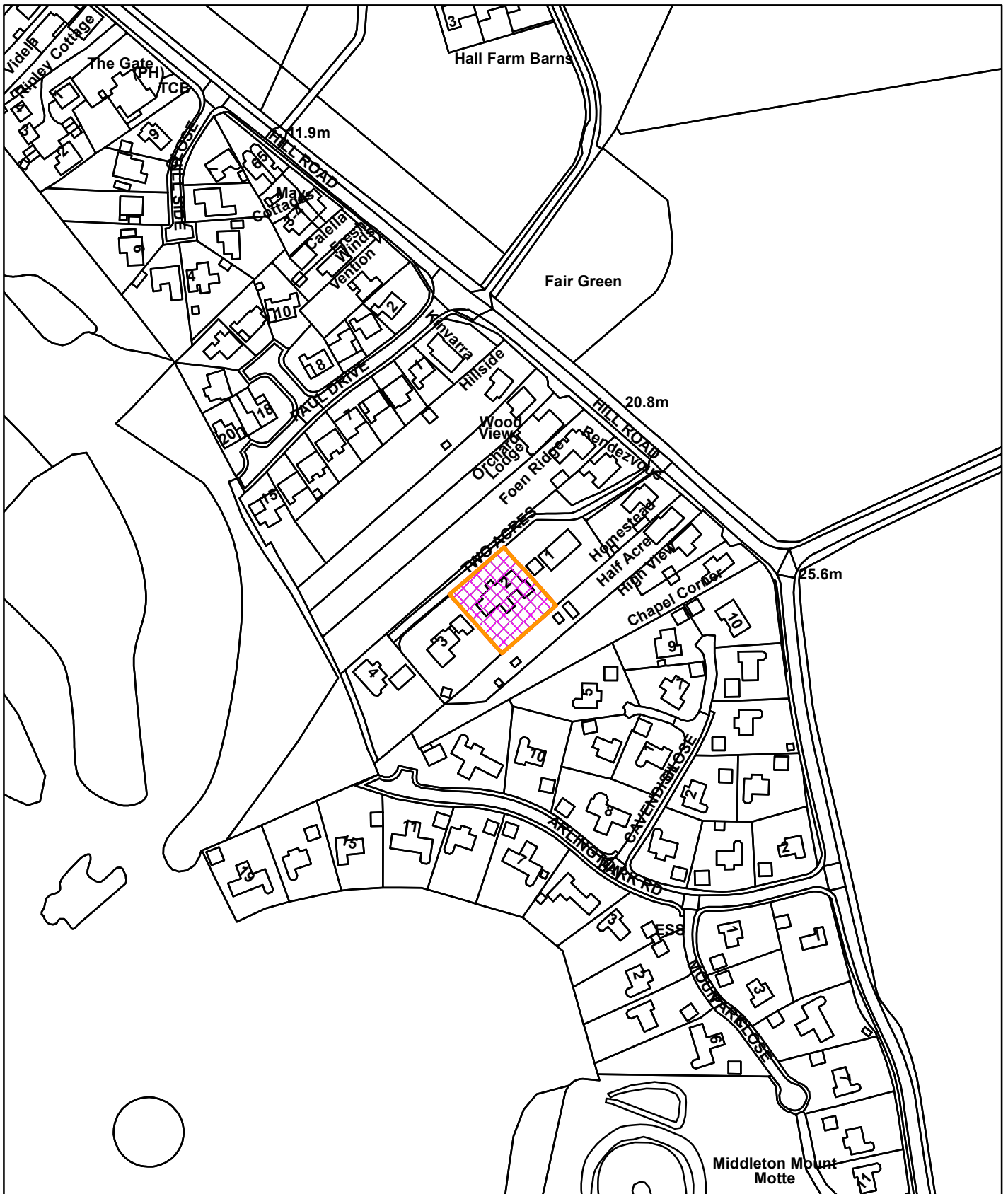
RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted has been determined in accordance with the following approved plans: Ordnance Survey Location Plan (scale 1:1250).
- 1 Reason: For the avoidance of doubt and in the interests of proper planning.

23/00361/F

2 Two Acres Middleton PE32 1YF



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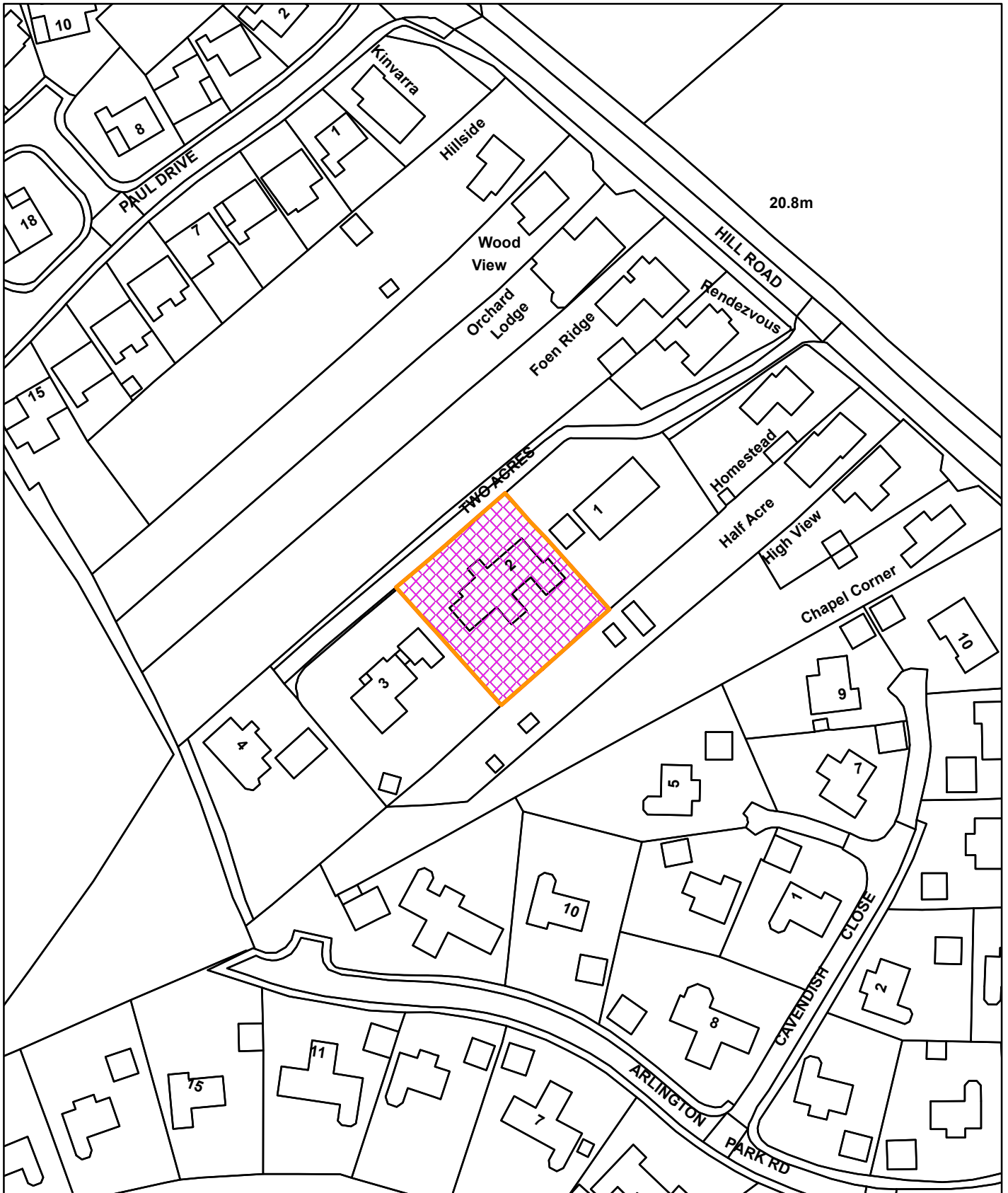
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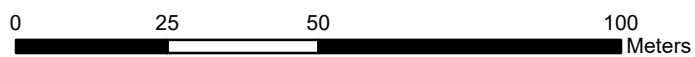


23/00361/F

2 Two Acres Middleton PE32 1YF



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22/05/2023

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Parish:	Middleton	
Proposal:	Single storey extension to front of house	
Location:	2 Two Acres Middleton King's Lynn Norfolk	
Applicant:	Mr D Hazelhurst-Jeavons	
Case No:	23/00361/F (Full Application)	
Case Officer:	Charlotte Castell-Smith	Date for Determination: 5 May 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Council staff involved in the planning process.

Neighbourhood Plan: No

Case Summary

The application relates to the construction of a single storey front extension at a detached dwelling, 2 Two Acres, Middleton. The site is located approximately 115m south-west of the highway, Hill Road.

The site is located within the development boundary of Middleton which is a Rural Village as by Policy CS02 of the Core Strategy.

Key Issues

Principle of Development
Form and Character
Impact on Neighbours

Recommendation

APPROVE

THE APPLICATION

The land is situated on the southern side of 2 Two Acres, Middleton.

The site comprises a detached two-storey dwelling, finished in brick and concrete interlocking tiles.

Boundary treatments include a mix of low brick wall, close-board timber fencing and hedges.

Planning Committee
5 June 2023

The application seeks the construction of a modest single storey extension with mono-pitched roof which would extend the study. It would be located on the north-west facing front elevation to the south west of the porch. The ridge height of the proposed extension would be 3.7m, and the eaves height would be 2.7m. The materials are to match those existing which are a red multi brick and a concrete interlocking roof tile, with brown upvc glazing.

SUPPORTING CASE

The aim of this proposal is to extend a single small room to the front of the dwelling. At present this box room is quite diminutive in scale which greatly limits its use. The proposal seeks to extend the room outwards by 1710mm (measured internally) which will provide the additional space required but leaves the front wall of the extension set back from that of the existing storm porch.

This arrangement maintains the visual balance of the elevation and also ensures that the extension remains subservient in its relationship with both the porch and the main house itself.

The proposal will be constructed using materials that closely match those of the existing structure and this will further limit the impact of this modest addition on the appearance of the existing dwelling.

PLANNING HISTORY

09/00923/F: Application Permitted (Delegated): 20/08/09 - Extensions to existing dwelling - 2 Two Acres Middleton
2/02/0345/F: Application Permitted (Delegated): 12/04/02 - Extensions to dwelling - 'Carinya' Two Acres Fair Green

RESPONSE TO CONSULTATION

Parish Council: NO RESPONSE received.

REPRESENTATIONS

None received.

LDF CORE STRATEGY POLICIES

CS02 - The Settlement Hierarchy

CS01 - Spatial Strategy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM1 – Presumption in Favour of Sustainable Development

Planning Committee
5 June 2023

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

- Principle of development
- Form and character
- Impact on neighbour amenity

Principle of Development:

The application proposes the construction of a single storey front extension to an existing detached dwellinghouse at 2 Two Acres, Middleton. The site is located within the development boundary for Middleton, and within the curtilage of the existing dwelling. The development proposed is in accordance with CS08 of the Core Strategy (2011) and DM1 of the Site Allocations and Development Management Policies Plan (2016).

The main issues in relation to this application are whether the proposal is acceptable in design terms and whether it will be detrimental to the amenity of neighbours.

Form and Character:

Two Acres is a small private road that composes of detached two-storey dwellings, with large, relatively modern dwellings situated to both the east and west of the application site.

The existing dwelling is a somewhat modern, two-storey detached dwelling built of red brick and concrete interlocking roof tiles. It has a front porch also constructed of brick. The proposed extension would adjoin the porch to the south-west side of the dwelling, measuring approximately 4.5m in width, 1.7m in depth and 3.6m in height. The proposal would be slightly set back from the porch, approximately 0.25m, and would be finished with facing brick and roof tiles to match the existing dwelling. Given the scale, positioning, and design of the proposal, which does not extend beyond the existing building, the proposal is considered to have no detrimental impact on the form and character of the area.

The proposal is therefore acceptable in design terms and complies with Policies CS06 and CS08 of the Core Strategy 2011 and Policy DM15 of the SADMPP 2016.

Impact on Neighbour Amenity:

The proposed extension is located to the north west elevation of the property adjoining the existing porch. There are no side facing windows to the south west facing 3 Two Acres. The proposed extension would be situated approximately 10.5m from the boundary and a further 2.5m (approx.) to the neighbour's garage. The site is also well screened to the west by close-board fencing.

The proposal would not impact the neighbour to the north-east (1 Two Acres) as the extension would be constructed on the south-west end of the dwelling and would be entirely screened by the existing porch.

There are no dwellings directly to the North of the application site and the land opposite is well screened with dense trees and hedges which serves as garden land for a dwelling situated on Hill Road.

Given the size of the proposal and the substantial distance from the neighbouring dwelling and amenity space, the proposal would result in no material impact with regard to overlooking, overshadowing, and overbearing.

The proposal is therefore acceptable in design terms and complies with Policies CS06 and CS08 of the Core Strategy 2011 and Policy DM15 of the SADMPP 2016.

CONCLUSION

It is considered that the proposed extension will not have an adverse impact on the appearance of the dwelling nor the form and character of the area. Given the modest scale of the extension and the boundary treatments in place, the proposal would not have an adverse impact upon neighbour amenity.

Overall, the proposal is in accordance with the requirements of the National Planning Policy Framework 2021, Policy CS08 of the Core Strategy and Policies DM1 and DM15 of the SADMPP. As a result, it is recommended that this application be approved.

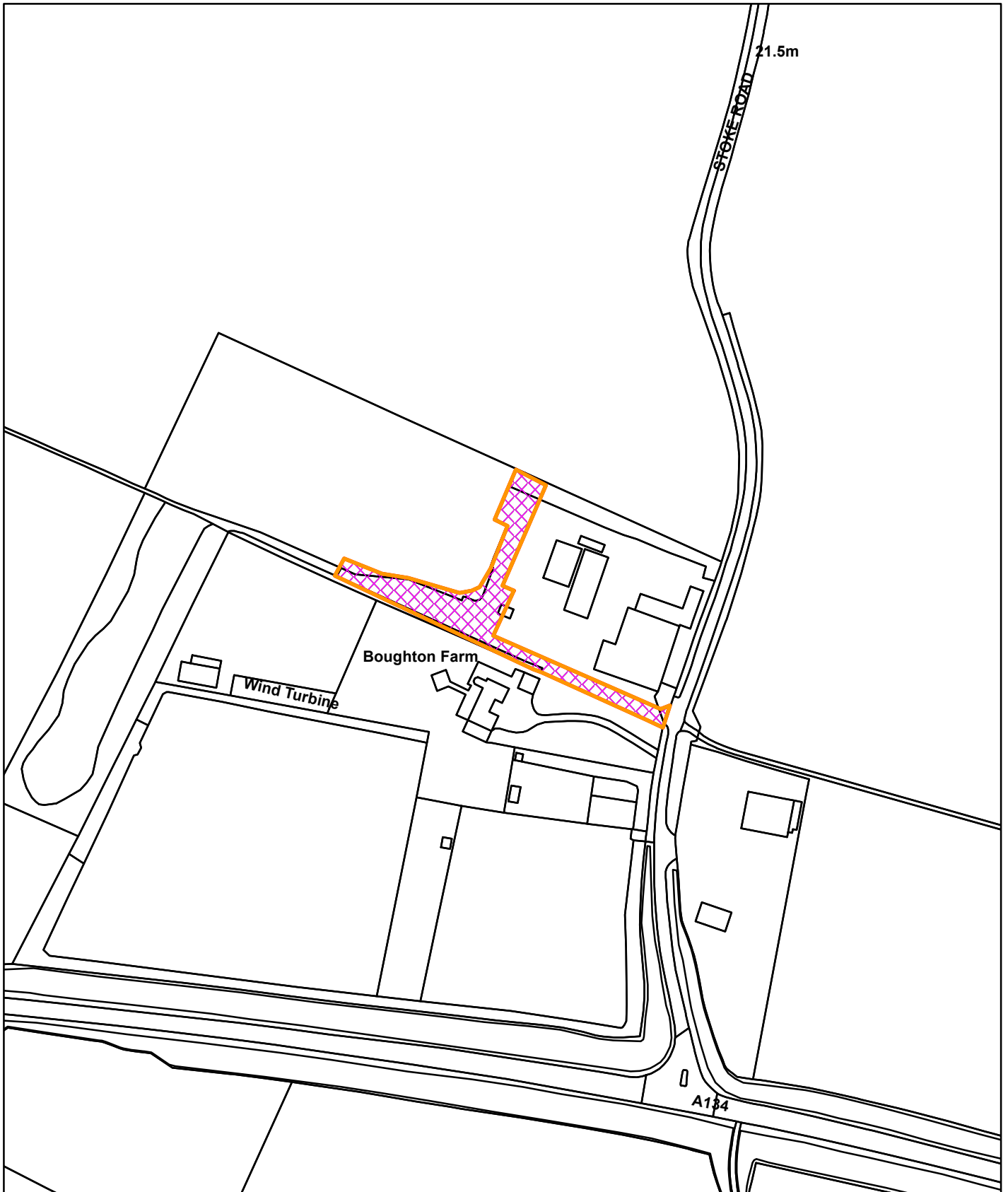
RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans: 3/440/2A
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.

23/00092/F

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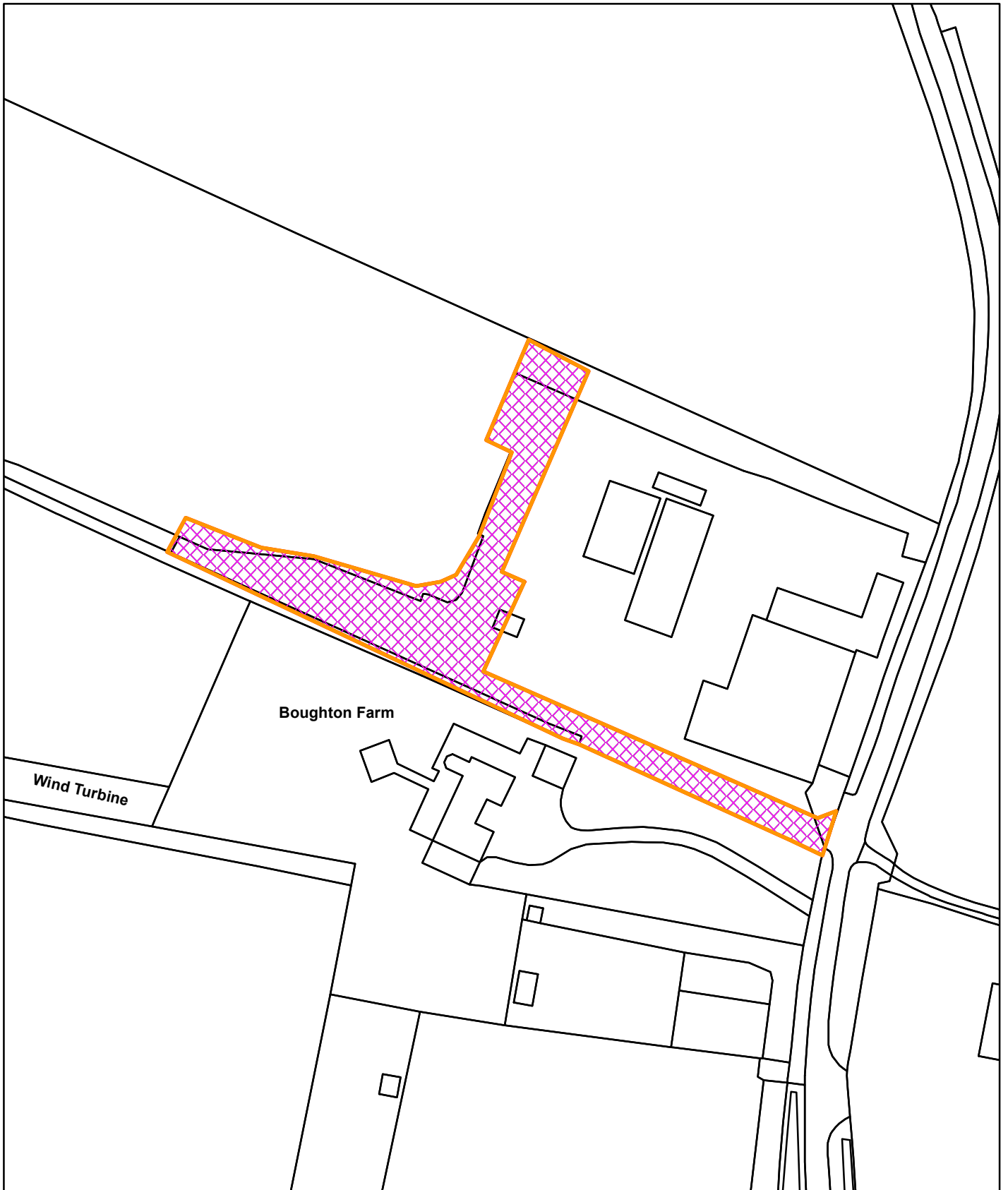
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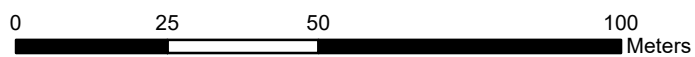


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24/05/2023

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Parish:	Stoke Ferry	
Proposal:	The siting of 30 '20 foot' containers for local storage (Part-retrospective)	
Location:	Stoke Ferry Timber Ltd Boughton Road North Stoke Ferry KINGS LYNN	
Applicant:	Mr Colin Bond	
Case No:	23/00092/F (Full Application)	
Case Officer:	Helena Su	Date for Determination: 17 March 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Called in by Cllr Sampson

Neighbourhood Plan: No

Case Summary

The application is for the part-retrospective siting of storage containers within Stoke Ferry Timber Yard. Currently there are ten unauthorised storage containers on the site. An additional twenty storage containers are proposed, totalling 30 containers.

The application site is Stoke Ferry Timber Yard which is located outside of Stoke Ferry's development boundary by approx. 230m. The whole site is approx. 2.9ha in size and consists of a number of buildings related to other local businesses. The application site is approx. 0.25ha, and is 'L' shape, along the south and east of the new woodlands, planted on land to the northwest of the yard. The application site also includes the access into Stoke Ferry Timber Yard.

Stoke Ferry is classified as a Key Rural Service Centre (KRSC) under the settlement hierarchy of Policy CS02 of the Core Strategy (2011). The application site is outside of the development boundary and therefore treated as countryside.

Key Issues

- *Principle of Development
- *Impact on Character
- *Impact on Neighbour Amenities
- *Flood Risk and Drainage
- *Other material considerations

Recommendation:

APPROVE

THE APPLICATION

The application is for the part-retrospective siting of storage containers within Stoke Ferry Timber Yard. Currently there are ten unauthorised storage containers on the site. An additional twenty storage containers are proposed, totalling 30 containers.

This application is one of two planning applications and one lawful development certificate recently applied for on this site. The other planning application relates to a retrospective storage area (planning ref: 23/00125/CU). Both planning applications have been called in to Planning Committee

The application site is Stoke Ferry Timber Yard which is located outside of Stoke Ferry's development boundary by approx. 230m. The whole site is approx. 2.9ha in size and consists of a number of buildings related to other local businesses. The application site is approx. 0.25ha, and is 'L' shape, along the south and east of the new woodlands, planted on land to the northwest of the yard. The application site also includes the access into Stoke Ferry Timber Yard.

The storage containers are presently, and proposed to be, offered to local residents, community groups, scouts, schools, and businesses as a secure facility for ad hoc storage. Some of the containers are also said to be used by Stoke Ferry Timber and associated businesses on the yard.

SUPPORTING CASE

None submitted to date.

PLANNING HISTORY

23/00125/CU: Pending Consideration - A change of use from an outdoor storage area and lorry park for agricultural use, to an outdoor storage area for commercial and personal use (retrospective)

23/00039/LDE: Would/Was Lawful - Application for a lawful development: To continue use of the buildings for an engineering company like it has been used for over 10 years

09/00136/CU: Application Permitted: 15/06/09 - Change of use of agricultural building and yard to storage and distribution of timber including trade counter (Committee Decision)

09/01866/F: Application Permitted: 08/02/10 - Variation of Condition 6 of Planning Permission 09/00136/CU revising siting of acoustic fence (Committee Decision)

2/99/0462/F: Application Permitted: 20/05/99 - Construction of office extension and vehicle workshop

2/96/0294/F: Application Permitted: 13/05/96 - Demolition of existing office and construction of enlarged office

2/93/1185/F: Application Permitted: 02/11/93 - Construction of agricultural store building (Committee Decision)

RESPONSE TO CONSULTATION

Parish Council: SUPPORT

Highways Authority: NO OBJECTION

Provided the containers are utilised for long term storage rather than operations that require more regular/ daily collections such as builders materials stores or Internet distribution business for example then we tend to find that the associated number of trips is otherwise low.

However, given that the site would generally be car dependant due to its distance from populations centres and public service provisions, I would recommend that a suitably worded condition be provided to restrict the use of the containers to being long term storage only.

Community Safety and Neighbourhood Nuisance: NO OBJECTIONS Subject to conditions relating to drainage, the acoustic fence, opening hours, lighting and submission of a noise management plan.

REPRESENTATIONS FIVE SUPPORT comments, comments regarding:

- Ideal location for residents/businesses in Stoke Ferry and surrounding areas.
- Will help those that need a secure storage place for personal or commercial use.
- Good location for small local businesses.
- Could provide storage for homes with no outbuildings
- Easily accessible during work hours and does not cause traffic through Stoke Ferry.

TWO OBJECTION comments, made by the same person regarding the following:

- Seems the council have gone out of their way to help the applicant's planning agent submit an application. I believe that the Planning Enforcement Officer at the Borough Council has both advised and acquiesced with the Applicant's planning agent over this intended misrepresentation.
- No screening, either visual or acoustic appears between our boundary and the containers. The very least would be to continue the acoustic fence at least 15m past the last container, to help shield us from the wood yard.
- Better solution would be to remove the row of containers altogether
- Increased number from 20 to 30 containers
- Hours of operation - applicants been giving out keys to the gate to the tenants. People would show up at night and throughout the weekend, outside of the operating hours of the yard. This raises concerns on security with people looking into neighbour garden.
- Concern that the row of containers running north-south, would be on top of the open soak away that takes the rain and storm water from the existing yard to wood yard buildings.
- The Application plans and maps submitted misrepresent the impact of the existing containers have on our property by misleading colouring of the Western end of our gardens. The Application plans show this area coloured in as though it is an agricultural field rather than part of the curtilage of our house.
- The Applicant's planning agent has either neglected / avoided showing that the land where these existing containers stand is nearly 1m higher than our garden level at the base of the boundary hedge, and 2m higher at the vegetable garden hedge (that runs East - West Close to the Wind Turbine. Thus, he has misrepresented, whether intentionally or otherwise, the visual impact of these containers on our gardens.

LDF CORE STRATEGY POLICIES

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS10 - The Economy

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

- Principle of Development
- Impact on Character
- Impact on Neighbour Amenity
- Flood Risk and Drainage
- Other material considerations

Principle of Development:

Stoke Ferry is classified as a 'Key Rural Service Centre' (KRSC) within the settlement hierarchy under Policy CS02 of the Core Strategy. The application site is outside of Stoke Ferry's development boundary and under Policy DM2 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016), will be treated as countryside.

Paragraph 85 of the NPPF (2021) states that planning decisions should recognise that sites to meet local business and community needs in rural areas may have to be found adjacent or beyond existing settlements, and in locations that are not well served by public transport. In these circumstances, it will be important to ensure that development is sensitive to its surroundings, does not have an unacceptable impact on local roads and exploits any opportunities to make a location more sustainable. The use of previously developed land, and sites that are physically well-related to existing settlements, should be encouraged where suitable opportunities exist.

Furthermore, Policy CS10 of the Core Strategy (2011) states the Council will support rural economy and diversification through a rural exception approach to new development within the countryside; and through a criteria based approach to retaining employment land and premises. Permission may be granted on land which might not otherwise be appropriate for development for an employment generating use which meets a local business need, where it

is appropriate in size and scale to the local area, adjacent to the settlement, and the development and use would not be detrimental to the local environment or local residents.

Subject to considerations of the detailed criteria in this report, the principle of development, for the siting of storage containers at Stoke Ferry Timber Yard, is considered to comply with Policy CS10 of the Core Strategy (2011), and provisions within the NPPF (2021).

Impact on Character:

Presently, there are ten containers situated to the south of the new woodlands planted to the west of the yard (running north-west to south-east). An additional three containers are proposed to join this row of containers, and a further seventeen containers to be sited along the west of the yard (running north to south). Each individual container would be approx. 6m deep and 2.5m wide.

As the containers are sited within a yard which comprises industrial buildings, the visual impact of the containers within this setting is considered to be limited. The neighbour raised concerns that due to the topography of the yard, which is raised compared to their domestic curtilage, there are views of the containers from their curtilage. However, considering the context of the site, and as views are not a material planning consideration, the containers are considered to result in limited visual harm given the industrial character of the yard.

On impact on character, the proposal is therefore considered to comply with policies CS06 and CS08 of the Core Strategy (2011) and Policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016).

Impact on Neighbour Amenity:

The containers currently on the site (running north-west to south-east) are situated approx. 6.2m from the south-west boundary, which is screened by the neighbour's (Boughton Farm) hedgerow. The proposed shipping containers (running north to south), at its closest point, would be sited 12.1m from the south-west boundary.

As a part-retrospective application, the neighbour has raised concerns with noise, operating hours, and security issues resulting from the use of the storage containers.

To mitigate against the potential noise implications arising from the development, the acoustic fence is proposed to be extended by approx. 106m and will screen beyond the length of Boughton Farm's domestic curtilage by approx. 21m. Full details of the acoustic fencing have been provided on drawing no 02c/CB/15/2024, which the Community Safety and Neighbourhood Nuisance team have not raised any concerns with and are satisfied would offer mitigation to noise resulting from this proposal. The erection of the acoustic fence will be controlled via condition.

The neighbour's concern with security and people being able to view into their private amenity space is considered sufficiently covered by the erection of the acoustic fence, which would be 1.8m high and extend beyond the domestic curtilage of Boughton Farm.

The planning agent and applicant have said they would like the use of the storage containers to go beyond the permitted hours of use of the yard. The applicant has proposed opening hours of the containers to be between 7.30 and 18.00 Monday to Saturday during British Summer Time (BST), and 7.30 and 16.00 Monday to Saturday during Greenwich Mean Time (GMT). In addition, the applicant has proposed occasional access to the storage containers, outside of these proposed extended hours by appointment only. It is considered the proposed extended hours would give rise to amenity concerns, in regard to noise and

disturbance. However, it is considered reasonable to allow the containers to be used within the currently permitted hours of the yard and shall be conditioned as such.

The Community Safety and Neighbourhood Nuisance (CSNN) team asked for a noise management plan to better understand how the applicant intends to operate the use of the storage containers outside of the currently permitted hours, without having a detrimental impact on the neighbour. Given this, a condition for the submission and agreement of a noise management plan is also sufficiently necessary in the interest of neighbour amenity. The noise management plan to be agreed will set out measures in place to control the use of the site outside of the permitted hours set out in condition 5.

On impact on the neighbour, the proposal is therefore considered to comply with policy CS08 of the Core Strategy (2011) and Policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016).

Flood Risk and Drainage:

The site is located within Flood Zone 1 of the Strategic Flood Risk Assessment (2018) and is therefore at the lowest risk of flooding. The Environmental Agency did not comment on the application.

A third party comment raised concerns with drainage on the site as the containers would be situated on an open soakaway. Drainage details have been provided by the planning agent. The Community Safety and Neighbourhood Nuisance (CSNN) raised no concerns with the drainage strategy submitted by the planning agent. The proposed drainage strategy will be controlled via condition.

Overall, the proposal complies with policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016) and paragraph 167 of the NPPF (2021).

Other Material Considerations:

The Local Highway Authority had no objections with the scheme if the storage containers were used for long-term storage (and conditions as such), which would thus generate low levels of traffic. It is the opinion of the Local Planning Authority that such condition would not meet the test for planning conditions.

The Community Safety and Neighbourhood Nuisance team asked for a condition relating to lighting to be agreed by the Local Planning Authority. Although no lighting is proposed, should lighting be required, it could harm the amenity of the neighbour and local wildlife. As such, a condition related to lighting will be included.

A third party objection raised concerns with the application process and advice offered by the Council to the applicant. Section 73A of Town and Country Planning Act 1990 allows the applicant to make a retrospective planning application and include development as part of the application. Irrespective, the determination of the application is not affected by the fact the application is made retrospectively and the decision is based on local and national planning policies.

CONCLUSION

Policy CS10 of the Core Strategy (2011) and paragraph 85 of the NPPF (2021) are supportive of rural businesses. The retention and siting of storage containers for the use of local businesses and communities within Stoke Ferry Timber Yard is considered to meet

national and local planning policies, whilst having minimal impact on the character of the yard and on neighbour amenity. Furthermore, the application received no objections from statutory consultees subject to conditions.

The proposal therefore complies with policies CS02, CS06, CS08 and CS10 of the Core Strategy (2011), policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016), and provisions within the NPPF (2021), and is recommended for approval.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be carried out using only the following approved plans:

dwg no. 01A/CB/02/2023 (dated 16-01-2023)
dwg no. 03/CB/03/2023 (dated 18-01-2023)
dwg no. 02c/CB/15/2024 (dated 08-04-2023)
dwg no. 02d/CB/17/2023 (dated 28-04-2023)

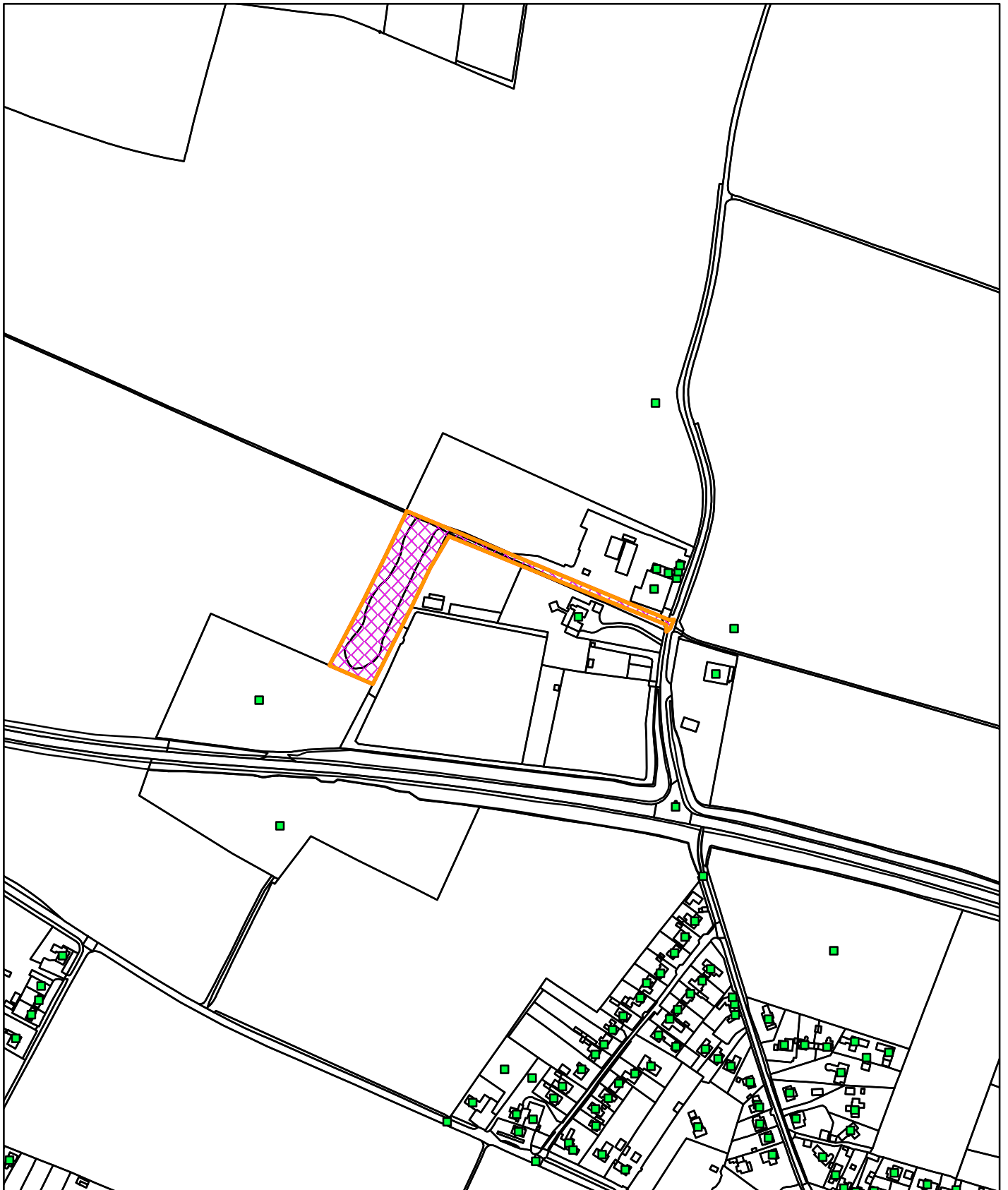
- 1 Reason: For the avoidance of doubt and in the interests of proper planning.
- 2 Condition: Prior to the installation/construction of any external lighting a detailed outdoor lighting scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the type of lights, the orientation/angle of the luminaries, the spacing and height of the lighting columns, the extent/levels of illumination over the site and on adjacent land and the measures to contain light within the curtilage of the site. The scheme shall be implemented in accordance with the approved scheme and thereafter maintained and retained as agreed
- 2 Reason: In the interests of minimising light pollution and to safeguard the amenities of the locality in accordance with the NPPF.
- 3 Condition: Within 3 months of the date of this permission, the surface water drainage provision will be constructed as per drawing 02d/CB/17/2023 (dated 28-04-2023). The drainage shall be designed and constructed in accordance with BRE Digest 365 (or CIRIA Report 156).
- 3 Reason: To ensure that there is a satisfactory means of drainage in accordance with the NPPF.
- 4 Condition: Within 3 months of the date of this permission, the acoustic fence shall be erected in accordance with the details specified on dwg no. 02c/CB/15/2024 (dated 08-04-2023) and shall thereafter be maintained and retained in the approved location.
- 4 Reason: To ensure that the work is carried out within a reasonable period and to ensure that the fence is maintained and retained in order to protect the amenities of the neighbour in accordance with Policy DM15 of the Site Allocations and Development Policies Plan (2016) and the NPPF.
- 5 Condition: The use of the storage containers shall only be used between the hours of 08.00 and 17.00 Monday to Friday, 08.00 to 12.00 on Saturdays and at no time on

Sundays, Bank or Public Holidays. Visits to the storage units outside of these hours will be in exceptional circumstances only, and in strict accordance with the approved Noise Management Plan, approved under condition 6.

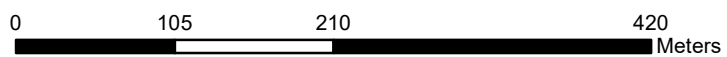
- 5 Reason: In order that the Local Planning Authority may retain control over the development in the interests of the amenities of the locality in accordance with the NPPF.
- 6 Condition: Within 2 months of the date of this permission, a noise management plan shall be submitted to the Local Planning Authority to be agreed in writing by the Local Planning Authority. Within 1 month of the date of the details agreed in writing, the noise management plan shall be implemented as approved.
- 6 Reason: To ensure a plan is submitted within a reasonable period and to protect the amenities of the neighbour in accordance with Policy DM15 of the Site Allocations and Development Policies Plan (2016) and the NPPF.
- 7 Condition: There shall be a maximum of 30 shipping containers on the site at any one time.
- 7 Reason: In the interests of the amenities of the locality in accordance with the NPPF.

23/00125/CU

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Parish:	Stoke Ferry	
Proposal:	A change of use from an agricultural lorry park and outside storage area to a commercial outside storage area for construction materials and Items connected with Stoke Ferry Timber Ltd. Lorry parking is not applied for. (Retrospective)	
Location:	Stoke Ferry Timber Ltd Boughton Road North Stoke Ferry KINGS LYNN	
Applicant:	Mr Colin Bond	
Case No:	23/00125/CU (Change of Use Application)	
Case Officer:	Helena Su	Date for Determination: 29 March 2023 Extension of Time Expiry Date: 9 June 2023

Reason for Referral to Planning Committee – Called in by Cllr Sampson

Neighbourhood Plan: No

Case Summary

The application is for the retrospective change of use of historic lorry park for commercial storage for Stoke Ferry Timber. No operational development has occurred, or is proposed to take place, for the change of use.

The application site is Stoke Ferry Timber Yard which is located outside of Stoke Ferry's development boundary by approx. 230m. The whole site is approx. 2.9ha in size and consists of a number of buildings related to local businesses. The application site is located to the west of the commercial yard of Stoke Ferry Timber and is approx. 0.83ha, including an existing concrete track along the south of the yard.

Stoke Ferry is classified as a Key Rural Service Centre (KRSC) under the settlement hierarchy of Policy CS02 of the Core Strategy (2011). The application site is outside of the development boundary and therefore treated as countryside.

Key Issues

- *Principle of Development
- *Impact on the landscape
- *Impact on Neighbour Amenities
- *Wildlife, ecology and biodiversity
- *Highway Safety
- *Flood Risk
- *Other material considerations

Recommendation: APPROVE

THE APPLICATION

The application is for the retrospective change of use of a historic 'lorry park' to commercial storage for Stoke Ferry Timber. No operational development has occurred, or is proposed to take place, to facilitate the change of use.

This application is one of two planning applications and one lawful development certificate recently applied for on this site. The other planning application relate to part-retrospective storage containers on the site (planning ref: 23/00092/F). Both planning applications have been called in to Planning Committee.

The application site is Stoke Ferry Timber Yard which is located outside of Stoke Ferry's development boundary by approx. 230m. The whole site is approx. 2.9ha in size and consists of a number of buildings related to other local businesses. The application site is located to the west of the commercial yard of Stoke Ferry Timber and is approx. 0.83ha, including an existing concrete track along the south of the yard. The storage area makes up 0.4ha of the site.

There is no planning history on the subject land, but aerial imagery shows the land was used as a lorry park in the 1990s, associated with the previous potato farm business on the site. Since the 1990s, the site appears to be used for sporadic storage of items, such as vehicles, and waste.

The development relates to expanding the storage of an existing commercial timber yard. The planning agent and applicant stated that Stoke Ferry Timber often has bulk deliveries where outside storage away from the retail area of the yard is required to enable sorting, batching, and grading for display. The storage area proposed also allows for secure and fast unloading away from the public areas of the yard, but near the point of sale.

SUPPORTING CASE

None submitted to date.

PLANNING HISTORY

23/00125/CU: Pending Consideration - A change of use from an outdoor storage area and lorry park for agricultural use, to an outdoor storage area for commercial and personal use (retrospective)

23/00092/F: Pending Consideration - The siting of 30 '20 foot' containers for local storage (Part-retrospective)

23/00039/LDE: Would/Was Lawful - Application for a lawful development: To continue use of the buildings for an engineering company like it has been used for over 10 years

09/00136/CU: Application Permitted: 15/06/09 - Change of use of agricultural building and yard to storage and distribution of timber including trade counter (Committee Decision)

09/01866/F: Application Permitted: 08/02/10 - Variation of Condition 6 of Planning Permission 09/00136/CU revising siting of acoustic fence (Committee Decision)

2/99/0462/F: Application Permitted: 20/05/99 - Construction of office extension and vehicle workshop

2/96/0294/F: Application Permitted: 13/05/96 - Demolition of existing office and construction of enlarged office

2/93/1185/F: Application Permitted: 02/11/93 - Construction of agricultural store building (Committee Decision)

RESPONSE TO CONSULTATION

Parish Council: SUPPORT

Highways Authority: NO OBJECTION

With reference to the amendment consultation and confirmation that this area would be for storage associated with Stoke Ferry Timber Ltd. On balance we would not have an objection to such a use being connected to the existing business.

Environmental Agency: NO OBJECTION

Waste and Minerals: The site is not on a Mineral Safeguarding Area, nor does it fall within the consultation area of any existing mineral site or waste management facility, or the consultation area of any allocated mineral extraction site. Therefore, Norfolk County Council in its capacity as the Mineral and Waste Planning Authority has no specific comments on this site

Environmental Quality (Land): NO OBJECTION

Ministry of Defence: NO SAFEGUARDING OBJECTIONS

The application site occupies the statutory safeguarding zones surrounding RAF Marham - in particular, the aerodrome height, technical and birdstrike safeguarding zones surrounding the aerodrome - and it is approximately 8.59km from the centre of the airfield.

After reviewing the application documents, I can confirm the MOD has no safeguarding objections to this proposal.

Community Safety and Neighbourhood Nuisance: NO OBJECTIONS Subject to conditions relating to drainage, the acoustic fence, opening hours, lighting and submission of a noise management plan. Also asked for if a condition relating to the ownership of lorries be included so that the use applies to vehicles owned by the business to ensure that the number of vehicles does not become excessive, and so that if it was sold in future, it wouldn't automatically have consent to be a commercial lorry park without further conditions being added.

Ecology Officer: NO OBJECTIONS

Hedgerow are an ecologically valuable habitat which do have the potential to support a variety of different species. However, given the context of the proposed development I do not believe there will be any significant adverse impacts to protected species or habitats as a result of the proposal at Stoke Ferry Timber.

As discussed this advice is based on the following factors:

Planning Committee
5 June 2023

- The track that will be used for access is already in use. Any wildlife currently using the hedgerow will be habituated to the conditions under which they are already accustomed. The increased use of the track, which will not be significant, as a result of the development will therefore not significantly impact wildlife.
- The timber yard operates within normal business hours i.e. daylight hours. There will therefore be no feasible impacts to nocturnal species currently utilising the hedgerow i.e. owls and small mammals during the construction phase. Should additional lighting be required for the security of the new development it should be cowled or face away from the hedgerow where possible to avoid potential impacts during the operational phase of the development.

REPRESENTATIONS ONE in OBJECTION, summarised as follows:

- This planning application is listed as retrospective, which is untrue. In August 2022, a complaint was made to the Council's Enforcement team about area D being used as a dumping ground area for the application and his self storage tenants, a huge bonfire site, a scrap yard for the applicant's dead cars and household white goods. Area D was neither being used as a lorry park, nor for agricultural/commercial storage.
- There is no indication as to the number of lorries that might be parked there.
- the term 'commercial storage' is a nebulous term, that if approved would provide legitimacy to the storage of anything from builders waste to composition operations.
- The site is accessed using the entrance to the timber yard from Boughton Road along the concrete Road that runs just 1m away and all the way along the boundary of the neighbouring dwelling. This would create noise, vehicle fumes, dust and distress to the neighbours and wildlife in the boundary hedge.
- Propose that any permission regarding parking of lorries should be restricted to the number of vehicles owned by the applicants and ancillary to their business; no unrelated vehicles; opening hours; non-retail area; no permanent artificial lighting; the area to remain ancillary and retained within a single planning unit.

SIX in SUPPORT, summarised as follows:

- There has always been a timber business in Stoke Ferry. It is very convenient for the local village.
- There will be less noise than if this was being used as agricultural, with HGV lorries running 24 hours a day.
- The location is set away from the main settlement and unlike the large industrial complex at the centre of the residential community where it was built without proper planning permission. This modest change of use on the periphery can only be seen as advantageous for the local economy.
- A well-established business and well supported by local businesses. It provides good employment opportunities for local people. It has not had any problems in the past so should be encouraged.
- The storage area will offer more employment to our local area, cutting down on exhaust emissions. The storage area will offer a great service to the local area and I can't see any detrimental effect on the local area.
- My experience is noise level is exceptionally quiet.
- Stoke Ferry has had a wood yard for many years without any problems. I cannot see any problems at all changing from agricultural to commercial. It is quiet and reliable business.
- Stoke Ferry Timber have been more than helpful over a number of years, working with local businesses. The management team and account secretary have always been polite and willing to help. There is a strong professional attitude towards their customers

and they are a great asset to the village. The planning application put forward by Stoke Ferry Timber will help others.

LDF CORE STRATEGY POLICIES

CS01 - Spatial Strategy

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS10 - The Economy

CS12 - Environmental Assets

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)
Planning Practice Guidance (PPG)
National Design Guide 2021

PLANNING CONSIDERATIONS

The main considerations are:

- Principle of Development
- Impact on the Landscape
- Impact on Neighbour Amenity
- Wildlife, ecology and biodiversity
- Highway Safety
- Flood Risk
- Other material considerations

Principle of Development:

Stoke Ferry is classified as a 'Key Rural Service Centre' (KRSC) within the settlement hierarchy under Policy CS02 of the Core Strategy. The application site is outside of Stoke Ferry's development boundary and under Policy DM2 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016), will be treated as countryside.

Paragraph 84 of the NPPF (2021) states that planning decisions should enable the sustainable growth and expansion of all types of businesses in rural areas and development and diversification of agricultural and other land-based rural businesses.

This is reiterated in Policy CS10 of the Core Strategy (2011), which states that permission may be granted on land which might not otherwise be appropriate for development for an employment generating use which meets a local business need, where it is appropriate in

size and scale to the local area, adjacent to the settlement, and the development and use would not be detrimental to the local environment or local residents.

Subject to considerations of the detailed criteria in this report, the principle of development, to use this area for storage related to Stoke Ferry Timber, is considered to comply with Policy CS10 of the Core Strategy (2011), and provisions within the NPPF.

Impact on the Landscape:

The application site is located to the west of the commercial yard of Stoke Ferry Timber Yard. The site is defined by an approx. 1.8m tall green mesh fencing along the north and east boundary of the storage area. Within the mesh fence to the north of the area, is a 2.5m tall green mesh gate. Along the southern boundary are trees and an earth bund to the west boundary. No building operations are proposed to facilitate this change of use.

The commercial storage in this location would largely be screened by the landscaping to the south and west, which screens views from the A134, and the commercial yard, which screens views from Boughton Road to the east.

In summary, the change of use would have limited impact on the character of the area. The fence and gate which has been erected on the site is of a design which does not adversely impact the rural setting.

On impact on the landscape, the proposal is therefore considered to comply with policies CS06 and CS08 of the Core Strategy (2011) and Policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP)(2016)

Impact on Neighbour Amenity:

There is one residential neighbour (Boughton Farm) located approx. 134m to the east of the application site.

Due to the location and nature of the application, the impacts on the neighbour would come from the use of the land and potential for noise and disturbance.

The concrete track leading to the storage site is located to the north of Boughton Farm's boundary, where there is an acoustic fence, along the eastern section of the boundary, and hedge to the rest of the boundary. The neighbour had raised concerns with noise resulting from the use of the concrete track to access the storage area.

The acoustic fence along the south boundary of the yard is now proposed to be extended by approx. 106m and will screen beyond the length of Boughton Farm's domestic curtilage by approx. 21m. The Community Safety and Neighbourhood Nuisance team (CSNN) have not raised any concerns related to the erection of the acoustic fence, which would mitigate potential noise implications of using the concrete track to the storage area. The erection of the acoustic fence would therefore be conditioned. Furthermore, the storage area would be used for the storage related to Stoke Ferry Timber and used during the permitted hours of the Yard. This again will be covered via condition.

Therefore, impact on neighbour amenity is considered to be minimal and comply with Policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP) (2016).

Wildlife, ecology and biodiversity:

A third party commented that the use of the access may impact wildlife and nesting bird in the boundary hedge, and light pollution would frighten owls in the surrounding areas.

The Council's Ecologist said that given the context of the development, it is unlikely there would be a significant adverse impact to protected species or habitats. The concrete track is already in use. Any wildlife using the hedgerow will be habituated to the conditions which they are already accustomed and therefore the increased use of the track, which will not be significant, and will not significantly impact wildlife. Additionally, the timber yard operates within normal business hours. There will therefore be no feasible impacts to nocturnal species currently utilising the hedgerow

Furthermore, no lighting is proposed with the scheme. The Ecologist has said that should additional lighting be required for the security of the new development it should be cowled or face away from the hedgerow where possible to avoid potential impacts during the operational phase of the development. As it is considered there is potential for external lighting to adversely affect wildlife, it is sufficiently necessary that the details of any lighting to be installed shall be submitted to the local planning authority for consideration.

On this basis, the proposal complies with policies CS06, CS08 and CS12 of the Core Strategy (2011) and policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP) (2016).

Highway Safety:

The development would use the existing entrance and track to the northwest of Stoke Ferry Timber to access the storage area. Considering this, the Local Highway Authority had no objections to the development, subject to the site being used in connection with the existing commercial yard. A condition will be included to ensure the storage area is held in connection with the existing commercial yard.

On this basis, the proposal complies with policies CS08 and CS11 of the Core Strategy (2011) and DM15 of the Site Allocations and Development Management Policies Plan (SADMPP) (2016).

Flood Risk:

The site is located within Flood Zone 1 of the Strategic Flood Risk Assessment 2018 and is therefore at lowest risk of flooding. Furthermore, the Environmental Agency had no objections.

Other Material Considerations:

There are no objections received or conditions required from MOD Safeguarding, Waste and Minerals team, and Environmental Quality.

There appears to be some confusion from the third party comment in objection that the site will be used as a lorry park. The site is not proposed to be used to store lorries, but for the storage of items related to Stoke Ferry Timber.

The Community and Safety Neighbourhood Nuisance team (CSNN) asked for a condition restricting the area to be used by lorries in the ownership of Stoke Ferry Timber. However, the application is for a storage area and not a lorry park. Therefore, such condition is not

considered necessary. A condition will be included to ensure the storage area is held in conjunction to Stoke Ferry Timber Yard only.

CONCLUSION

Policy CS10 of the Core Strategy (2011) and paragraph 84 of the NPPF (2021) are supportive of development and expansion of rural businesses in the countryside. The proposed storage area would be used by an existing commercial timber yard, to enable sorting, batching, and grading for display of stock away from the retail area of the yard. The change of use would have limited landscape and visual impact, on the neighbour and local wildlife, with no objections by any statutory or non-statutory consultees.

The proposal therefore complies with policies CS02, CS06 and CS10 of the Core Strategy (2011), policy DM15 of the Site Allocations and Development Management Policies Plan (SADMPP) (2016), and provisions within the NPPF, and is recommended for approval.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

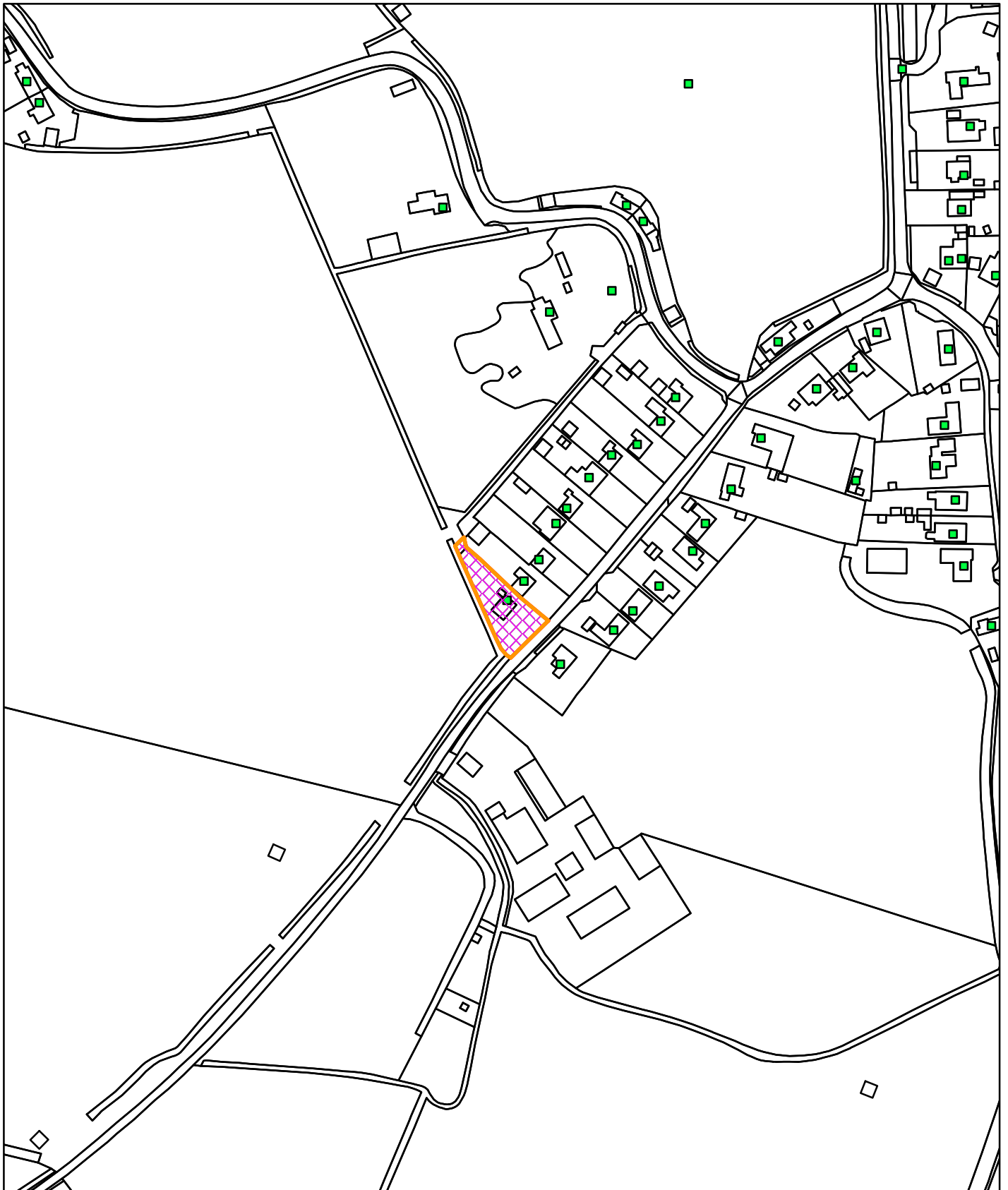
- 1 Condition: The development hereby permitted shall be carried out using only the following approved plans:

dwg no. 04e/CB/20/2023 (dated 18/05/2023)
- 1 Reason: For the avoidance of doubt and in the interests of proper planning.
- 2 Condition: The storage area hereby permitted shall be held in conjunction with Stoke Ferry Timber Ltd (outlined in blue on dwg no. 04e/CB/20/2023 (dated 18/05/2023)), and shall only be used for commercial storage in association with Stoke Ferry Timber and at no times shall be used for personal use.
- 2 Reason: For the avoidance of doubt and to safeguard the amenities of the locality in accordance with the NPPF.
- 3 Condition: The use of the land hereby permitted shall only be used between the hours of 08.00 and 17.00 Monday to Friday, 08.00 to 12.00 on Saturdays and at no time on Sundays, Bank or Public Holidays.
- 3 Reason: In order that the Local Planning Authority may retain control over the development in the interests of the amenities of the locality in accordance with the NPPF.
- 4 Condition: Prior to the installation/construction of any external lighting a detailed outdoor lighting scheme shall be submitted to and approved in writing by the Local Planning Authority. The scheme shall include details of the type of lights, the orientation/angle of the luminaries, the spacing and height of the lighting columns, the extent/levels of illumination over the site and on adjacent land and the measures to contain light within the curtilage of the site. The scheme shall be implemented in accordance with the approved scheme and thereafter maintained and retained as agreed.

- 4 Reason: In the interests of minimising light pollution and to safeguard the amenities of the locality in accordance with the NPPF.
- 5 Condition: Within 2 months of the date of this permission, a noise management plan shall be submitted to the Local Planning Authority to be agreed in writing by the Local Planning Authority. Within 1 month of the date of the details agreed in writing, the noise management plan shall be implemented as approved.
- 5 Reason: To ensure a plan is submitted within a reasonable period and to protect the amenities of the neighbour in accordance with Policy DM15 of the Site Allocations and Development Policies Plan (2016) and the NPPF.
- 6 Condition: Within 3 months of the date of this permission, the acoustic fence shall be erected in accordance with the details specified on dwg no. 04e/CB/20/2023 (dated 18/05/2023) and shall thereafter be maintained and retained in the approved location.
- 6 Reason: To ensure that the work is carried out within a reasonable period and to ensure that the fence is maintained and retained in order to protect the amenities of the neighbour in accordance with Policy DM15 of the Site Allocations and Development Policies Plan (2016) and the NPPF.

22/00265/F

10 Folgate Lane Walpole St Andrew PE14 7HY



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23/05/2023

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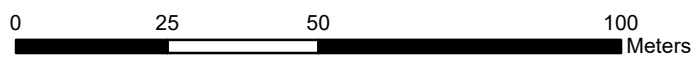


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23/05/2023

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Parish:	Walpole	
Proposal:	Change of use of bungalow from a dwelling (C3) to a children's home (C2) for up to two children	
Location:	10 Folgate Lane Walpole St Andrew Wisbech Norfolk	
Applicant:	Mr T Dumitru	
Case No:	23/00265/CU (Change of Use Application)	
Case Officer:	Clare Harpham	Date for Determination: 18 April 2023 Extension of Time Expiry Date: 12 June 2023

Reason for Referral to Planning Committee – The application has been called in to the Planning Committee by the Assistant Director of Environment and Planning due to the scale of objections. While not formally objecting the Parish Council raised questions of concern.

Neighbourhood Plan: No

Case Summary

The application site is located within the development boundary of Walpole St Andrew which is classified as a Rural Village within Policy CS02 of the Core Strategy 2011.

The site is located along Folgate Lane, at the end of a row of single storey dwellings, before the lane leads out into open countryside. On site is an existing dwelling of prefabricated construction.

The proposal seeks the change of use of the existing dwelling (C3) to a children's home (C2) for up to two children. The proposed number of children was reduced during the course of the application from 'up to three' and an amended description advertised.

Key Issues

Principle of development
 Form and character
 Impact on neighbour amenity
 Highway safety
 Flood risk
 Any other matters requiring consideration prior to determination of the application
 Crime and Disorder

Recommendation

APPROVE

THE APPLICATION

The application site is located on the northern side of Folgate Lane and is the last dwelling adjacent to the open countryside. On site stands a single storey prefabricated bungalow and at the time of the site visit it was evident that two new access points had been formed onto Folgate Lane along with some fencing and gates and a parking area. Along the northern side of Folgate Lane most of the dwellings have vehicular access to the rear of the dwellings, although some dwellings have also got a vehicular access to the front, directly on to Folgate Lane.

This application is for the change of use of the existing bungalow from a dwelling (Class C3) to a children's home (C2 for up to two children).

SUPPORTING CASE

The following sets out a supporting summary of the application: -

The proposal seeks permission for the change of use of an existing two-bedroom bungalow to a children's home to provide supervised residential accommodation for up to two children with associated non-resident adult supervisors.

The application has been amended from 'up to three children' to two children only.

Members will appreciate that the application should be considered on planning merit alone: The change of use relates to an existing residential property to a very small children's home within, albeit at the edge of, a village location.

The scale of use is not dissimilar to a small family home (two adults and two children).

Significant concern by local residents has been raised with regard to the 'type' of children to be homed at the property. However, it will be appreciated that the planning change of use is only the start of any registration process through Social Services and Ofsted.

All that is known is that the proposed children's home would not comprise a 'secure unit'. It will be for Social Services to determine which children requiring a residential placement would be appropriate and should be accommodated in this rural location.

The children would be aged between 11 and 17 years of age and would be transported by the care provider to schooling (Wisbech or King's Lynn) plus any extracurricular activities such as sports, social and leisure trips etc.

With regard to potential anti-social behaviour, Norfolk Constabulary has not expressed any concern regarding this aspect of the proposal.

From a planning perspective, given the scale of the change of use and the fact that two overseeing adults would be present at the site at any one time, it is not considered that the proposal would give rise to any significant 'nuisance', but, if this was to occur, there are separate legislative means to tackle such behaviour.

The proposal has generated significant concern and objection by local residents, fearing crime, anti-social activity and lack of facilities available locally to cater for the children to be homed. However, it is pointed out that the children requiring homes within a principally rural location would have ready access to transportation for their schooling and other needs not

catered for locally. Social Services will take into account the rural and relatively inaccessible location of the children's home as part of the registration and placement arrangements.

Some children require rehoming through no fault of their own.

The proposal represents a low-key use of an existing dwelling for much-needed accommodation and of a scale similar to a family dwelling.

In planning terms, the proposal would accord with National and local planning policies.

PLANNING HISTORY

No recent planning history

RESPONSE TO CONSULTATION

Parish Council: At the time of writing this report the Parish Council had raised questions, but not expressed whether they support or object to the proposal.

- Questions were raised regarding the suitability of such a rural area with few facilities.
- Concerns have been raised by members of the public regarding issues caused by another 'children's home' in the village and whether the children will be young offenders.

Highways Authority: NO OBJECTION

I have no objection to the principle of the change of use application; however, I did observe that some works at the site have started to provide a new access and parking for the use that would currently not accord with highway requirements.

Firstly, the fencing and gates are erected forward of the frontage boundary and encroach on the highway verge. These features would need to be taken down and re-erected on the correct line. The boundary should be in line with the roadside edge of the adjacent ditch.

There is also two new access points that are being formed with gates and the new points of access need to be hard surfaced back to the gates which should be set back 5m from the near edge of the carriageway. I am of the view that these aspects can be covered by conditions which I recommend be applied to any decision.

Internal Drainage Board: NO OBJECTION

The applicant proposes to discharge surface water to a sewer which is the least sustainable method of surface water disposal within the drainage hierarchy. I recommend you satisfy yourself that this is viable in this location. Foul drainage is proposed to a main sewer, should the proposal change and treated foul water be disposed of to a watercourse, consent is required under Byelaw 3. I note the presence of a Board maintained watercourse (DRN146P0296 – Folgate Drain) adjacent to the western boundary. While not proposed should the applicant's proposal change to include alterations to the watercourse consent would be required under Byelaw 4 and the Land Drainage Act 1991. No works are currently proposed within 9m of the Board maintained watercourse. Should there be any proposed works within 9m of the drain (for instance fencing) then consent will be required under Byelaw 10.

Norfolk Constabulary: NO OBJECTION

The agent has indicated that their client will consider incorporating Secured by Design principles. General advice given regarding lighting, natural surveillance etc.

REPRESENTATIONS

THIRTY TWO letters of **OBJECTION** from twenty two different people covering the following:-

- Unsuitable due to demographics of the area, Folgate Lane is predominantly retired people, with many living alone.
- The elderly feel intimidated by youths.
- The Local Authority have a duty to safeguard vulnerable elderly people.
- Concern regarding the age of proposed children (11 to 17 year olds).
- Applicant's statement says children will be given support to manage their behaviours and support will be given so then can step-down into foster care or safety return home. These may be troubled teens and this will be a half-way house.
- Will there be staff present at all times?
- Fear of crime. There is another children's home approx. 0.5km from this site and this has caused issues. Vandalism and damage to property as well as verbal abuse.
- There is evidence to show fear of crime is based in reality with the Office for National Statistics (ONS) finding in December 2022 that 'more than 52% of children in had a criminal record by the age of 24.
- Children with BESD (Behavioural Emotional and Social Difficulties) is more prevalent among young people who have interacted with the criminal justice system. 1 in 5 will have been excluded from school and 4 in 5 suspended (ONS).
- Noise and disturbance caused in a very quiet area by troubled youths as well as the disturbance cause by staff arriving / leaving the site.
- Road is narrow and so changeover of staff, deliveries and social care workers etc will cause disruption and disturbance.
- The location is unsuitable, quiet rural location with a narrow dark lane with no streetlights or footpaths (heavy farm traffic often going past).
- Nothing in the area for young people to do, no footpaths, amenities and limited bus service. No local school for 11-17 year olds.
- This could lead to lack of socialisation which could affect children's mental/physical health.
- Have worked with vulnerable young people and would not house them in this type of accommodation or this rural location.
- There is planning approval for a lithium battery storage facility within 0.5km with all the noise and light pollution that will bring.
- The dwelling is a family home and not a place of business or work, which will affect the character of the area.
- The building is unsuitable for the purpose proposed.
- The building is prefabricated and originally built as temporary accommodation to last for 20 years (possible asbestos).
- This building and other prefabs in Folgate Lane had long term tenants who were moved out by Freebridge as the bungalows were considered not to be suitable for human habitation, and Freebridge then sold them. How are they therefore suitable for children to live in?
- Concerns that other bungalows have been bought by the same applicant for the same purpose.
- Size is inadequate, this is only a two-bedroom bungalow, how can 2/3 people stay there? Where will staff stay, especially overnight?

- Impact upon surrounding property prices.
- The property will be overseen by NCC and supervised by qualified staff with a large number of people per child, this will be costly for taxpayers.
- Was a site notice put up at the site?
- Works have been carried out at the site without consent, a tall fence was erected with gates directly up against the highway (restricts visibility).
- An underground cable was damaged during renovation works and this caused an inconvenience to surrounding dwellings who had to rely on a generator until a transformer was replaced by energy company.
- Works have been done to the drainage at the site.
- Noise and disturbance during renovation works.
- Loss of privacy in the garden.
- The deeds state that the homes on Folgate Lane should not be used for business purposes, who will they meet this restriction?
- Query regarding the letter of support and how Derby differs from a rural village.
- Query regarding staffing levels and how these have changed over course of application.
- Applaud the applicant's intentions, helping young people, but unsure it is practical.

ONE letter of **SUPPORT** covering the following:-

- Last year proposals to convert 3 domestic properties into children's homes in Derby suburbs were met with petitions and letters of objection citing fear of crime, increase in traffic and noise. Two were approved (61 and 90 objections respectively) and one was refused (100 objections).
- The needs of vulnerable children are ignored. Kids are at rock bottom and need compassion.
- Peter Sandiford, chief executive of ICHA (Independent Children's Home Association) spoke to the BBC and highlighted how applications for new care homes are often greeted by hostility by nearby residents. "People often think about themselves rather than valuing children's wellbeing... children seen as criminals not victims treating children in care as 'the other'. Moving children out of area to other placements can make them pray to abuse.'
- Demand outstrips residential places and care homes are needed.
- Having worked in 6 children's homes in Derby, can attest to relaxed relationship enjoyed between the kids in care and the local residents.

LDF CORE STRATEGY POLICIES

CS02 - The Settlement Hierarchy

CS06 - Development in Rural Areas

CS08 - Sustainable Development

CS10 - The Economy

CS11 - Transport

SITE ALLOCATIONS AND DEVELOPMENT MANAGEMENT POLICIES PLAN 2016

DM2 – Development Boundaries

DM15 – Environment, Design and Amenity

DM17 - Parking Provision in New Development

NATIONAL GUIDANCE

National Planning Policy Framework (NPPF)

Planning Practice Guidance (PPG)

National Design Guide 2021

National Planning Policy Framework – sets out the Government’s planning policies for England and how these are expected to be applied.

National Planning Practice Guidance - Provides National Planning Practice Guidance, in support of and in addition to the NPPF

PLANNING CONSIDERATIONS

The main considerations are:

- Principle of development
- Form and character
- Impact on neighbour amenity
- Highway safety
- Flood risk
- Any other matters requiring consideration prior to determination of the application
- Crime and Disorder

Principle of Development:

The application site is within the development boundary of Walpole St Andrew as identified within the Site Allocations and Development Management Policies Plan 2016.

The proposal is to change the use of the existing bungalow (Use Class C3) to Use Class C2 (residential use with care). It is important to note that Use Class C3 (Dwellinghouses) includes within it Use Class 3(b) which states ‘not more than six residents living together as a single household where care is provided for residents.’

Case Law as summarised within *Hinckley & Bosworth BC v Chartwell Care Limited* (2011) states that care homes more typically fall under Use Class C2 as use ‘for the provision of residential accommodation and care to people in need of care.’ And the definition of ‘care’ as set out within Article 2 of the Use Class Order includes, ‘the personal care of children.’ There is therefore some overlap between Use Class C2 and C3 in so far as they relate to care homes. Therefore, for a proposal to require planning permission, the change of use has to be a material change of use and this applies to the overall character of the use and not just certain aspects of it.

Therefore, in order to determine whether planning permission is required the council must determine whether the proposed use would fall within Use Class C2 and whether that change of use would be material.

The case of *North Devon DC v FSS and Southern Childcare Ltd* (2003) confirmed the view that carers who provide 24-hour care, but who are not resident, could not be regarded as living together in a household (which would be required in the context of Use Class C3(b)). Therefore, in the case of this application, where the applicant states there will be staff present on site, working in shifts, the use is considered to fall within Use Class C2. The use

Planning Committee
5 June 2023

at the site is self-limiting given the size of the existing bungalow, however given the fact that staff will be present at the site, working in shifts, it is considered that a material change of use could occur and this will be discussed below.

The change of use of the existing bungalow (Use Class C3) to a children's home (Use Class C2) is acceptable in principle and would comply with the principles of the NPPF, Policy CS02, CS06, CS08, and CS10 of the Core Strategy 2011 and Policy DM2 and DM15 of the SADMPP 2016.

Form and Character:

The proposal is for the change of use from a dwelling (C3) to a children's home for 2 resident children (C2). No physical changes to the existing dwelling are proposed as part of this application and the development is therefore unlikely to pose significant impact on the form and character of the area.

It was evident from a site visit carried out on 24th February that two new points of access and driveway/parking area had been constructed to the front of the application site onto Folgate Lane, however these do not require planning consent as Folgate Lane is not a classified road and so this can be carried out under Class B, Part 2, Schedule 2 of the Town and Country Planning (General Permitted Development) Order 2015 as amended (consent is required from the Highways Authority).

In addition, fencing and gates had been erected, both of which exceeded 1.0m in height and was adjacent to the highway and therefore required planning permission. Following discussion with the agent it was confirmed that the fencing and gates would be relocated and lowered to no more than 1.0m which would not require planning permission (this would comply with Class A, Part 2, Schedule 2 of the Town and Country (General Permitted Development) Order 2015, as amended). The relocation and lowering of the fencing and gates will be conditioned. The agent has also confirmed that the driveway and parking area will be permeable which could therefore be constructed utilizing permitted development rights.

This application is therefore solely for the change of use of the existing dwelling.

An email from the agent dated 29th April, confirms that due to the size of the bungalow (2 bedrooms) there will a maximum of two children resident at any one time (between the ages of 11 and 17). Two full-time supervisors (non-resident) would be at the property at any one time to oversee the children's care, welfare and transport arrangements to school and clubs etc. The supervisors would operate on a 12 hourly rotational shift arrangement. It is noted that the applicant still needs to apply to Ofsted prior to the children's home becoming operational and therefore these arrangements may need to be altered slightly depending upon the needs of the children at the home.

The applicants statement dated 23rd January states that the intention is to offer residential placements for children and young adults who require support to manage their behaviours and keep themselves safe, supporting them to achieve their potential so that they can step down into foster care or safely return home.

A number of objections have been received relating to the location of the proposal given the rural location and lack of amenities with no street lighting, narrow road etc. It is not for the LPA to make a judgement regarding the suitability of the location for looked after children, the proposal is within an existing residential dwelling the occupants of which would also have to rely upon the same local amenities. The applicant still has to go through the process of

registering the site with Ofsted in line with The Children's Homes (England) Regulations 2015, as amended.

Concern has been expressed that the demographics of Folgate Lane are predominantly elderly, retired people who enjoy the quiet rural location and that the home will alter the character of the area, with neighbours feeling intimidated by youths. However, there would be nothing to stop a younger demographic buying the existing dwelling as the road is not restricted to older people, and an assumption cannot be made that the residents (of which there would be only two children who would be supervised) are going to cause issues.

Objections have also been received regarding the fact that the proposal is a business use in a residential location which would alter the character of the lane. While the applicant will no doubt have financial recompense and will employ staff to care for the children at the site, the aim is to provide a nurturing place for the children to live and therefore it would be expected to locate such homes in residential areas, and not in town centre / business / industrial estate location.

Due to the slightly uncertain nature regarding the number of staff (this will be finalised with Ofsted/Social Services) it is considered that a material change of use could occur for this reason and therefore consent is required. However, given the scale of the proposed development, with 2 children (maximum) and 2/4 members of staff, the proposed development is minor in nature and is considered unlikely to pose a significant impact on the immediate vicinity. Whilst there is no footpath, the scale of the proposal is similar to that of a typical dwelling of this size (the number of trip modes above the existing permitted dwelling use is not likely to be significant) and therefore in this instance it is considered acceptable and similar to its existing use.

The proposal therefore complies with the principles of the NPPF, Policies CS02, CS06 and CS08 of the Core Strategy 2011 and Policy DM2 and DM15 of the SADMP 2016.

Impact on Neighbour Amenity:

Given the nature of the proposed development, which is for a change of use only, there would be no impact upon neighbour amenity with regard to overlooking, overshadowing or being overbearing. As already stated, changes to the points of access, the parking area and fencing can be carried out under permitted development rights.

There is an objection relating to loss of privacy in the garden, however the application does not physically alter the building, and it still relates to a single storey dwelling which would not cause material overlooking.

There are objections stating that the change of use is likely to lead to anti-social behaviour, which may be exacerbated by the quiet rural location with children becoming bored. This fear of crime is exacerbated by the experience elsewhere within the village where there is a children's home and there have apparently been issues regarding vandalism, damage to property, and verbal abuse. Whilst fear of crime can have an impact upon well-being and objectors state it is the duty of the Local Authority to safeguard the vulnerable elderly people in Folgate Lane, we cannot assume that the future occupants will cause a nuisance and they will be supervised. The proposal is to provide a nurturing atmosphere for vulnerable children, and it should not be predetermined that the children are going to create a disturbance due to antisocial behaviour. In addition, the small scale of the dwelling limits the number of children at the site.

There are objections that there could be additional noise and disturbance created by the increase in vehicle movements generated by the staff and deliveries etc. Given the level of

staff proposed it is not considered that the number of trips generated would be hugely different from a single dwelling, and while there may be times when staff are changing shifts and there are more cars on site, there is adequate room on site for parking and manoeuvring and this is considered to be acceptable and would not cause a dis-amenity due to noise and disturbance.

Overall, the proposal is not considered to cause amenity issues with regard to noise and disturbance, given the level generated by the proposed use which would not be dissimilar to a single dwelling. The proposal therefore complies with para. 130 of the NPPF and Policy DM15 of the SADMPP 2016.

Highway Safety:

No objection was received from the Local Highway Authority. It was noted by the Highways Officer following a site visit that works had been carried out at the site that would not accord with highways requirements.

The fencing and gates that had been erected would need to be re-erected on land that was not highway verge, and it would also be necessary to upgrade the access and set the gates back 5m from the edge of the highway. It was the Highways Officers view, and the view of your officer that the issues relating to the fencing and access can be resolved by condition.

The proposal complies with para. 110 of the NPPF, Policy CS11 of the Core Strategy 2011 and Policy DM15 of the SADMPP 2016.

Flood Risk:

The site is located within a Flood Zone 3 and the area adjacent to the Board maintained drain on the western boundary is located within a Tidal Hazard Mapping area and an area identified as impacted by climate change with regard to surface water and Tidal 0.1% and 0.5%AEP.

Notwithstanding that a flood risk assessment has been submitted it is material that the flood risk vulnerability of the proposal is no different than its current vulnerability classification as a dwelling (both are classed as 'More vulnerable').

Consequently, there is no change in the vulnerability classification as a result of the change of use. An informative relating to the EA flood warning direct service and the preparation of a Flood Evacuation Plan will be placed on the decision notice as it is in an area affected by flooding.

Other matters requiring consideration prior to the determination of this application:

Surface water drainage and foul drainage is as existing and is not amended by this change of use application.

Comments were received from the IDB relating to their Byelaws and restrictions given the proximity of the Board maintained drain along the western boundary. The letter will be referred to as an informative for the applicant's consideration/information.

A number of objections to the proposal were submitted, many of whom are addressed above, however the suitability of the dwelling has also been questioned given it is of prefabricated construction and was sold by Freebridge as it was not considered suitable as accommodation. The fact that Freebridge did not consider the prefabricated buildings economically viable to upgrade does not preclude somebody else from renovating the

dwelling. It would be for Ofsted when registering the home to decide whether it was suitable accommodation.

There have been objections as to whether the size of the dwelling is adequate for the intended purpose. During the course of the application the number of children proposed at the site was reduced from 3 to 2 given the number of available bedrooms. It is also a matter for the regulating authority i.e. Ofsted to determine whether the size of the dwelling is adequate.

Concern has been expressed that the same applicant has bought other prefabricated bungalows in the road and will also use them as children's homes. If this is the case and there is considered to be a material change of use, then planning permission would also be required. If the change was not considered to be material, as in it was the same level of use as a normal dwelling then consent would not be necessary.

Objections were received relating to works carried out at the site prior to planning permission being sought. This application is for a change of use and any renovations to the dwelling are not impacted by this application. Any issues that arose during the renovations, i.e. damaging an underground cable, are unfortunate and can occur when building work is carried out, but do not relate to this application. The works which were done regarding the points of access, parking and fencing will be addressed via condition.

An objection has been received regarding noise and disturbance caused during the renovation of the dwelling, however this does not relate to this change of use application.

There is an objection that the proposed change of use will impact upon property values. The increase or decrease in value of properties surrounding an application site is not a material planning consideration.

A query has been raised regarding whether a site notice was put up at the site. Two site notices were erected at the site, the first on 24th February and the second one on 4th May after there had been a change in the description of the application reducing the number of proposed children to two. Neighbour/objector letters were also sent out corresponding with the aforementioned dates.

There is an objection on the basis that the deeds relating to the homes on Folgate Lane state that they should not be used for business purposes. This is civil matter and is for the applicant to determine whether they are complying with any other legal requirements relating to their site.

There is an objection on the basis that the home will only support two children and that the number of staff required will not be very cost effective in relation the public paying tax for children's homes. The numbers of staff required to supervise the children at the home will presumably be determined by the regulations relating to children's homes. This is not material to this application.

Crime and Disorder:

There are a number of objections from the neighbouring dwellings relating to 'fear of crime' issues relating to the proposed change of use. The planning application should not be prejudged on this basis as the future occupants are not known at this time and will also be supervised. Comments were received by the Police Architectural Liaison Officer which provided general advice regarding 'Secured by Design' principles.

CONCLUSION

The scale of the proposed C2 use is of a size which would be similar to the existing use of the site as a dwelling (C3). There are no physical changes proposed to the building and therefore there would be no impact on the character and appearance of the countryside. Additionally, while it is possible that there would be slightly more visitors to the site due to staffing over and above a normal dwelling, there would be no material impact on highway safety or neighbour amenity and therefore the proposal is considered acceptable and complies with the principles of the NPPF, Policies CS06, CS08, CS10 and CS11 of the Core Strategy 2011 and Policies DM2, DM15 and DM17 of the SADMPP 2016 and the application is therefore recommended for approval.

RECOMMENDATION:

APPROVE subject to the imposition of the following condition(s):

- 1 Condition: The development hereby permitted shall be begun before the expiration of three years from the date of this permission.
- 1 Reason: To comply with Section 91 of the Town and Country Planning Act, 1990, as amended by Section 51 of the Planning and Compulsory Purchase Act, 2004.
- 2 Condition: The development hereby permitted shall be carried out in accordance with the following approved plans 'Location / Site Plan' received by the Local Planning Authority on 25th April 2023, and 'Floor Plan' received by the Local Planning Authority on 21st February 2023.
- 2 Reason: For the avoidance of doubt and in the interests of proper planning.
- 3 Condition: Within eight weeks of the date of this decision, the fencing and gates that exceed 1.0m in height and encroach upon the highway verge, shall be removed and the land reinstated to its former condition.
- 3 Reason: For the avoidance of doubt and to ensure that the unauthorised development is removed in a timely manner.
- 4 Condition: Prior to the first use of the development hereby permitted the vehicular / pedestrian / cyclist accesses / crossings over the verge shall be constructed in accordance with the highways specification TRAD 5 and thereafter retained at the position shown on the approved plan. Arrangement shall be made for surface water drainage to be intercepted and disposal of separately so that it does not discharge from or onto the highway.
- 4 Reason: To ensure construction of a satisfactory access and to avoid carriage of extraneous material or surface water from or onto the highway in the interests of highway safety.
- 5 Condition: Any access gates / bollard / chain / other means of obstruction shall be hung to open inwards, set back, and thereafter retained a minimum distance of 5 metres from the near channel edge of the adjacent carriageway. Any sidewalls / fences / hedges adjacent to the access shall be splayed at an angle of 45 degrees from each of the outside gateposts to the front boundary of the site.

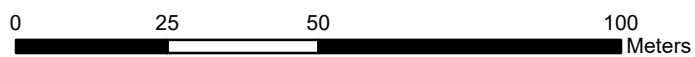
- 5 Reason: In the interests of highway safety enabling vehicles to safely draw off the highway before the gates/obstruction is opened.
- 6 Condition: Prior to the first occupation/use of the development hereby permitted 2.0 metre wide parallel visibility splay (as measured back from the near edge of the adjacent highway carriageway) shall be provided across the whole of the site's roadside frontage (and additionally along the flank frontage of the adjacent property as outlined in blue on the submitted details).The splay(s) shall thereafter be maintained at all times free from any obstruction exceeding 1.02 metres above the level of the adjacent highway carriageway.
- 6 Reason: In the interests of highway safety in accordance with the principles of the NPPF.

2/TPO/00647

41 Greevegate Hunstanton Norfolk PE36 6AF



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25/05/2023

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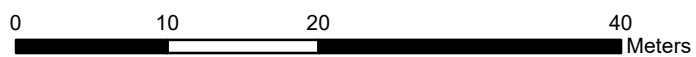


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25/05/2023

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AGENDA ITEM NO: 10/3(a)

Parish:	Hunstanton	
Purpose of report:	TO CONSIDER WHETHER TREE PRESERVATION ORDER 2/TPO/00647 SHOULD BE CONFIRMED, MODIFIED OR NOT CONFIRMED IN THE LIGHT OF OBJECTIONS	
Location:	41 Greevegat Hunstanton Norfolk PE36 6AF	
Case No:	2/TPO/00647	
Grid Ref:	567550 341071	Date of service of Order: 14 December 2022

RECOMMENDATION - CONFIRM ORDER WITHOUT MODIFICATION

The Site

The two Cypress trees occupy part of the southern boundary of a garden which wraps around the north of number 43 Greevegat. The mature trees are established and it is likely that large trees would have been part of the gardens in that area for many years. The trees make a valuable landscape contribution.

Background Information

It is understood that the owners at No.41 and the occupants of No.43 do not have a good relationship. The trees are one source of dispute and by serving a TPO issues relating to the trees can be dealt with in an even manner by The Borough's Tree Officer and on Arboricultural merit which is hoped will dampen any ill feelings between the two parties.

Reason for Placing the TPO

The owner of the trees met with the former Arboricultural Officer, Richard Fisher, on site in 2022 and he was of a mind that the trees should be retained. The TPO was not served as Mr Fisher was soon to leave The Borough.

Jonathan Bundock, acting Tree Officer, visited the owners in December 2022 following an application from the neighbours at No.43 to prune the overhanging branches back to the boundary. This was deemed excessive and would have led to a one-sided and unsightly appearance. A TPO was prepared in order to better manage any future works to the trees and also to ensure future tree cover in that area should the current Cypress trees become over-mature or decline.

RECOMMENDATION: CONFIRM ORDER WITHOUT MODIFICATION

Background Papers

TPO file reference: 2/TPO/00647

Appendix 1: Copy of scoring assessment

TREE EVALUATION METHOD FOR PRESERVATION ORDERS - TEMPO

SURVEY DATA SHEET & DECISION GUIDE

Date: 7-12-22 Surveyor: J BUNDOCK

Tree details
 TPO Ref (if applicable): Tree/Group No: 41 Species: 2x Heyland Cypress
 Owner (if known): Location: 41 GREENEGATE, HUN.

REFER TO GUIDANCE NOTE FOR ALL DEFINITIONS

Part 1: Amenity assessment

a) Condition & suitability for TPO

- | | |
|--------------------------|-------------------------|
| 5) Good | Highly suitable |
| 3) Fair/satisfactory | Suitable |
| 1) Poor | Unlikely to be suitable |
| 0) Dead/dying/dangerous* | Unsuitable |

Score & Notes
3

* Relates to existing context and is intended to apply to severe irremediable defects only

b) Retention span (in years) & suitability for TPO

- | | |
|-----------|-----------------|
| 5) 100+ | Highly suitable |
| 4) 40-100 | Very suitable |
| 2) 20-40 | Suitable |
| 1) 10-20 | Just suitable |
| 0) <10* | Unsuitable |

Score & Notes
4

*Includes trees which are an existing or near future nuisance, including those clearly outgrowing their context, or which are significantly negating the potential of other trees of better quality

c) Relative public visibility & suitability for TPO

Consider realistic potential for future visibility with changed land use

- | | |
|---|---------------------|
| 5) Very large trees with some visibility, or prominent large trees | Highly suitable |
| 4) Large trees, or medium trees clearly visible to the public | Suitable |
| 3) Medium trees, or large trees with limited view only | Suitable |
| 2) Young, small, or medium/large trees visible only with difficulty | Barely suitable |
| 1) Trees not visible to the public, regardless of size | Probably unsuitable |

Score & Notes
3

d) Other factors

Trees must have accrued 7 or more points (with no zero score) to qualify

- | |
|--|
| 5) Principal components of formal arboricultural features, or veteran trees |
| 4) Tree groups, or principal members of groups important for their cohesion |
| 3) Trees with identifiable historic, commemorative or habitat importance |
| 2) Trees of particularly good form, especially if rare or unusual |
| 1) Trees with none of the above additional redeeming features (inc. those of indifferent form) |
| -1) Trees with poor form or which are generally unsuitable for their location |

Score & Notes
1

Part 2: Expediency assessment

Trees must have accrued 10 or more points to qualify

- | |
|---|
| 5) Immediate threat to tree inc. s.211 Notice |
| 3) Foreseeable threat to tree |
| 2) Perceived threat to tree |
| 1) Precautionary only |

Score & Notes
3

Part 3: Decision guide

- | | |
|-------|-----------------------|
| Any 0 | Do not apply TPO |
| 1-6 | TPO indefensible |
| 7-11 | Does not merit TPO |
| 12-15 | TPO defensible |
| 16+ | Definitely merits TPO |

Add Scores for Total:
14

Decision:
TPO

PLANNING COMMITTEE -

APPLICATIONS DETERMINED UNDER DELEGATED POWERS

PURPOSE OF REPORT

- (1) To inform Members of the number of decisions issued between the production of the 24 April 2023 Planning Committee Agenda and the 5 June 2023 agenda. 180 decisions issued 174 decisions issued under delegated powers with 6 decided by the Planning Committee.
- (2) To inform Members of those applications which have been determined under the officer delegation scheme since your last meeting. These decisions are made in accordance with the Authority’s powers contained in the Town and Country Planning Act 1990 and have no financial implications.
- (3) This report does not include the following applications – Prior Notifications, Discharge of Conditions, Pre Applications, County Matters, TPO and Works to Trees in a Conservation Area
- (4) Majors are assessed against a national target of 60% determined in time. Failure to meet this target could result in the application being dealt with by Pins who will also receive any associated planning fee.

RECOMMENDATION

That the reports be noted.

Number of Decisions issued between 5 April 2023 – 18 May 2023

	Total	Approved	Refused	Under 8 weeks	Under 13 weeks	Performance %	National Target	Planning Committee decision	
								Approved	Refused
Major	9	7	2		7	78%	60%	2	2
Minor	69	60	57	57		83%	80%	2	0
Other	102	98	4	86		84%	80%	0	0
Total	180	165	63						

Planning Committee made 6 of the 180 decisions, 3%

PLANNING COMMITTEE -

APPLICATIONS DETERMINED UNDER DELEGATED POWERS

PURPOSE OF REPORT

To inform Members of those applications which have been determined under the officer delegation scheme since your last meeting. These decisions are made in accordance with the Authority's powers contained in the Town and Country Planning Act 1990 and have no financial implications.

RECOMMENDATION

That the report be noted.

DETAILS OF DECISIONS

DATE RECEIVED	DATE DETERMINED/ DECISION	REF NUMBER	APPLICANT PROPOSED DEV	PARISH/AREA
06.02.2023	14.04.2023 Application Permitted	23/00213/F	The Old Rectory Hyde Park Road Bagthorpe Norfolk VARIATION OF CONDITION 2 FROM PLANNING APPLICATION 22/01453/F - Proposed Annexe.	Bagthorpe With Barmer - VACANT
03.03.2023	05.05.2023 Application Permitted	23/00461/F	Toftsend Syderstone Road Bircham Tofts KINGS LYNN VARIATION OF CONDITIONS 2 AND 3 OF PLANNING PERMISSION 22/02252/F: Demolition of existing dwelling and construction of replacement dwelling	Bircham

01.02.2023	12.05.2023 Application Permitted	23/00232/F	Park House Mill Hill Road Boughton King's Lynn Proposed Extensions and Alterations	Boughton
11.08.2022	25.04.2023 Application Permitted	22/01606/F	Birch Trees Broad Lane Brancaster King's Lynn Replacement dwelling, garage and outdoor dining area following demolition of existing structures.	Brancaster
29.09.2022	14.04.2023 Application Permitted	22/01864/F	The Smithy Main Road Brancaster Staithe King's Lynn Variation of condition 2 of Planning Permission 19/02000/F: Replacement dwelling following partial demolition	Brancaster
22.03.2023	21.04.2023 Application Permitted	22/01912/NMA_1	12 Sawyers Yard Brancaster Norfolk PE31 8FW NON MATERIAL AMENDMENT TO PLANNING PERMISSION 22/01912/F: Loft Conversion and Installation of new rear dormer, new gable windows on new upper floor	Brancaster
21.07.2022	04.05.2023 Application Permitted	22/01499/F	Land South of Hall Farm Cottage Herrings Lane Burnham Market Norfolk Construction of a two storey dwelling and associated works	Burnham Market

08.02.2023	17.04.2023 TPO Work Approved	23/00009/TPO	Burnham Rise Herrings Lane Burnham Market King's Lynn T4-Beech. previous application approval for removal was denied. the client would now like to reduce the tree back to suitable growth points by up to 1.5m. All over crown reduction 2/TPO/00258	Burnham Market
20.02.2023	05.05.2023 Application Permitted	23/00310/F	The Old Rectory Overy Road Burnham Market King's Lynn Formation of open veranda canopy to south elevation of annexe, formation of wall to south of pool, alteration to south boundary wall and freestanding estate fence to east of main house	Burnham Market
20.02.2023	26.04.2023 Application Permitted	23/00311/LB	The Old Rectory Overy Road Burnham Market King's Lynn Formation of open veranda canopy to south elevation of annex, formation of wall to south of pool and alteration to south boundary wall.	Burnham Market
13.03.2023	10.05.2023 Application Permitted	23/00445/F	20 Market Place Burnham Market KINGS LYNN Norfolk Alteration and extension to dwelling as per application 20/00439/F	Burnham Market

21.04.2023	16.05.2023 Application Permitted	19/02109/NMA_2	Cherry Trees Church Walk Burnham Market King's Lynn NON-MATERIAL AMENDMENT of Planning Permission 19/02109/F: Demolition of an existing dwelling and replacement with a pair of link detached dwelling and associated external works	Burnham Market
30.01.2023	17.04.2023 Application Permitted	23/00161/F	Denning 7 Marsh Lane Burnham Norton Norfolk Single storey extension to two storey cottage, requiring removal of shed and localised adaptation to existing building	Burnham Norton
01.02.2023	13.04.2023 Application Permitted	23/00240/CU	Annexe At Hill Stile House 26 Norton Street Burnham Norton Retrospective Application for the change of use of an existing annexe to allow use for short stay holiday accommodation	Burnham Norton
20.02.2023	19.04.2023 Application Permitted	23/00394/F	Larksfield Gong Lane Burnham Overy Staithe King's Lynn REMOVAL OR VARIATION OF CONDITION 2 OF PLANNING PERMISSION 16/00771/F: Demolition of existing house and erection of new house. Existing garage to be retained	Burnham Overy
27.09.2022	14.04.2023 Application Permitted	22/01712/F	Old Parsonage Lodge Creake Road Burnham Thorpe King's Lynn Replacement of a Single-Storey Extension, External and Internal Refurbishment to Existing Structure	Burnham Thorpe

27.09.2022	12.04.2023 Application Permitted	22/01713/LB	Old Parsonage Lodge Creake Road Burnham Thorpe King's Lynn Replacement of a Single-Storey Extension, External and Internal Refurbishment to Existing Structure	Burnham Thorpe
22.02.2023	05.05.2023 CAMP Objection NO	23/00354/CAMP	LEITH HOUSE ORCHARDS Proposed Freedom Camping Club Leith House Mill Lane Burnham Thorpe CONSULTATION FOR AN EXEMPTED CAMP SITE	Burnham Thorpe
20.02.2023	02.05.2023 Application Permitted	23/00315/F	3 Baileygate Cottages Stocks Green Castle Acre King's Lynn Proposed rear Garden Room extension and minor alterations to first floor bathroom	Castle Acre
20.02.2023	02.05.2023 Application Permitted	23/00316/LB	3 Baileygate Cottages Stocks Green Castle Acre King's Lynn Proposed rear Garden Room extension and minor alterations to first floor bathroom	Castle Acre
24.03.2023	09.09.2023 TPO Served	23/00083/TREECA	Minns Farm Barns Priory Road Castle Acre King's Lynn T1 - Lime - pollard to a height circa 14 m (current height 20 m). T2 - Lime - pollard to a height circa 10 m (current height 18 m) T3 - Line - pollard to height circa 16 m (current height 20 m)	Castle Acre

14.02.2023	12.04.2023 Application Permitted	23/00280/CU	Simla 42 Low Road Congham King's Lynn Retrospective change of use from residential dwelling to residential dwelling and the keeping and breeding of dogs	Congham
28.12.2022	13.04.2023 Application Permitted	22/02307/F	6 Pansey Drive Dersingham King's Lynn Norfolk Two storey rear extension to dwelling	Dersingham
09.02.2023	20.04.2023 Application Permitted	23/00249/F	5 Woodside Close Dersingham King's Lynn Norfolk Retrospective single storey rear extension	Dersingham
22.03.2023	03.05.2023 GPD HH extn - Not Required	23/00545/PAGPD	3 Woodside Avenue Dersingham King's Lynn Norfolk Single storey rear extension which extends beyond the rear wall by 5.20m with a maximum height of 3.58m and a height of 2.38m to the eaves	Dersingham
27.03.2023	09.05.2023 TPO Work Approved	23/00020/TPO	53B Chapel Road Dersingham King's Lynn Norfolk (2/TPO/00179) T1 (Walnut) - Prune back lower limb by up to 2.5 metres in order to crown raise to increase light levels. T2 (Beech) - Prune back protruding mid crown branches by up to 2 metres in order to control crown spread.	Dersingham

26.01.2023	16.05.2023 Variation of Legal Agreement	16/00866/VAR1B	Land On the North Side of High Street Docking KINGS LYNN MODIFICATION OF PLANNING OBLIGATION, REFERENCE LC/S106/17/15 RELATED TO PLANNING APPLICATION REFERENCE 16/00866/OM	Docking
08.02.2023	04.05.2023 Application Permitted	23/00229/F	Cedar Lodge Sedgeford Road Docking King's Lynn Proposed Front, Rear and Side Extension to Existing Property with Internal Alterations	Docking
24.02.2023	10.05.2023 Application Permitted	23/00403/F	54 Monks Close Bircham Newton King's Lynn Norfolk VARIATION OF CONDITION 2 FROM PLANNING APPLICATION 21/00779/F - Single storey rear extension	Docking
14.02.2022	28.04.2023 Application Permitted	22/00357/FM	Grantham To Bexwell Pipeline Scheme Hybrid Planning Application for the proposed Grantham to Bexwell Pipeline Scheme with full planning consent sought for 95 kilometres of pipeline and 4 kilometre spur, and outline consent for associated above ground infrastructure at Elton and Welby Heath with all matters reserved except for access.	Downham Market

13.05.2022	27.04.2023 Application Permitted	22/00946/FM	Land SW of Denver House Sovereign Way Trafalgar Industrial Estate Downham Market 5 New industrial units for use classes comprising B1, B2 and B8	Downham Market
02.12.2022	06.04.2023 Application Permitted	22/02174/LB	Downham & Clackclose Conservative Club Eagle House 18 - 20 Bridge Street Downham Market To reduce the height of an existing chimney to 600mm at the rear of the property	Downham Market
07.12.2022	21.04.2023 Application Permitted	22/02195/F	4 Cedar Close Downham Market Norfolk PE38 9PL Proposed extension to the north (right when facing) of the existing bungalow dwelling to replace the existing brick garage and minor modification to the existing bathroom external wall to the east (front) . The proposed works to the front of the property do not extend beyond the main elevational line of the existing dwelling. The proposed north (right) extension is to be constructed to of materials to match those of the existing dwelling. The extension extends to the rear of the property leaving a 4m distance between the external wall and the neighbouring fence.	Downham Market
31.01.2023	14.04.2023 Application Permitted	23/00252/F	93 Bexwell Road Downham Market Norfolk PE38 9LJ Extension to Dwelling	Downham Market

07.02.2023	06.04.2023 Application Permitted	23/00219/F	Kabalin 112A Bexwell Road Downham Market Norfolk Proposed extensions and alterations to dwelling	Downham Market
22.02.2023	12.05.2023 Application Permitted	23/00337/F	Rose Bank 27 Crow Hall Estate Downham Market Norfolk Construction of one bungalow following demolition of existing bungalow	Downham Market
03.03.2023	27.04.2023 Application Permitted	23/00459/F	92 Retreat Estate Downham Market Norfolk PE38 9QH Erection of single storey side & rear extension and front porch	Downham Market
10.03.2023	28.04.2023 Application Permitted	22/01443/NMAM_2	Croylands 157 Bexwell Road Downham Market Norfolk NON-MATERIAL AMENDMENT: Demolition of existing dwellings and re-development to provide a 72 bedroom care home (Use Class C2) together with associated access, car and cycle parking, structural landscaping and amenity space provision.	Downham Market
31.01.2023	27.04.2023 Application Permitted	23/00175/F	Anchorage House Broomsthorpe Road East Rudham King's Lynn Proposed cart shed	East Rudham
28.03.2023	14.04.2023 Application Permitted	22/01377/NMA_1	Faize Cottage 21 Station Road East Rudham King's Lynn NON-MATERIAL AMENDMENT TO PLANNING PERMISSION 22/01377/F: Erection of single storey outbuilding to provide a games room and store	East Rudham

05.04.2023	02.05.2023 Application Permitted	22/00481/NMA_1	Mallard Cottage Station Road East Rudham King's Lynn NON-MATERIAL AMENDMENT TO PLANNING PERMISSION 22/00481/F: Single-storey extension to rear of dwelling. Insertion of 3 no. dormer windows to rear roof. (with associated internal alterations). Replacement Porch to front of dwelling. Erection of detached 3-Bay Garage and Store building on site	East Rudham
23.12.2022	14.04.2023 Application Permitted	22/02315/F	141B Elm High Road Emneth Wisbech Norfolk Change of use to form dog grooming business, and form new drop kerb access	Emneth
20.01.2023	28.04.2023 Application Permitted	23/00112/F	Hi-Rize 178 Hungate Road Emneth Wisbech Proposed garage and brick wall to the south east	Emneth
09.02.2023	25.04.2023 Application Permitted	23/00244/O	1 Gaultree Square Emneth Wisbech Norfolk Proposed 1.5 Storey Dwelling with New Access. Proposed New Access to 1 Gaultree Square, Emneth.	Emneth
24.08.2022	02.05.2023 Application Permitted	22/01686/CU	17 Fairfield Way Feltwell Thetford Norfolk Retrospective application to change the 4 bedroom property from residential to commercial, to use 4th bedroom and garage to run a dog grooming salon	Feltwell

02.11.2022	18.05.2023 Application Permitted	22/01944/F	5 Short Beck Feltwell Norfolk IP26 4AD New build dwelling with associated parking and renovation and extension to the existing garage, Outline planning already approved 18/01706/O	Feltwell
09.02.2023	05.04.2023 Application Permitted	23/00241/LB	Ivy House High Street Fincham King's Lynn Subdivision of bathroom and study, repairs and decoration	Fincham
15.02.2023	12.04.2023 Application Permitted	23/00293/F	The Old Chapel Chapel Lane Fincham King's Lynn alterations to dwelling including replacing flat roof with pitched roof	Fincham
10.03.2023	03.05.2023 Application Permitted	23/00439/F	The Bungalow High Street Fincham KINGS LYNN Single storey extension to rear of existing single storey dwelling	Fincham
06.12.2022	03.05.2023 Application Permitted	22/02184/F	3 Howards Way Gayton KINGS LYNN Norfolk Install a building mounted vertical wind turbine to the gable end of single storey garage.	Gayton
25.01.2023	18.05.2023 Application Permitted	23/00130/F	2 Manor Corner Cottage Common Lane Gayton Thorpe Norfolk Proposed two storey side extension	Gayton
01.02.2023	05.05.2023 Application Permitted	23/00188/F	Acrefield House Winch Road Gayton KINGS LYNN Singlestorey rear extensions with porch infill and internal alterations to existing	Gayton

13.02.2023	18.04.2023 Application Permitted	23/00267/F	Field Cottage Back Street Gayton King's Lynn Retention of detached gazebo to rear garden	Gayton
03.03.2023	27.04.2023 Application Permitted	23/00464/F	Field Cottage Back Street Gayton King's Lynn Side extension to garage	Gayton
22.03.2021	03.05.2023 Application Refused	21/00748/F	18 Abbeyfields Abbey Road Great Massingham King's Lynn Porch on principal elevation of the property	Great Massingham
04.08.2022	19.04.2023 Application Refused	22/01580/LB	South View 17 Weasenham Road Great Massingham King's Lynn Proposed rear single storey extension	Great Massingham
23.12.2022	15.05.2023 Application Permitted	22/02294/F	The Old Stores 3 Abbey Road Great Massingham King's Lynn Proposed First floor rear extension, reintroduction of dormer window to front elevation, rear extension to loft and new porch to front elevation.	Great Massingham
16.01.2023	18.05.2023 Application Permitted	23/00064/F	8 Abbeyfields Abbey Road Great Massingham King's Lynn Loft conversion including increased roof pitch of a single storey dwelling.	Great Massingham
15.03.2023	09.05.2023 Application Permitted	23/00487/F	Sunrise 52 Weasenham Road Great Massingham King's Lynn Conversion Of Garage Block Roof Void Into Home Office Space	Great Massingham

17.04.2023	15.05.2023 Consent Required	23/00710/SU	Mast Telecom Greengate Lane Great Massingham Norfolk Notification: Proposed Telecommunications Installation: To install of 1 GPS node at 46.1m, install 3 no. MHAs and refresh the existing cabinet internally.	Great Massingham
01.08.2022	20.04.2023 Application Refused	22/01354/LB	Ivy Farm House 37 Congham Road Grimston King's Lynn Listed building application for removal of existing rear lean-to conservatory and construction of new orangery style rear single storey extension	Grimston
23.01.2023	04.05.2023 Application Permitted	23/00121/F	2 Manor Road Heacham King's Lynn Norfolk Extensions to private dwelling including extension to existing garage	Heacham
01.03.2023	10.05.2023 Application Permitted	23/00424/F	12 Folgate Road Heacham King's Lynn Norfolk Single Storey Rear Extension And Erection Of Single Storey Front Porch	Heacham
20.04.2023	18.05.2023 Application Permitted	19/01670/NMA_1	Rest Haven 23 South Beach Heacham Norfolk NON-MATERIAL AMENDMENT TO 19/01670/F: Alterations and extensions	Heacham
05.10.2022	04.05.2023 Application Permitted	22/01904/F	Willow Lodge Flats Manor Road Hilgay Norfolk Demolition of existing building and erection of 7 no. affordable dwellings	Hilgay

09.02.2023	18.04.2023 Application Permitted	23/00247/F	The Laurels Hubbards Drove Hilgay Downham Market Construction of one dwelling and garage	Hilgay
21.02.2023	14.04.2023 Application Permitted	23/00331/F	Rose Bungalow Hubbards Drove Hilgay Norfolk Extension and alterations to bungalow	Hilgay
21.02.2023	10.05.2023 Was_Would be Lawful	23/00393/LDP	East View Ely Road Hilgay KINGS LYNN Application for lawful development for the demolition of existing conservatory and construction of utility room extension	Hilgay
02.03.2023	10.05.2023 Was_Would be Lawful	23/00451/LDE	East View Ely Road Hilgay KINGS LYNN Application for a Lawful Development Certificate for an existing garage	Hilgay
09.03.2023	10.05.2023 Application Permitted	23/00415/F	Land Adjacent 3 Wheatfields Hillington King's Lynn Norfolk Proposed dwelling and garage	Hillington
20.04.2022	12.05.2023 Application Permitted	22/00691/FM	Richmond House 6 - 8 Westgate Hunstanton HUNSTANTON Extensions, alterations and part conversion of mixed use building (hotel & flats) into 13 residential units	Hunstanton
03.11.2022	10.05.2023 Application Permitted	22/02007/F	Telephone Exchange Homefields Road Hunstanton Norfolk Creation of 2 storage units (Shipping containers), new fence and area of hardstanding	Hunstanton

13.12.2022	28.04.2023 Was Lawful	22/02222/LDE	14 Boston Square Hunstanton Norfolk PE36 6DU Application for a lawful development: Renovation of 3 flats, including roof light to top flat in accordance with Planning Permission HU1887	Hunstanton
09.02.2023	25.04.2023 Application Permitted	23/00236/F	24 Kings Road Hunstanton Norfolk PE36 6ES Proposed Replacement Dwelling Following Demolition of Existing Bungalow	Hunstanton
17.02.2023	03.05.2023 Application Permitted	23/00302/F	15 Lincoln Street Hunstanton Norfolk PE36 6AS Proposed Garage, Workshop and Alterations	Hunstanton
20.02.2023	05.05.2023 Was_Would be Lawful	23/00378/LDP	2 Chiltern Crescent Hunstanton Norfolk PE36 5DB Application for a Lawful Development Certificate for the proposed siting of a mobile home/caravan within the residential curtilage comprising an ancillary residential use	Hunstanton
06.03.2023	28.04.2023 Application Permitted	23/00379/F	Cliff Edge 29 Lighthouse Lane Hunstanton Norfolk Single Storey extension to dwelling	Hunstanton
31.03.2023	15.05.2023 CAMP Objection NO	23/00613/CAMP	Glebe House School 2 Cromer Road Hunstanton Norfolk CONSULTATION FOR AN EXEMPTED CAMP SITE	Hunstanton
29.11.2022	27.04.2023 Application Permitted	22/02135/F	12 Davy Field Lynn Road Ingoldisthorpe KINGS LYNN Construction of dwelling on Plot 12	Ingoldisthorpe

06.10.2022	15.05.2023 Application Permitted	22/01905/F	38 Railway Road King's Lynn Norfolk PE30 1NF Single storey, second floor extension over existing fabric and internal alterations to create 3 flats in place of existing dwellinghouse.	King's Lynn
06.12.2022	17.04.2023 Application Permitted	22/02183/F	2 St Nicholas Street King's Lynn Norfolk PE30 1LY Conversion of dwelling into 3No Dwellings	King's Lynn
20.12.2022	17.05.2023 Application Refused	22/02248/F	Everard Mews 16 North Everard Street King's Lynn Norfolk 4no. proposed residential units on vacant land	King's Lynn
22.12.2022	18.04.2023 Application Permitted	22/02280/LB	2 Thoresby College Queen Street King's Lynn Norfolk Listed Building Application: Internal alterations and improvements including some general repairs	King's Lynn
22.12.2022	19.04.2023 Application Permitted	22/02281/A	2 Thoresby College Queen Street King's Lynn Norfolk Retrospective Advertisement Application: 4x Internal non illuminated signs displaying logo of KL Magazine	King's Lynn
23.12.2022	13.04.2023 Application Permitted	22/02292/A	Seacroft Mobillity 50 High Street King's Lynn Norfolk advertisement application for 1 x non illuminated fascia sign and 1 x non illuminated hanging shop sign	King's Lynn

05.01.2023	11.04.2023 Application Permitted	23/00010/LB	Seacroft Mobillity 50 High Street King's Lynn Norfolk Retrospective Listed Building Application: Removal of external signage with new external signage	King's Lynn
13.01.2023	21.04.2023 Application Permitted	23/00053/F	Mars Foods Ltd Hansa Road Hardwick Industrial Estate King's Lynn CULVERTING OF 40m OF DITCH ADJACENT TO 59 KINGS AVENUE TO ASSIST STABILISING THE GARDEN OF 59 KINGS AVENUE	King's Lynn
23.01.2023	14.04.2023 Application Permitted	23/00117/F	31 Methuen Avenue Gaywood King's Lynn Norfolk Single storey rear extension and internal alterations (retrospective)	King's Lynn
24.01.2023	12.04.2023 Application Permitted	23/00147/F	12 Milton Avenue King's Lynn Norfolk PE30 2QH Single storey rear extension and alterations to dwelling.	King's Lynn
26.01.2023	03.04.2023 Application Permitted	23/00145/LB	Woodgreen 83 High Street King's Lynn KINGS LYNN Listed Building Application: Like for like replacement of the signage from 'Hotter' to 'Woodgreen'	King's Lynn
27.01.2023	14.04.2023 Application Permitted	23/00193/F	Greenland House 28 Bridge Street King's Lynn Norfolk Replacement of dilapidated section of boundary wall new brick wall to match adjacent walls	King's Lynn

31.01.2023	28.04.2023 Application Permitted	23/00181/LB	TSB 1 - 3 Tuesday Market Place King's Lynn Norfolk Listed Building Application: Proposal for the careful removal of the black paint to the engraved high level Lloyds Bank lettering.	King's Lynn
01.02.2023	11.04.2023 Application Permitted	23/00264/LB	The Dental Surgery 6 King Street King's Lynn Norfolk LISTED BUILDING CONSENT: To remove two partition walls and a door leading into the partitioned area (referred to as the dark room/doffing room) in the rear first floor staff room. Block up door to reinstate room to original layout and remedial works to the gable end	King's Lynn
09.02.2023	13.04.2023 Application Permitted	23/00246/LB	2 St Nicholas Street King's Lynn Norfolk PE30 1LY Conversion of dwelling into 3No Dwellings	King's Lynn
13.02.2023	14.04.2023 Application Permitted	23/00262/F	The Cottage Water Lane King's Lynn Norfolk The proposal is to provide a new single storey extension to form larger Kitchen / Dining area and convert existing garage space to form new utility and gym space.	King's Lynn
20.02.2023	14.04.2023 Application Permitted	23/00318/F	1 Gresham Close King's Lynn Norfolk PE30 3EJ Proposed Single Storey Rear and Side Extensions Incorporating Garage Conversion	King's Lynn

21.02.2023	18.05.2023 Application Permitted	23/00328/F	33 Kensington Road King's Lynn Norfolk PE30 4AS Two-storey side extension, single storey rear extension, and new detached workshop.	King's Lynn
09.03.2023	04.05.2023 Application Permitted	23/00416/F	BCKLWN Depot Oldmedow Road Hardwick Industrial Estate King's Lynn PROPOSED EXTERNAL LIGHTING TO EXISTING STAFF CARPARK	King's Lynn
13.03.2023	03.05.2023 Application Permitted	08/01122/NMA_1	333 Wootton Road King's Lynn Norfolk PE30 3AX NON-MATERIAL AMENDMENT TO PLANNING PERMISSION 08/01122/F: First floor extension and new garage	King's Lynn
13.03.2023	15.05.2023 Application Permitted	23/00432/F	R & L Engineering 26 - 28 Enterprise Way Hardwick Narrows King's Lynn retrospective first floor cabin extension	King's Lynn
17.03.2023	16.05.2023 Application Permitted	23/00509/F	35 Burnham Avenue King's Lynn Norfolk PE30 3EN Removal of existing extension and conservatory and replaced with single storey ground floor rear extension complete with internal alterations.	King's Lynn
21.03.2023	12.05.2023 Was_Would be Lawful	23/00533/LDP	3 Silver Hill King's Lynn Norfolk PE30 4TL Proposed Garage Conversion to 3 Silver Hill, Kinglynn, PE30 4TL	King's Lynn

27.03.2023	11.05.2023 Application Permitted	23/00572/LB	Dental Surgery 10 Stonegate Street King's Lynn Norfolk Replacement of 4 modern door and fireproofing of one existing door	King's Lynn
29.03.2023	18.04.2023 Application Permitted	20/00470/NMAM_1	West Lynn Primary School St Peters Road West Lynn King's Lynn NON-MATERIAL AMENDMENT TO PLANNING PERMISSION 20/00470/RMM: Reserved Matters application for 38 dwellings. Details of layout, scale, appearance and landscaping. In accordance with Condition 8 of the outline planning permission, the scheme includes a vehicular access to the West Lynn Drain along with a 9m easement strip. In accordance with Condition 26 of the outline planning permission, the scheme includes a 15m exclusion zone around the Anglian Water Pumping Station	King's Lynn
11.04.2023	02.05.2023 Application Permitted	15/02132/NMA_1	Victory Court 34 Bryggen Road North Lynn Industrial Estate King's Lynn NON MATERIAL AMENDMENT TO PLANNING PERMISSION 15/02132/F: New office units at Victory Court (retrospective)	King's Lynn

03.03.2023	28.04.2023 Application Permitted	23/00465/F	Almaza House 2A Gayton Road Ashwicken Norfolk Single Storey Front Extension To Garage And Single Storey Side Extension With Covered Outdoor Area	Leziate
13.12.2022	28.04.2023 Application Permitted	22/02215/F	The Lookout Station Road Little Massingham KINGS LYNN Retrospective Application: Installation of ground level solar panels in an existing garden	Little Massingham
15.02.2023	26.04.2023 Application Permitted	23/00366/F	Ashhopton Spring Lane Marham KINGS LYNN Double storey side Extension	Marham
06.01.2023	21.04.2023 Application Refused	23/00029/F	Barn E of Crown Farmhouse Middle Drove Marshland St James Norfolk Proposed demolition of existing barns and proposed 2 No. Dwellings	Marshland St James
25.01.2023	03.05.2023 Application Permitted	23/00131/F	Mays Farm Cottage Moyses Bank Marshland St James Norfolk Two Storey Side Extension and Part Retrospective Detached Garage	Marshland St James
05.01.2023	03.05.2023 Application Permitted	23/00020/F	1 Globe Street Methwold Norfolk IP26 4PQ Change of use and renovation of existing barn to residential dwelling.	Methwold
27.02.2023	21.04.2023 Application Permitted	23/00404/F	1 Denton Lodge Cottages Mundford Road Feltwell Norfolk Single Storey side and rear extensions	Methwold

23.01.2023	18.05.2023 Application Permitted	23/00138/F	William George Business Park William George Way Blackborough End Norfolk Construction of 3no Storage Units (Non Residential) complete with electricity supply to each individual unit, concrete hardstanding and dedicated access drive onto William George Way	Middleton
04.05.2022	26.04.2023 Application Permitted	22/00781/FM	White Barn Farm Silt Road Nordelph Norfolk Erection of a commercial B2 General industrial building	Nordelph
03.03.2023	12.04.2023 Tree Application - No objection	23/00062/TREECA	Threeways Downham Road Nordelph Downham Market We wish to cut down all five small tress growing in our back lawn and grind the remaining small stumps to enable lawn to flourish. One magnolia and one holly behind the shed is overshadowing lawn and making garden damp. Two small cooking apple trees have not been pruned and are now way too tall to harvest and one small nearby tree nearby that is dead.	Nordelph
02.02.2023	14.04.2023 Application Permitted	23/00198/F	Glebe Farm Wells Road North Creake Fakenham Construction of traditional wooden cart shed & potting shed.	North Creake

14.04.2023	12.05.2023 AG Prior Notification - NOT REQD	23/00686/AG	Agricultural Buildings Crossways Farm W of Highfield Burnham Road North Creake Norfolk Agricultural Prior Notification: New wooden cabin to be used solely as a a new farm office and restroom with welfare facilities	North Creake
02.11.2018	27.04.2023 Application Permitted	18/01966/RMM	Morston Point Land North East of Scania Way Hardwick Industrial Estate King's Lynn Reserved Matters: Erection of supermarket	North Runcton
02.11.2018	28.04.2023 Application Permitted	18/01967/RM	Morston Point Land North East of Scania Way Hardwick Industrial Estate King's Lynn Reserved Matters: Erection of food establishment/drive thru takeaway	North Runcton
16.02.2023	28.04.2023 Application Permitted	23/00372/F	Oakdean Manor Road North Wootton King's Lynn Demolition of existing garage, conservatory and rear extensions and construction of new single/two storey extension with attached garage	North Wootton
21.02.2023	04.05.2023 Application Permitted	23/00336/F	Fairhaven 11 Wheatley Drive North Wootton King's Lynn Front and rear extensions to detached bungalow	North Wootton
10.03.2023	16.05.2023 Application Permitted	23/00494/F	31 Carlton Drive North Wootton King's Lynn Norfolk Proposed first floor rear extension and single storey front extension	North Wootton

21.02.2023	18.04.2023 Application Permitted	23/00387/F	11 Smugglers Close Old Hunstanton Hunstanton Norfolk First floor extension and alteration to dwelling (identical re-submission of approval 20/00014/F)	Old Hunstanton
03.03.2023	03.05.2023 Application Permitted	23/00463/F	78 Old Hunstanton Road Old Hunstanton Hunstanton Norfolk VARIATION OF CONDITION 2 FROM PLANNING APPLICATION 22/02076/F - Proposed extensions to private dwelling	Old Hunstanton
16.02.2023	28.04.2023 Application Permitted	23/00371/F	14 Isle Bridge Road Outwell Wisbech Norfolk Single storey side/rear extension to dwelling	Outwell
16.02.2023	04.05.2023 LDP LB NOT Lawful	23/00300/LDE	4 Harvestile Lane Pentney King's Lynn Norfolk Lawful use of land as residential instead of agricultural	Pentney
08.11.2022	18.04.2023 Application Permitted	22/01979/F	Manningham House 21 High Street Ringstead Hunstanton Rear extension, loft conversion and alterations to dwelling	Ringstead

20.12.2022	09.05.2023 Application Permitted	22/02249/F	Woodlakes Leisure Ltd Woodlakes Caravan & Camping Park Holme Road Stow Bridge Development of health and wellbeing guest facilities, including: hot food preparation/kitchen area, bar/food outlet, WC facilities, 2 no. beauty treatment lodges, accommodation, and associated dining/seating, ancillary to the wider existing Woodlakes Park (Retrospective)	Runcton Holme
03.10.2022	05.05.2023 Application Permitted	22/01752/FM	Downham Country Garden Store Stonecross Road Bexwell Downham Market Proposed Garden Centre Extension to create storeroom / new frontage façade / entrance and exit lobbies, installation of surface drainage attenuation tank and Generator for additional surface water drainage and extension of existing gravel carpark.	Ryston
03.03.2023	24.04.2023 Application Permitted	23/00453/F	2 Jarvie Close Sedgeford Norfolk PE36 5NG Proposed Single Storey Rear Extension	Sedgeford
15.02.2023	12.04.2023 Application Permitted	23/00289/LB	The Coach House Snettisham House St Thomas Lane Snettisham Proposed change of pantile roof to glazed roof to allow more natural light into the lounge and kitchen	Snettisham

17.06.2022	06.04.2023 Application Permitted	22/01246/LB	WWII Observation Post Between Bloodgate Hill And London Lane South Creake Norfolk Listed building application for restoration of existing ROC Monitoring Post/Subterranean Bunker and erection of 5no self contained holiday lets with associated parking and landscaping	South Creake
17.02.2023	24.04.2023 Application Permitted	23/00301/F	Rose Cottage 8 Burnham Road South Creake Fakenham 1st Floor extension over existing footprint to create an additional bedroom. Existing glass roof to be replaced with a flat roof with a lantern at rear.	South Creake
28.12.2022	11.05.2023 Application Permitted	22/02301/F	Land To the South of 9 And N of Oxborough Drive Green Lane South Wootton Norfolk VARIATION OF CONDITION 7 OF PLANNING PERMISSION 20/01123/O: Proposed 4 No Dwellings (Outline)	South Wootton
27.02.2023	28.04.2023 Application Permitted	23/00406/F	18 Fountaine Grove South Wootton King's Lynn Norfolk Proposed two storey and single storey rear extension	South Wootton
01.03.2023	24.04.2023 Application Permitted	23/00427/F	10 Green Lane South Wootton King's Lynn Norfolk Rear extension and alterations to dwelling	South Wootton

02.03.2023	27.04.2023 Application Permitted	23/00448/CU	25 Wimpole Drive South Wootton King's Lynn Norfolk Retrospective planning permission is required to change the use of the land from agricultural to garden Land. It will be used for lawn and plants and for children to play on (recreational).	South Wootton
06.03.2023	28.04.2023 TPO Partial	23/00017/TPO	Sylvan House 116 Nursery Lane South Wootton King's Lynn (2/TPO/00072) T4 Common Laburnun - Remove deadwood and sever ivy upto 1.5m from the ground level. T5 Hazel - Remove deadwood. T15 - Common Alder - Monolith to main fork at 4m to retain habitat features leaving lowest live branch. Leave cord wood in habitt piles and chip into woodland. T28 Aspen - Clear up failed stems or use to make dead hedge. T29 Common Ash - Remove deadwood over 50mm in diameter. T30 Common Ash - Fell to ground level.	South Wootton

09.03.2023	09.05.2023 Application Permitted	23/00490/F	Land W of South Wootton School Off Edward Benefer Way King's Lynn Norfolk VARIATION OF CONDITIONS 1 and 9 OF PLANNING APPLICATION 20/01954/RMM - Reserved Matters Application following outline planning permission 17/01151/OM for the construction of 450 dwellings with associated infrastructure, to include access, landscaping, appearance, layout and scale	South Wootton
10.03.2023	10.05.2023 Application Permitted	23/00413/F	22 Wimpole Drive South Wootton King's Lynn Norfolk RETROSPECTIVE APPLICATION: I intend to use this land to extend my existing drive and also use as a garden area, with grass, shrubs, flowers, herb garden and raised bed.	South Wootton
17.03.2023	18.04.2023 Application Permitted	15/01782/NMAM_1	Land Accessed Between 144 And 150 Grimston Road Grimston Road South Wootton Norfolk NON MATERIAL AMENDMENT TO OUTLINE PLANNING PERMISSION 15/01782/OM: Outline application with all matters reserved for proposed residential development with access off Grimston Road. Land accessed between 144 and 150 Grimston Road South Wootton	South Wootton

25.01.2023	25.04.2023 Application Permitted	23/00134/F	39 Feltwell Road Southery Downham Market Norfolk Construction of one dwelling following demolition of existing dwelling	Southery
13.02.2023	24.04.2023 Application Permitted	23/00346/F	14 Upgate Street Southery Downham Market Norfolk Single storey rear extension and internal alterations	Southery
22.02.2023	10.05.2023 Application Permitted	23/00343/F	Godsold House Lynn Road Stoke Ferry King's Lynn Demolition of existing rear porch extension,,erection of a single storey rear extension, alteration of a window to the rear of the house to form a new door, insertion of two windows to the side elevation and installation of flush integrated photovoltaic panels to the south facing slope of the existing slate roof.	Stoke Ferry
24.02.2023	18.05.2023 Application Permitted	23/00347/F	Corgi Lodge Greatmans Way Stoke Ferry Norfolk New roof to create higher pitch and add 2 bedrooms for large family.	Stoke Ferry
14.03.2023	12.05.2023 Application Permitted	23/00477/F	13 The Causeway Stow Bridge King's Lynn Norfolk single storey extension on side of existing single storey dwelling	Stow Bardolph
28.03.2023	25.04.2023 AG Prior Notification NOT REQD	23/00568/AG	Newlings Farm Outwell Road Stow Bridge Norfolk Agricultural Prior Notification: Proposed grain store	Stow Bardolph

24.01.2023	12.04.2023 Application Permitted	23/00127/F	Nelson House 113 Hay Green Road South Terrington St Clement King's Lynn Single storey extension	Terrington St Clement
10.02.2023	19.04.2023 Application Permitted	23/00258/F	56 Wanton Lane Terrington St Clement King's Lynn Norfolk Extension to detached bungalow	Terrington St Clement
20.02.2023	10.05.2023 Application Permitted	23/00320/F	6 Nursery Close Terrington St Clement Norfolk PE34 4RL Small side extension	Terrington St Clement
22.02.2023	04.05.2023 Application Permitted	23/00395/F	128 Old Roman Bank Terrington St Clement King's Lynn Norfolk VARIATION OF CONDITION 1 OF PLANNING CONSENT 17/00857/RM: Reserved Matters Application: Detached dwelling	Terrington St Clement
22.02.2023	03.05.2023 Application Permitted	23/00397/F	128 Old Roman Bank Terrington St Clement King's Lynn Norfolk Retrospective Application: Change of use from agricultural land to residential land to extend residential curtilage and erection of domestic outbuilding	Terrington St Clement
03.03.2023	05.05.2023 Application Permitted	23/00466/F	Wynchmor 15 Hillgate Street Terrington St Clement King's Lynn Single storey rear extension and alterations	Terrington St Clement

17.04.2023	03.05.2023 Application Permitted	22/00809/NMA_2	Porcherie Barn 46 Tuxhill Road Terrington St Clement KINGS LYNN NON-MATERIAL AMENDMENT TO PLANNING PERMISSION 22/00809/F: Conversion of the existing barn, which currently has Class Q approval, on site to a residential dwelling	Terrington St Clement
07.11.2022	12.05.2023 Application Refused	22/01969/F	The Ridings 94 School Road Terrington St John Norfolk Construction of 2no. Dwellings and Extension/Alterations to Existing Dwelling	Terrington St John
04.11.2022	14.04.2023 Application Permitted	22/01960/F	Land At Thurnham Lyng Farm Mast Telecom Ringstead Road Thornham The removal of the existing 3no.antennas and replacement with 3no. antennas, 6m extension to existing lattice tower, installation of 3no. transmission dishes, 9no. Remote Radio Units (RRHs), relocation of existing cabinet and ancillary development thereto including 1no. GPS module	Thornham

16.02.2023	12.05.2023 Application Permitted	23/00303/F	Plumtrees Green Lane Thornham Hunstanton Minor alterations including replacing existing 1500mm wide rear french doors to lounge with 2400mm sliding doors to north elevation and new aluminim doors to replace exisitng wooden frnech doors to south elevation	Thornham
30.01.2023	02.05.2023 Application Refused	23/00162/F	1 Willow Drive Tilney All Saints King's Lynn Norfolk Rear Extension, Roof Works and New Dormer Windows.	Tilney All Saints
08.02.2022	14.04.2023 Application Permitted	22/00197/F	Scrimshaw Farm Lynn Road Tilney All Saints Norfolk Rear two storey extension, and new entrance with porch	Tilney St Lawrence
01.02.2023	27.04.2023 Application Permitted	23/00189/F	Gray's Hall Lynn Road Tilney All Saints Norfolk Replacement windows and alterations to listed building	Tilney St Lawrence
01.02.2023	28.04.2023 Application Permitted	23/00190/LB	Gray's Hall Lynn Road Tilney All Saints Norfolk Replacement of 11 windows and 2 external doors; Demolition and making good of the boiler house and internal alterations	Tilney St Lawrence
14.02.2023	19.04.2023 Application Permitted	23/00270/F	35 Magdalen Road Tilney St Lawrence King's Lynn Norfolk Dropped kerb for vehicle access for driveway	Tilney St Lawrence

28.12.2022	17.05.2023 Application Refused	22/02306/F	Briarfields Hotel Main Road Titchwell King's Lynn Creation of new access to the west of the site and construction of new wall to block existing access to the east of the site and creation of gravel parking area.	Titchwell
23.03.2023	09.05.2023 Tree Application - No objection	23/00070/TREECA	Manor Hotel Main Road Titchwell King's Lynn T1 - Evergreen Oak - Fell, tree stump will remain at a suitable height above ground level. The tree has been previously pollarded several times and has numerous pockets of rot within the stem, landscape alterations are planned for the immediate area.	Titchwell
20.10.2022	21.04.2023 Application Permitted	22/01946/F	Two Jays 50 Baptist Road Upwell Norfolk Proposed extension and alterations. Proposed swimming pool and Proposed garage and Retrospective Fence	Upwell
01.11.2022	18.04.2023 Application Permitted	22/02001/F	The Old Piggery March Riverside Upwell WISBECH Front extension to garage to form car port, single storey side extension and two storey rear extension and alterations to dwelling and construction of covered area and construction of a landing stage.	Upwell

14.11.2022	14.04.2023 Application Permitted	22/02027/F	Anvil Main Road Three Holes Norfolk front extension to create garage and installation of extended drop curb.	Upwell
15.12.2022	20.04.2023 Application Permitted	22/02226/F	Land And Buildings Immediately S of 5 Pinfold Road Upwell Wisbech Residential development - Four new dwellings, involving the demolition of three farm buildings.	Upwell
21.12.2022	27.04.2023 Application Permitted	22/02260/F	19 Green Road Upwell Wisbech Norfolk Proposed replacement dwelling	Upwell
09.11.2022	24.04.2023 Application Permitted	22/01993/F	Two Jays Church Road Walpole St Peter Norfolk Erection of detached machine/tack/hay store for use in connection with existing paddock	Walpole
17.02.2023	13.04.2023 Application Permitted	23/00314/F	Townshend Farm Cottage Church Road Walpole St Peter WISBECH Proposed single-storey side and rear extensions to dwelling, including demolition of existing single-storey extension.	Walpole
25.10.2022	27.04.2023 Application Permitted	22/01899/F	Fern House Market Lane Walpole St Andrew Wisbech Retention of existing Manege Arena for private use	Walpole Cross Keys
22.06.2022	28.04.2023 Application Permitted	22/01090/F	Trinity Hall Farm Trinity Road Walpole Highway WISBECH Proposed farmhouse dwelling for farm's site manager	Walpole Highway

09.02.2023	25.04.2023 Application Permitted	23/00237/F	Ivy House West Drove South Walpole Highway Wisbech Proposed single and two storey extension.	Walpole Highway
14.03.2023	16.05.2023 Application Permitted	23/00476/F	3 Hall Road Walpole Highway Wisbech Norfolk Proposed loft conversion and rear and side extension.	Walpole Highway
03.08.2022	28.04.2023 Application Permitted	22/01567/F	1 Popenhoe Cottages Station Road Walsoken Wisbech Replacement barn style dwelling. The proposal is to demolish the existing barn, which sits with permission to convert into a dwelling under ref 21/01889/PACU3. The existing barn has been deemed not suitable for conversion due to its poor quality and operational inefficiencies. It is proposed that a new residential development will be constructed in its place.	Walsoken
26.09.2022	06.04.2023 Application Permitted	22/01701/O	Heating & Pumping Services 5 Church Road Walsoken Wisbech Outline application with some matters reserved for residential development of the site - 4 dwellings, involving the demolition of the existing commercial existing buildings on the site	Walsoken

23.11.2022	26.04.2023 Application Permitted	22/02097/F	Land Rear of Little Eastfield Barn Lynn Road Walsoken 2 x Proposed storage building (retrospective application) for horticultural .	Walsoken
13.12.2022	26.04.2023 Application Refused	22/02221/O	Little Eastfield Barn Lynn Road Walsoken Norfolk Outline application with all matters reserved for up to two proposed dwellings	Walsoken
16.02.2023	03.05.2023 Application Permitted	23/00299/F	4 Herneside Hurn Drove Welney Norfolk Demolish rear single storey extension and rebuild on new 250mm thick piled RC raft	Welney
22.11.2022	27.04.2023 Application Permitted	22/02085/F	Ashcroft Farm Main Road Crimpleham Norfolk Removal of Condition 10 attached to Planning Permission 08/01046/F: Demolition of existing storage building and erection of offices, improvements to car parking and landscaping	West Dereham
12.01.2023	27.04.2023 Application Permitted	23/00042/F	Ingleborough Farm Mill Road West Walton Norfolk Proposed wildlife pond and landscaping	West Walton
13.01.2023	27.04.2023 Application Permitted	23/00051/F	Ingleborough Farm Mill Road West Walton Norfolk Proposed Wildlife Pond	West Walton
07.02.2023	14.04.2023 Application Permitted	23/00279/F	Scholes 9 Ingleborough Farm Mill Road West Walton Extension to Dwelling	West Walton

21.02.2023	03.05.2023 Application Permitted	23/00324/F	Georgina Wisbech Road Church End West Walton Rear Extension, Alterations and New Garage	West Walton
27.02.2023	18.05.2023 Application Permitted	23/00408/F	Sebastapol Farm Mill Road West Walton Wisbech Proposed full plan conversion of a Barn with existing prior approval into a 3 bed 2 storey dwelling, domestic garden and upgraded access.	West Walton
07.02.2023	13.04.2023 Application Permitted	23/00276/F	Hall House 4 Commonside West Winch Norfolk Extension of dwelling and conversion of detached garage to create annexed accommodation.	West Winch
10.02.2023	19.04.2023 Application Permitted	23/00256/F	Foxbury Millfield Lane West Winch King's Lynn Proposed Single Storey Rear Extensions To Existing Bungalow And Annexe With Additional Windows / Doors	West Winch
02.08.2022	17.05.2023 Application Permitted	22/01549/RM	Land NW of Willow Farm 45 Mill Road Wiggshall St Germans Norfolk Reserved Matters: Construction of 4 Detached Dwelling Houses	Wiggshall St Germans

05.09.2022	20.04.2023 Application Permitted	22/01590/F	48 Common Road Wiggshall St Mary The Virgin KINGS LYNN Norfolk Detached dwelling with garage and landscaping works incidental to the development area. (Revised design to planning consent 18/01288/RM)	Wiggshall St Germans
09.02.2023	11.05.2023 Application Refused	23/00335/F	28 St Peters Road Wiggshall St Germans Norfolk Demolition of an existing building and construction of a new detached 2 bedroom replacement dwelling	Wiggshall St Germans
10.02.2023	12.04.2023 Application Permitted	23/00259/F	Bramble Cottage 46 Fitton Road Wiggshall St Germans King's Lynn Replacement of single storey kitchen with two storey extension to detached dwelling	Wiggshall St Germans
01.03.2023	25.04.2023 Application Permitted	23/00428/F	Manor Farm Stow Road Stow Bridge King's Lynn To construct an additional silo to existing grain store to store 5000 tonnes.	Wiggshall St Mary Magdalen

14.04.2023	19.04.2023 Application Permitted	23/00140/NMA_1	Land South of 85 Stow Road Wiggenhall St Mary Magdalen Norfolk NON-MATERIAL AMENDMENT TO PLANNING PERMISSON 23/00140/F: VARIATION OR REMOVAL OF CONDITIONS 1, 2 AND 6 OF PLANNING PERMISSION 22/01892/F: VARIATION OF CONDITION 1 OF PLANNING PERMISSION 21/00253/F: Reserved Matters application: Construction of 9 dwellings	Wiggenhall St Mary Magdalen
04.11.2022	12.05.2023 Application Permitted	22/01955/F	Upper Farm New Road Wimbotsham Norfolk Redevelopment of Upper Farm Barns barns to provide two new dwellings	Wimbotsham
09.03.2023	03.05.2023 Application Permitted	23/00414/F	3 Millers Lane Wimbotsham Norfolk PE34 3QF Extension to front of existing garage to form open-sided carport	Wimbotsham
21.02.2023	14.04.2023 Application Permitted	23/00323/F	TWO Bardolphs Way Wormegay Norfolk Proposed single storey rear extension	Wormegay
16.03.2023	15.05.2023 Application Permitted	23/00501/F	Ladysmith Cottage Castle Road Wormegay King's Lynn Rear and side single storey extension including demolition of existing garage	Wormegay

24.03.2023	21.04.2023 AG Notification NOT REQD	23/00549/AG	Agricultural Field Off West Dereham RoadWereham Norfolk Agricultural Prior Notification: Agricultural storage building	Wretton
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